



J u n e 2 0 0 8

Salt Lake County Jordan River Trail Master Plan



LANDMARK
DESIGN
INCORPORATED



**PARKS
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1.0 Introduction and Background

1.1 Brief History of the River

Seven creeks flowing west from the Wasatch Mountains and three creeks flowing east from the Oquirrh Mountains capture snow melt and rainwater. Precipitation from this vast watershed drains into the Jordan River as it flows through Salt Lake County. Farther south, creeks and rivers capture water in Utah County filling Utah Lake – the Jordan River's headwaters.

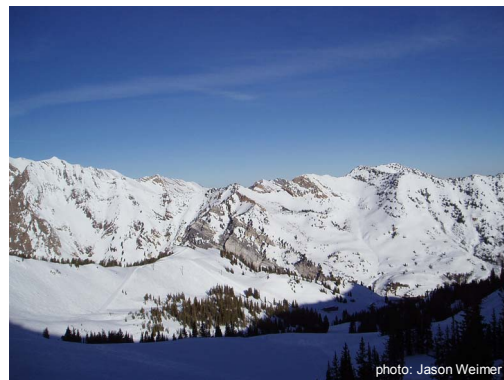


Figure 1.1 Wasatch Mountains

Waters flow north for approximately 44 miles and ultimately empty into Great Salt Lake, gathering in the shallow, broad lake until evaporation takes it drop-by-drop back into the cycle that nourishes and sustains life forms in Salt Lake Valley. Great Salt Lake is the water's final destination.

The cultural history of the Jordan River starts with the first human presence in the Valley when people lived with the river, its floods and meanders, and made no attempt to tame or conquer it. Over time, that changed, early settlers dug canals for irrigation, built bridges, and forced the river into narrower and narrower spaces. Through time, the Jordan River has been a place to gather foodstuffs and hunt, a barrier and frightening rampage, an important player in the local agricultural and industrial economy, habitat for numerous forms of wildlife, a navigable waterbody assisting in the transportation of goods and people, a biblical reference, a dumping ground, and a wild river tamed by human influence. Yet, it has always been a connecting thread of green winding through the lowest elevations in the Valley. In the harsh Great Basin Desert, it remains a welcome and refreshing ribbon of vegetation now almost completely connected with trails and access to recreational opportunities.

It is the river's ability to provide human challenges and pleasures, to evoke appreciation and respect for nature and natural processes, and



Figure 1.2 Water wheel on Jordan River, 1914



Figure 1.3 Man in boat on Jordan River, 1910

to inspire insights into those critical qualities, that give modern residents a sense of connection to nature and wildness. All this in an otherwise thoroughly developed urban environment. Thus, the Jordan River has long been identified as an especially important target for planning, conservation, and protection.

Many studies have been completed documenting various assets and aspects of the Jordan River in Salt Lake County. Twelve communities along the river's course have drawn their boundaries to cross or touch its banks. From Draper and Riverton to Salt Lake City, planning documents focus on the Jordan River as a valuable community resource and a unique opportunity. At the time of this plan's development, other studies are underway as well.

Some communities have completed building entire sections of the trail, while others still have critical gaps that need to be filled in order to complete a thoroughly connected surface trail for pedestrians, bicyclists, and in some places equestrian users. Some communities have included the river as a navigable urban water trail; others have not, but the opportunity and potential remain.

1.2 Brief Description of the Corridor

The Jordan River moves through a rapidly developing Salt Lake Valley where land uses range from remnant agricultural fields to residential, commercial, and industrial development. Occasionally the river runs through or past parks, golf courses, and other open spaces for recreation. Ownership of properties adjacent to the river are diverse including public lands held by Salt Lake County, various cities along the corridor, Utah State government agencies, private gun and duck clubs, and many other public and private owners.

The Jordan River is traversed by major roadways, bridges, railroad tracks, and highways that connect communities east and west. Dams, weirs, bank-armoring, and channelization activities modify its natural flows. Fences cross the river bed, giant machines dredge straight sections where silt builds-up, oxbows are drained and filled, and piers, ramps, and platforms are constructed to permit direct access to the water for boaters and anglers. Jordan River is not wild any more, yet it still contains stretches that allow users to feel secluded and in a wild place.

In the south at the Narrows, the Jordan River forms a deep canyon where its waters are almost inaccessible. Toward the center of the valley it broadens and meanders north to Great Salt Lake in Davis County. In some locations the riverbanks are stable, tree-lined, heavily vegetated with native plant species, and connected to valuable wetlands. In other areas the banks have been disturbed through natural events such as river meander or human activities such as dredging for flood control, allowing the establishment of weeds and invasive plant species and resulting in unstable banks and the destruction of some adjacent trail facilities.

Users of the multi-purpose trail generally fall into three categories:

pedestrians, cyclists, and equestrians, though other uses such as in-line skating also take place on the trail system. Pedestrians are walking, jogging, and running for pleasure or exercise. Cyclists are most commonly recreational riders who leisurely bike the trail, but bicycle commuters traveling to and from work and shopping also use the trail, usually at a faster pace. Riders on horseback use portions of designated equestrian trails, as well as other non-paved trails along

the river. It is not uncommon for all of these users to be in the corridor at the same time, generally without major conflicts.

Users of the urban water trail float or paddle non-motorized watercraft like canoes, kayaks, and rafts. They access the water trail at various locations using launches and portages that have been developed by entities adjacent to the river. Boaters are both expert and novice; and the river offers valued experiences for both.

As Figure 1.4 illustrates, this master plan includes the river and its trails in Salt Lake County.

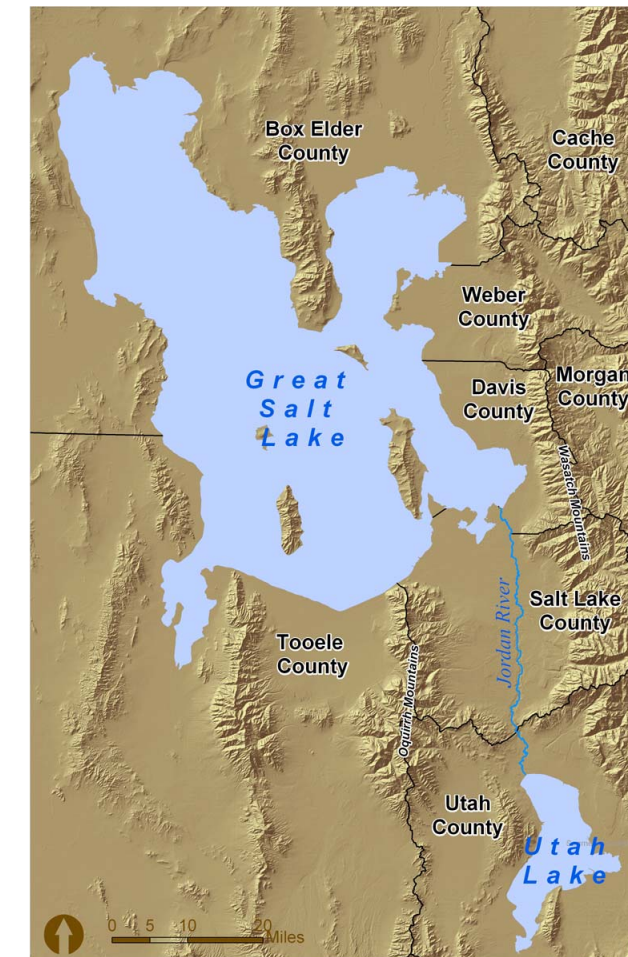


Figure 1.4 Jordan River Context Map

1.3 Purpose

The Jordan River is a complex environment in a highly urbanized area of Utah, and the demand for recreational uses along the corridor increases rapidly as its unique, remaining attributes become even more popular. This trails plan must find balance – ways to allow and even encourage people to interact with the river in a variety of positive ways, while at the same time preserving its remaining valued qualities. The trails plan must also make connections, eliminate barriers, close gaps, and provide a framework for implementation.

The intent of this plan is to provide comprehensive information to jurisdictional agencies to assist in their recreation planning and trails implementation efforts within the Jordan River Corridor.

Key objectives outlined in the project Request For Proposal include:

- Research, obtain, and evaluate all applicable information from the County and municipalities along the river but not limited to: project history, aerial photos, property plats, utility right-of-ways, easements, etc.;
- Assemble all completed planning documents, open space plans, and trail and park plans related to the Jordan River Trail; extract pertinent planning information; and include it in the overall planning document;
- Field verify and accurately document and map all existing trails, trailheads, parks, open space, and access points along the river;
- Clearly identify uncompleted segments of the regional trail and develop detailed plans, cost estimates, and land to be acquired to aid in completion of these gaps. This may include alternates for consideration;
- Develop a detailed plan for a water trail that would include but not be limited to such considerations as: navigability, hazards, access points, portage points, and cost estimates;
- Meet monthly or semi-monthly as necessary throughout the duration of the project with Salt Lake County Parks and Recreation staff and review team to discuss the status of the project progress, design problems and solutions, schedule, issues, etc.;
- During the planning process coordinate with and obtain input from appropriate agencies including but not limited to: Department of Natural Resources, Army Corps of Engineers, Salt Lake County Flood Control, cities, private property owners, utility companies, etc.;
- Prepare a Jordan River Trail Master Plan document which fully explains and illustrates all aspects of the existing and planned paved trail and water trail.

1.4 Planning Process

Analysis of Existing Conditions

The results of the existing conditions analysis is documented in Chapters 2 and 3. It includes mapping for all of the elements and features along the river corridor such as the width of trails, locations of benches, interpretive signing, launch ramps, and bridges, and any other fixed features on or adjacent to trails and the river. Photographs support the text and help establish a “snap shot” of conditions along the corridor.

Analysis of Existing Information

Several other planning studies and documents relating to the Jordan River were consulted and reviewed, and all of the cities along the river corridor were contacted and interviewed. This information is contained in the bibliography attached to the document. Of particular interest and value were trail studies completed by the South Valley Jordan River Trail Committee and the Jordan River Natural Area Foundation, as well as data sources provided by Salt Lake County.

Public Involvement in the Plan Process

The Jordan River Trail Master Plan Stakeholder Group was an invited group of representatives from all of the cities along the river corridor, special interest groups who are surface and water trail users, and representatives from County agencies. They met four times throughout the plan’s development and were a valuable source of first-hand information on trail conditions, issues, and concerns. They were also an enthusiastic group of individuals concerned about the river and its environment.

The Jordan River Trail Master Plan Stakeholder Group created a Vision Statement (below) that includes a list of goals, desired outcomes, and considerations that the Plan should address.

1. Close gaps and provide a continuous trail system.
2. Provide adequate public access to the Jordan River Trail, both surface and water.
3. Connect to other trail facilities and maximize shared use with other facilities.
4. Prioritize trail development and land acquisition.
5. Achieve a balance between recreation needs and the natural environment.
6. Preserve open spaces and natural areas.

The Stakeholder Group also felt it was important to think of the parkway as a whole entity, and the trail as one component; to plan for trail maintenance and develop costs for maintenance; to provide opportunities for multi-modal use; and to coordinate with the efforts of other river corridor planning activities.

Public Meetings

Once the Draft Plan was developed, it was presented at county-wide public meetings that took place in three locations in Salt Lake Valley. The meetings were held on November 8, 2007 at the Day-Riverside Library in Salt Lake City, on November 13, 2007 at the Draper Library, and on December 6, 2007 at the Salt Lake County Complex. Approximately 67 persons signed-in at the meetings where their comments were recorded and summarized. Comments and ideas were categorized and presented to the Stakeholder Group. Comments are summarized and included in the Appendix.

Project Web Page

An interactive web page was established and linked to Salt Lake County’s web page. It contained up-to-date information about the project including notes from the Stakeholder Group Meetings, maps, meeting announcements, and the Draft Plan. Numerous individuals used the website to comment on aspects of the plan, provide valuable information about



Figure 1.5 Project Web Page

specific segments or issues related to the plan, and to stay informed about the process.

Coordination with Other Planning Efforts

Several other Jordan River planning-related projects occurred simultaneously, each with a different specific focus. The plans include The Mayor’s Corridor Plan completed in conjunction with Envision Utah, the Watershed Stewardship Plan, Jordan River Total Maximum Daily Load Study, the County’s Open Space Master Plan, and the Jordan River Restoration Program. The focus of each is summarized below.

Blueprint Jordan River

The purpose of this plan is to be a broad, multi-topic effort covering land use and environmental issues associated with the Jordan River Corridor. It is being conducted by Envision Utah and Mayor Corroon, and is looking at economic development, land use, restoration, habitat, recreation, and other issues. It will highlight any inconsistencies in plans and tie together other plans into a comprehensive planning document. The Jordan River Trail Master Plan will be included in the Corridor Plan.

Water Quality Stewardship Plan

This three year study is a county-wide watershed planning effort that will be completed in 2008. Its purpose is to support and enhance watershed functions that include water quality, habitat, water conveyance systems, and social, recreation, and aesthetic values. It will establish an adaptive management system, integrate existing planning efforts, identify opportunities for collaboration, restoration, and improvement, assist in the procurement of funding, and enhance the quality of life for Salt Lake County residents. It will address economics, wastewater, water supply, stormwater, non-point pollution sources, in-stream flows, Utah Lake, and wildlife habitat. Additional information is available at www.waterresources.slco.org.

Jordan River Total Maximum Daily Load (JRTMDL) Study

Twice each year the Utah Division of Water Quality is required to monitor water quality in the state’s waterbodies, and identify those that do not meet state standards. Currently the Jordan River does not meet those standards. This study is designed to identify pollution sources and develop a plan for improving water quality in the river. The TMDL study will determine the maximum amount of pollution that the river can receive on a daily basis and still meet water quality standards. The schedule for completion is approximately 5 years in the future. Additional information is available at www.waterresources.slco.org.

Salt Lake County Open Space Master Plan

The Salt Lake County Open Space Committee which was formed under the County Mayor’s office wishes to complete an open space plan for the County. The Plan will develop a process for identifying critical open lands in the county. The Plan has not yet begun; however, it is anticipated that it will begin prior to the end of 2007.

Jordan River Restoration Programs

Several partnering organizations are involved in restoration projects along the Jordan River including Salt Lake County, Utah Reclamation Mitigation and Conservation Commission (URMCC), Great Salt Lake Audubon, Tree Utah, Division of Wildlife Resources, several cities along the corridor, and numerous other non-profit and government agencies with an interest in water quality, flood control, wildlife, and habitat. Some of the effort is coordinated through the U.S. Fish and Wildlife Service, which is working three sites along the river and Salt Lake County Public Works is working on at least nine sites in the corridor. These projects typically include bank stabilization, revegetation, and habitat restoration.

Salt Lake County Parks and Recreation website at: www.recreation.slco.org, under "Planning." For more information on this Master Plan, please contact Salt Lake County Parks and Recreation staff at (801) 468-2299.

1.5 About This Document

The layout of this Master Plan was designed to be easily used, allowing everyone, from government employees to interested individual citizens, to learn more about the Jordan River Trail System and encourage active participation in the current and future planning, restoration, and access development efforts in the corridor.

Following the brief overall background provided above, Section 2.0 provides greater detail on the principles that guided the decision-making process. Each one of the Vision Statements developed by the Stakeholder Group is discussed, and strategies for implementation are provided.

Section 3.0 explores the existing conditions, summarizes the issues and opportunities, and makes recommendations for the paved trails and equestrian trails for each of the twelve trail segments established for this project. Relevant photos and maps are included with each segment to assist with clarity and provide readers with visual references.

Urban water trail facilities are examined in Section 4.0. As with the paved trails, information on existing conditions, summaries of issues and opportunities, and recommendations are included for each of the twelve project segments, along with associated maps and photos.

Trail development standards are provided in Section 5.0, with detailed information, photos, and illustrations on elements such as paved trail design, water trail facility design, and signage for the corridor.

Implementation measures, policies, responsibilities, costs, and funding methods are explored in Section 6.0, in general, and broken down by each project segment. Strategies on *how* to implement the recommendations made in earlier sections are discussed.

A bibliography follows the document, as well as an appendix, which provides detailed information on the public participation process, including verbatim public comment received at the project open houses and throughout the project.

An electronic copy of this Master Plan can be downloaded from the

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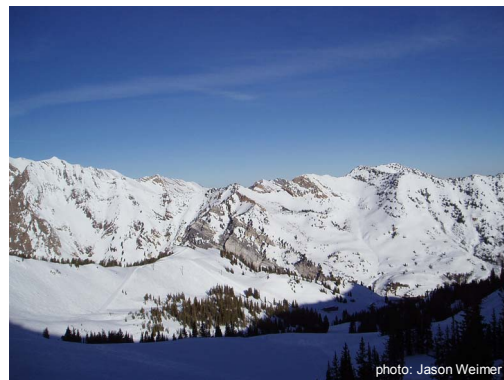


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This three year study is a county-wide watershed planning effort that will be completed in 2008. Its purpose is to support and enhance watershed functions that include water quality, habitat, water conveyance systems, and social, recreation, and aesthetic values. It will establish an adaptive management system, integrate existing planning efforts, identify opportunities for collaboration, restoration, and improvement, assist in the procurement of funding, and enhance the quality of life for Salt Lake County residents. It will address economics, wastewater, water supply, stormwater, non-point pollution sources, in-stream flows, Utah Lake, and wildlife habitat. Additional information is available at www.waterresources.slco.org.

Jordan River Total Maximum Daily Load (JRTMDL) Study

Twice each year the Utah Division of Water Quality is required to monitor water quality in the state’s waterbodies, and identify those that do not meet state standards. Currently the Jordan River does not meet those standards. This study is designed to identify pollution sources and develop a plan for improving water quality in the river. The TMDL study will determine the maximum amount of pollution that the river can receive on a daily basis and still meet water quality standards. The schedule for completion is approximately 5 years in the future. Additional information is available at www.waterresources.slco.org.

Salt Lake County Open Space Master Plan

The Salt Lake County Open Space Committee which was formed under the County Mayor’s office wishes to complete an open space plan for the County. The Plan will develop a process for identifying critical open lands in the county. The Plan has not yet begun; however, it is anticipated that it will begin prior to the end of 2007.

Jordan River Restoration Programs

Several partnering organizations are involved in restoration projects along the Jordan River including Salt Lake County, Utah Reclamation Mitigation and Conservation Commission (URMCC), Great Salt Lake Audubon, Tree Utah, Division of Wildlife Resources, several cities along the corridor, and numerous other non-profit and government agencies with an interest in water quality, flood control, wildlife, and habitat. Some of the effort is coordinated through the U.S. Fish and Wildlife Service, which is working three sites along the river and Salt Lake County Public Works is working on at least nine sites in the corridor. These projects typically include bank stabilization, revegetation, and habitat restoration.

Salt Lake County Parks and Recreation website at: www.recreation.slco.org, under "Planning." For more information on this Master Plan, please contact Salt Lake County Parks and Recreation staff at (801) 468-2299.

1.5 About This Document

The layout of this Master Plan was designed to be easily used, allowing everyone, from government employees to interested individual citizens, to learn more about the Jordan River Trail System and encourage active participation in the current and future planning, restoration, and access development efforts in the corridor.

Following the brief overall background provided above, Section 2.0 provides greater detail on the principles that guided the decision-making process. Each one of the Vision Statements developed by the Stakeholder Group is discussed, and strategies for implementation are provided.

Section 3.0 explores the existing conditions, summarizes the issues and opportunities, and makes recommendations for the paved trails and equestrian trails for each of the twelve trail segments established for this project. Relevant photos and maps are included with each segment to assist with clarity and provide readers with visual references.

Urban water trail facilities are examined in Section 4.0. As with the paved trails, information on existing conditions, summaries of issues and opportunities, and recommendations are included for each of the twelve project segments, along with associated maps and photos.

Trail development standards are provided in Section 5.0, with detailed information, photos, and illustrations on elements such as paved trail design, water trail facility design, and signage for the corridor.

Implementation measures, policies, responsibilities, costs, and funding methods are explored in Section 6.0, in general, and broken down by each project segment. Strategies on *how* to implement the recommendations made in earlier sections are discussed.

A bibliography follows the document, as well as an appendix, which provides detailed information on the public participation process, including verbatim public comment received at the project open houses and throughout the project.

An electronic copy of this Master Plan can be downloaded from the

2.0 Guiding Principles for the Trail Master Plan

2.1 Overview

In developing the Master Plan Recommendations, several guiding principles form the framework for decision-making.

- The Vision Statements developed by the Stakeholder Group, stated in Chapter 1.
- The desire to identify a clearly-defined Jordan River Trail, and to distinguish it from other trails and recreation opportunities in the river corridor.
- The desire to protect sensitive environments in the corridor from human impacts, and not to “over-build” trails and other facilities.
- The need to locate the trails outside or to the edge of the Jordan River meander corridor to avoid future trail damage caused by the shifting river.
- The exact alignment cannot always be determined in this master plan, therefore it will need to be determined and refined when development occurs.
- Connections to other city trails to locations east and west of the river are shown when known; major on-street bicycle lanes and routes need to be identified.

2.2 Vision Statements and Implementation Strategies

Vision Statement One: CLOSE GAPS TO PROVIDE A CONTINUOUS TRAIL SYSTEM

Several known gaps in the paved trail, equestrian trail, and river trail have been identified. Closing gaps will take coordination and cooperation with several local government agencies, private landowners, and trail users and planners.

Strategies:

- 1.1 Build on the existing relationships already in place to continue to find ways to implement the plan and close gaps.
- 1.2 Develop a continuous, paved trail throughout the entire river corridor.
- 1.3 Develop looped equestrian trails that begin and end at the same location, and provide facilities for loading and unloading horses.
- 1.4 Locate equestrian trails in areas that are less urbanized; equestrian trails throughout the entire corridor are not feasible or desirable.
- 1.5 Develop a continuous river trail that reduces or avoids hazards.

Vision Statement Two: PROVIDE ADEQUATE PUBLIC ACCESS TO THE JORDAN RIVER TRAIL, BOTH SURFACE AND WATER

Adequate public access is difficult to determine and is a balance between not enough and too much. By developing only official, Jordan River Trail access points at key locations throughout the corridor, it is possible to avoid the clutter of over-development.

Strategies:

- 2.1 Develop designated paved and equestrian trail heads and access points that provide direct access to the Jordan River Trail System.
- 2.2 Secondary paved and equestrian access points and trailheads that are outside of and separate from the Jordan River Trail System, and that include connecting trails to the Jordan River System are not considered part of the trail system, but are valuable neighborhood connections.
- 2.3 Develop designated launches and portages in protected, safe, locations that are accessible by a variety of boaters with differing skill levels.
- 2.4 Discourage private or unofficial water access points.

Vision Statement Three: CONNECT TO OTHER TRAIL FACILITIES AND MAXIMIZE SHARED USE WITH OTHER FACILITIES

The Plan strives to establish an official Jordan River Trail System and to designate it as such with consistent signing and markings. However, there are times when sharing parking, restrooms, and other support facilities with adjacent development in parks and public areas is desirable and cost-effective.

Strategies:

- 3.1 When shared-use opportunities occur, the Jordan River Trail System components should be clearly identified so that there is no confusion about the location of trails, where users are welcome, or how the system functions.
- 3.2 Develop designated paved and equestrian trail heads and access points that provide direct access to the Jordan River Trail System.
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Vision Statement Four: PRIORITIZE TRAIL DEVELOPMENT AND LAND ACQUISITION

In order to implement the Plan, priorities will need to be established so that resources can be expended where they are needed most. This Jordan River Trail Master Plan will identify categories of ownership when it is relevant to closing gaps and completing trails. Other on-going plans relating to the Jordan River will address more specific issues of restoration, open land acquisition, maintaining and improving

water quality, and other issues that have an impact on the quality of the experience along the trail system, but are outside the purview of this plan.

Strategies:

- 4.1 The highest priority is on closing gaps in the paved trail to achieve the desire for a continuous system, and eliminating or avoiding unsafe conditions.
- 4.2 Other priorities include completing usable, ride-able, enjoyable equestrian trails and facilities; completing useable, navigable, and enjoyable water facilities; enhancing the environment along the trails; and providing diverse recreation opportunities for residents of Salt Lake County.

Vision Statement Five: ACHIEVE A BALANCE BETWEEN RECREATION NEEDS AND THE NATURAL ENVIRONMENT

This again is an issue of how much to do and when to stop. Some stretches of the river are highly urbanized where evidence of human interaction is immediate and obvious while other stretches still contain qualities that are natural-appearing or rural. Ideally, the Jordan River Trail corridor should contain a variety of experiences to appeal to a broad range of interests.

Strategies:

- 5.1 Identify key sections of the river corridor that are currently natural-appearing or rural in character, and designate that character as the desired future condition.
- 5.2 Refrain from developing facilities that are immediately adjacent to the river, with the exception of the designated river access points. Bridges in particular, cross the river and degrade the experience of boaters and rafters on the river.

Vision Statement Six: PRESERVE OPEN SPACES AND NATURAL AREAS

Natural areas and open spaces that provide the variety of experience mentioned before are essential to the ultimate trail system. Important natural areas and open space should be identified. The identification of open spaces for preservation is an element of this plan; however, specific planning efforts are underway to identify and prioritize open land acquisition.

Strategies:

- 6.1 Identify areas where a natural-appearing or rural experience is desired and identify land ownership.
- 6.2 Coordinate with other planning efforts to identify and preserve open lands and natural areas along the Jordan River Trail corridor.

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Strategies:

- 6.1 Identify areas where a natural-appearing or rural experience is desired and identify land ownership.
- 6.2 Coordinate with other planning efforts to identify and preserve open lands and natural areas along the Jordan River Trail corridor.

3.0 Paved and Equestrian Trail

Several issues were identified at the outset of this project in meetings with Salt Lake County staff members, and more were recognized during field surveys, from meetings with the stakeholder group, and through the analysis of existing conditions. Many pertain to the corridor as a whole, while others pertain only to specific sections of the trail corridor.

Issues relating to the paved and equestrian trail corridor as a whole are discussed below and include references for more detailed information to be found in this planning document. Following the general corridor issues, detailed existing conditions, issues and opportunities, and recommendations are explored for each of the twelve corridor segments in sections 3.1 through 3.12 of this chapter.

Overall Issues, Opportunities and Recommendations

Trail Gaps

Discontinuous trail segments are the primary factor preventing trail users from enjoying the full spectrum of recreation activities offered along the Jordan River Trail. Trail users that are not familiar with the location of these gaps set out to explore the river corridor, only to find that their trip is cut short by the end of the pavement. There are currently eleven gaps in the Jordan River Trail corridor, as shown on the “Existing Paved and Equestrian Trail Gaps Map” following this section. Some of these gaps are for paved trails, some for equestrian trails, and others represent gaps in both types of trails in a particular segment.

One of the most important keys to improving the functional level of the trail corridor as a whole is to fill in these missing trail gaps, in accordance with the trail standards established in Chapter 5. As this planning process has been underway, several municipalities have been working to close some of the gaps in their areas by the end of 2007. Detailed information about each gap and the current status is provided in sections 3.1 through 3.12 in this chapter.

Signage

The existing signage throughout the Jordan River Trail Corridor currently represents a hodgepodge of designs, information content, and purposes, and presents trail users with incomplete, and sometimes confusing, information about the trail facilities. Mileage markers are sporadic and inconsistent from city to city, and the trail corridor lacks consistent location reference signs for information and emergency purposes.

The Jordan River Parkway Foundation in conjunction with Salt Lake County, recently developed a standard trailhead sign, which has been installed at several trailheads along the river. This plan recommends the implementation of a comprehensive signage program to improve the function level and safety of the trail. Detailed recommendations can be found in section 5.4 of this plan.

Trail Maps

The lack of a comprehensive, easy-to-read, readily available map prevents many trail users from traveling beyond their familiar areas and exploring other portions of the Jordan River Trail corridor. As discussed in Chapter 5, a comprehensive trail map should be incorporated as part of the Salt Lake County standard trailhead sign design. In addition, the County should make mapping available online, and in a printed format that paved trail users can utilize to plan trips, and to take with them on their visits to the Jordan River. These maps could take the form of a guide booklet or large foldout map, and could include information on the history of the river, flora and fauna found along the river, and cultural information. These printed guides should be made available online and at County facilities throughout the Salt Lake Valley. An excellent example is the “Willamette River Water Trail Guide,” which can be viewed and downloaded at www.willamettewatertrail.org.

Maintenance

Another major factor that can affect the functional level of the Jordan River Trail corridor is maintenance. Proper trail maintenance must be provided or the functionality of facilities is negatively impacted. An example of a common problem is the difficulty cyclists have with puncture vines. If they are allowed to thrive, the small goat head shaped seeds cause flat tires, sometimes even in tires protected with tire liners.

When trails are installed according to the trail design standards described in Chapter 5.0, proper trail maintenance becomes easier and more cost-effective.

In addition, simple maintenance measures such as keeping trash cans emptied and dog waste station bags refilled encourages trail users to take advantage of these features, which in turn, helps keep the trail corridor clean and reduces cleanup efforts required by maintenance crews.

Cycling Design Requirements

Thoughtful design and location of curb cuts for cyclists is needed, especially when the trail dead-ends into a parking area (as in Germania Park and Walden Park, Segment 7). In some locations, the trail enters a park and merges with a sidewalk. Sometimes these areas present cyclists with difficult navigation through bollards with immediate sharp 90 degree turns, and no curb cuts straight ahead if cyclists can't make the turn. The official trail through parks can sometimes be unclear, leaving cyclists to roam sidewalks, picnic areas, and parking lots to find where the trail continues on the other side of the park.

Curb cuts need to be included where the trail intersects parks, trailheads, and streets. In addition, directional signage should be included as part of the signage program in section 5.4 to assist cyclists with safe navigation along the trail.

Equestrian Trail Design

Equestrians are less common, but equally important, users of the Jordan River Trail corridor, and special considerations must be applied when designing, implementing, and maintaining trails for equestrian use.

Due to equipment and transportation needs, equestrian trails should be loops wherever possible so that riders can travel back to the original location of their horse trailers. In addition, these trails should be separated from the paved trail by as much distance as possible to minimize user conflicts presented by the higher-traffic paved trail.

Chapter 5 provides specific standards for equestrian trail design and maintenance within the river corridor.

Flooding & Erosion Issues

When Utah Lake reaches a set high water level known as its “compromise level,” more water is released into the Jordan River. This is usually the peak flow of the river, and presents flooding issues at many underpasses throughout the trail corridor, and increases bank erosion at several locations in the river corridor.

It is the existing policy of Salt Lake County that portions of the trail that are covered due to flooding are closed, and official trail detours are not provided. This plan recommends the continuation of that policy and recommends that all areas subject to flooding are signed in accordance with the Signage Program discussed in Chapter 5. Specific underpasses that are subject to flooding are addressed in sections 3.1 through 3.12 of this chapter.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.












The issue of stream bank erosion presents serious challenges to Salt Lake County, and adds significant expense due to the need for stream bank stabilization and trail relocation. This plan recommends that all new trail connections be established outside of the official meander corridor (the zone within which the river channel may be expected to migrate within the next 100 years) wherever possible, and that, where there is an opportunity to move a trail further away from the river, the County should pursue this effort.

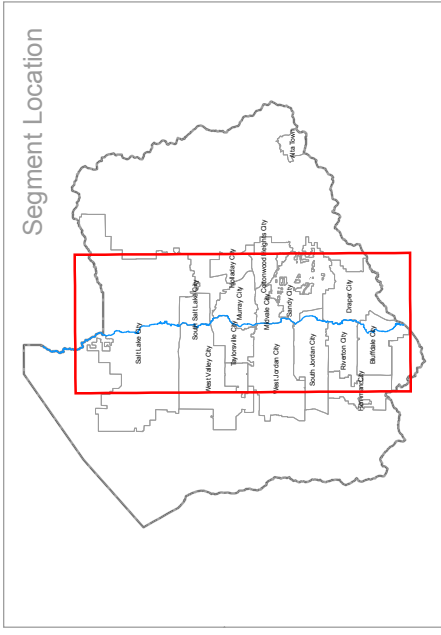
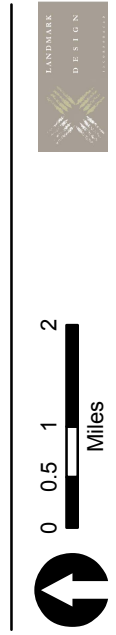
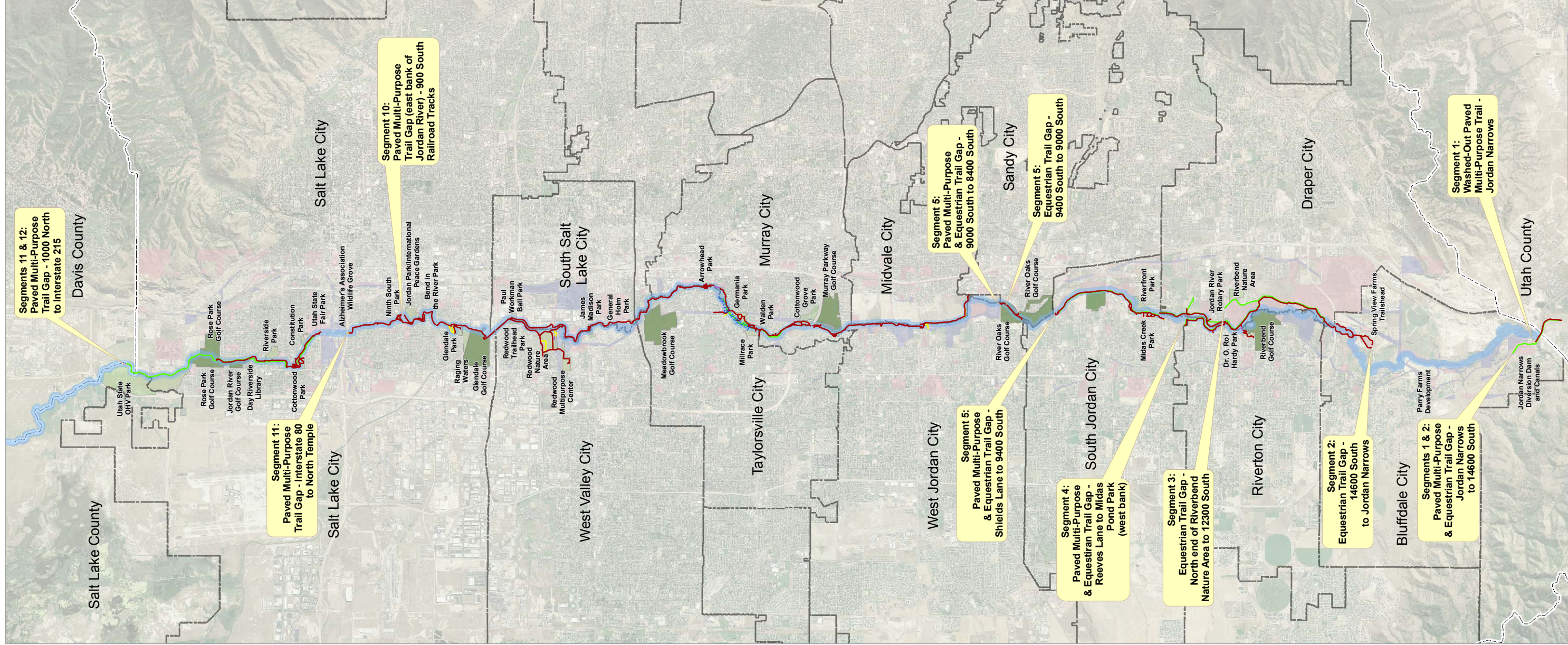
Trail Access

Convenient access to the Jordan River Trail from surrounding residential areas and businesses is key to increasing trail use. Increased trail use raises the fitness level of County residents, improves the safety of trail facilities by adding more “eyes” to the corridor, and encourages more concern by the public for the condition of the Jordan River corridor. When jurisdictions are reviewing development plans for areas near the river, access to the Jordan River Trail should be a key requirement for development approval.

Jordan River Trail Master Plan

Trail Gaps

-  Existing Paved Jordan River Trail
-  Existing Unpaved Jordan River Trail
-  Existing Jordan River Equestrian Trail
-  Existing Jordan River Boardwalk
-  Jordan River
-  County Boundary
-  City Boundary
-  Golf Course
-  Park
-  Publicly Owned Land
-  Utility Owned Land



3.1 Segment 1: Bluffdale (Southern County Line to 15400 South)

Existing Conditions

The Salt Lake County portion of the Jordan River Trail begins at the southern county line in Bluffdale, Utah, east of the Utah Veteran's Memorial Park. The asphalt trail continues north and west from Utah County, overlooking the Jordan Narrows Diversion Dam and Canal area, and is paralleled along the south side by a narrow equestrian trail. An interpretive sign along the path explains the history of and need for the Jordan Narrows Diversion Dam and Canals.



Figure 3.1.1 Salt Lake/Utah County line, looking southeast towards Utah County

The trail continues for a short distance, where the asphalt paving ends at the top of a steep hill. An unpaved path continues at this point, traveling down the slope and curving northward. Beef Hollow to the west provides a view of structures in the Utah Veteran's Memorial Park from this portion of the trail. The asphalt trail picks up again for a short distance, ending at an informal, unpaved trailhead located on the south side of Jordan

Narrows Road. There is a small, unpaved area on the north side of the road with room for approximately 3 vehicles. This is the end of the official trail in this segment;

however, an unofficial equestrian trail continues from the unpaved parking area north of the trailhead, winding up a short, steep hill, crossing Sunrise Riding Stable Street, and terminating at the Sunrise Riding Stables.



Figure 3.1.2 Interpretive signage: Jordan Narrows Diversion Dam and Canals

The northern portion of this segment does not have developed trails or access to the river. New development on the west side of the river comes relatively close to the river and the canal that parallels it, but there is no access to the river.

The Porter Rockwell portion of the Mountain View Corridor Highway may pass through this segment at some point in the future, but planning is just in the beginning stages. If the Corridor passes through this area in the future, it would require a new bridge crossing over the Jordan River.



Figure 3.1.3 View to northwest, overlooking Jordan Narrows Diversion Dam and Canals



Figure 3.1.4 Equestrian trail paralleling asphalt trail above Jordan Narrows



Figure 3.1.5 Top of steep slope where asphalt trail begins again



Figure 3.1.6 View from unpaved trail through ravine towards Utah State Veteran's Memorial Park



Figure 3.1.7 Asphalt trail, looking north

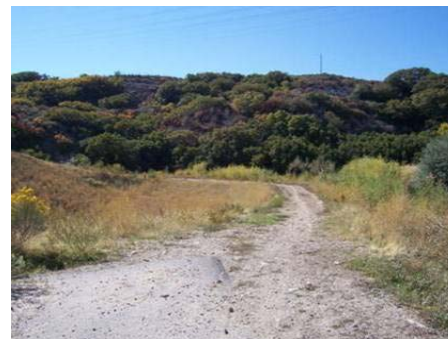


Figure 3.1.8 Asphalt trail ends - narrow unpaved trail continues up steep slope



Figure 3.1.9 Jordan Narrows



Figure 3.1.10 Parking north of Trailhead



Figure 3.1.11 Horse Crossing Sign near Sunrise Riding Stable Street



Figure 3.1.12 Jordan River paralleled by canal in northern portion of Segment 1

Issues and Opportunities

- There is a short gap in the paved trail from a mudslide. This segment is very steep.
- No formal parking or trailhead exists at Jordan Narrows.
- The paved trail has a gap between Jordan Narrows Road and 14600 South at the Spring View Farms Trailhead in Segment 2.
- The equestrian trail ends near the Sunrise Riding Stables, with the next access point just south of Bangerter Highway in Segment 2.
- It is not desirable to have trail users accessing the Jordan Narrows Diversion Dam and Canals area.

Recommendations: Paved Trail

3.1 A Mudslide Damaged Trail Segment

The short, steep section of trail that was damaged by a mudslide is scheduled to be repaired by spring of 2008 by Salt Lake County and Bluffdale City.

3.1 B Jordan Narrows Trailhead

Salt Lake County currently has a contract in place to plan and design a trailhead park at Jordan Narrows, with construction beginning Spring 2008. This facility will feature restrooms, and paved and unpaved parking to accommodate both paved and equestrian trail users. A future phase of this trailhead should add more accommodations for equestrian users.

3.1 C Trail Gap: Jordan Narrows Road to South End of Parry Farms Development

Develop a paved trail along the Utah Lake Distributing Canal from the future Jordan Narrows trailhead for approximately 1.5 miles to Wood Hollow natural drainage. Follow the drainage east to the southern edge of the subdivision, just east of where Packsaddle Drive ends.

3.1 D Trail Gap: South end of Parry Farms Development to Future Bridge Crossing

From the south end of the Parry Farms Development, the trail should continue north for 1/2 mile through Salt Lake County land and privately

JORDAN RIVER TRAIL MASTER PLAN

owned land to cross the South Jordan Canal at the location of an existing bridge abutment developed as part of the Parry Farms residential development. This section will require an easement from Rocky Mountain Power Company (Listed as Utah Power and Light in the Parcel Data). At this location a new bridge for pedestrians and equestrians will be required to cross the river and connect to the East Jordan Canal right-of-way on the east side of the river. This section of trail is currently under design by Salt Lake County, in association with Recommendation 3.2 A in the next section.

The Parry Farms Development has a proposed trail which loops around the property and ties into the Jordan River Trail corridor at the bridges.

These paved trail alignments will connect to future trails east of the Jordan River. These trails will be required as part of the development approvals in Bluffdale or will be developed by Bluffdale City with grant monies.

3.1 E Trail Gap Alternative: Jordan Narrows Road to North End of Parry Farms Development

An alternative paved trail route could take the form of an easement directly adjacent to the canal, but not on the canal right-of-way. This trail would parallel the Utah Lake Distributing Canal from the future Jordan Narrows trailhead for approximately 1.5 miles to Wood Hollow natural drainage. The trail would then follow the drainage east to the southern edge of the subdivision, just east of where Packsaddle Drive ends, and continue to the north end of the subdivision, where Iron Horse Boulevard ends.

Recommendations: Equestrian Trail

3.1 F Equestrian Trail Gap: Sunrise Riding Stables to Parry Farms
Salt Lake County intends to develop an equestrian trail from the future Jordan Narrows Trailhead to Parry Farms in a future phase. The separated equestrian trail should begin at the future Jordan Narrows Trailhead, and could meander down the hillside through the Utah and Salt Lake Canal Company property, to the Utah and Salt Lake Canal road. At this point a fence would be needed to keep equestrian users and others from going south on the canal road toward the Jordan Narrows Diversion Dam.

The trail would then follow the Utah and Salt Lake Canal north to where it crosses the Wood Hollow drainage, and continue north to eventually intersect at the South Jordan Canal and Jordan River crossings. The equestrian trail would connect to the future equestrian trail to be developed east of the Jordan River. Implementing this trail segment will require negotiation with Rocky Mountain Power and the Canal Companies.

3.1 G Equestrian Trail Gap Alternative (and small portion of Paved Trail): Jordan Narrows to North End of Parry Farms Development

An alternative equestrian trail route could take the form of an easement directly adjacent to the canal, but not on the canal right-of-

way. The trail would parallel the Utah and Salt Lake Canal east of the Sunrise Riding Stables near the river, remaining on the west side of the canal. It would continue north, crossing the Wood Hollow drainage, past the subdivision. As it nears the northern end of the subdivision, there is also a possibility to align the paved and equestrian trails closely along this alternative route to avoid the canal right-of-way, to the north end of the subdivision.

3.1 H Gate Across Canal Road

Implementing recommendation 3.1 C will require a gate across the canal road to keep equestrian trail users from traveling south on the canal road.

3.1 I Equestrian Bridge Across Utah and Salt Lake Canal

Implementing recommendation 3.1 C will require an equestrian bridge across the Utah and Salt Lake Canal.

3.1 J Bridge Across Utah Lake Distribution Canal

Implementing 3.1D and 3.1 F will require a pedestrian/equestrian bridge across the Utah Lake Distribution canal at the north end of the Parry Farms development.

3.1 K Bridge Across Jordan River

Implementing 3.1D and 3.1 F will require a pedestrian/equestrian bridge across the Utah Lake Distribution canal at the north end of the Parry Farms development.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.1 A	Funded
Trailhead	3.1 B	Funded
Paved Trail	3.1 C	\$178,200
Paved Trail	3.1 D	\$415,800
Paved Trail	3.1 E	\$308,880
Equestrian Trail	3.1 F	\$121,440
Equestrian/Paved Trail	3.1 G	\$113,520
Gate	3.1 H	\$2,000
Bridge	3.1 I	\$150,000
Bridge	3.1 J	\$150,000
Bridge	3.1 K	\$250,000

Ownership

Ownership for recommended trail alignments is shown in the maps below, followed by detailed ownership information in tables.

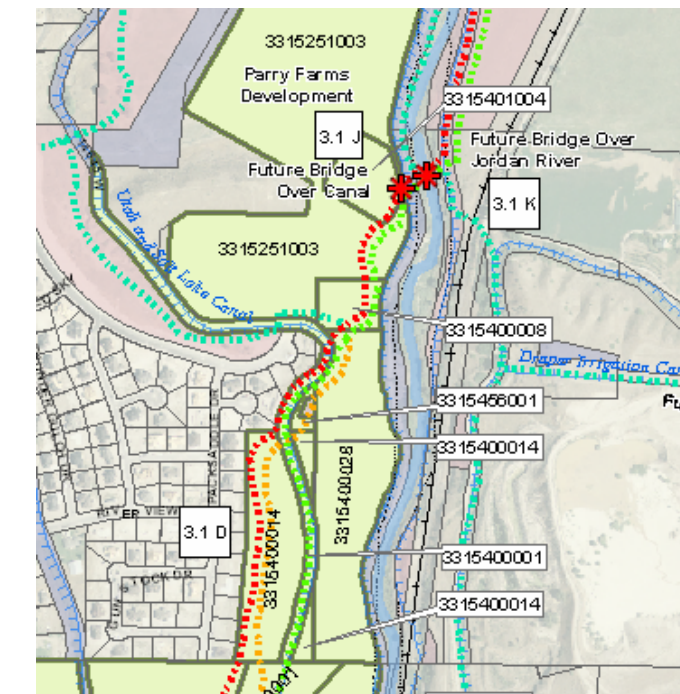


Figure 3.1.13 Ownership: Parry Farms to Future Bridge Crossing

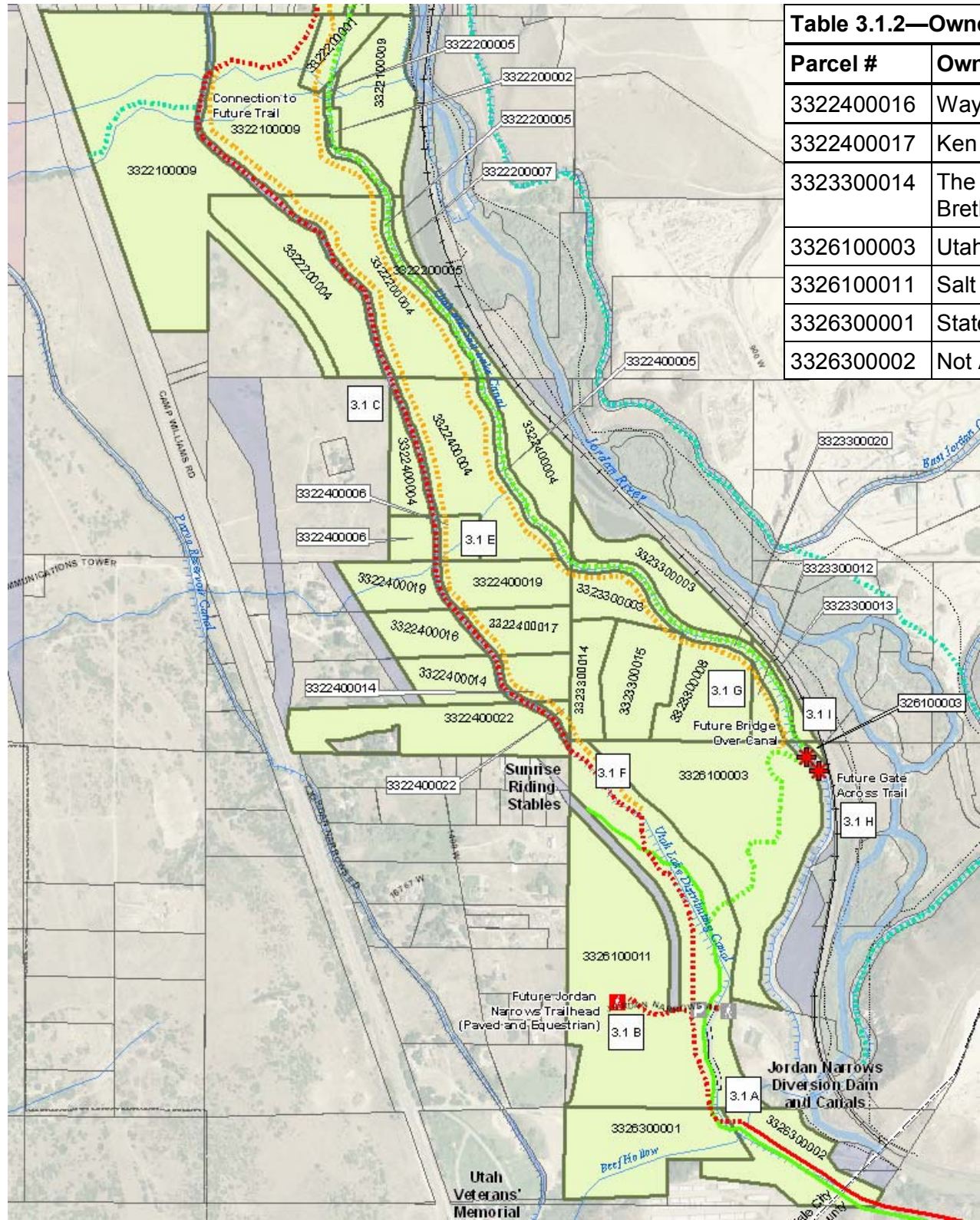


Figure 3.1.14 Ownership: Jordan Narrows to Parry Farms

Table 3.1.2—Ownership: Recommendation 3.1 C—Paved

Parcel #	Owner	Acres
3322400016	Wayne Workman	5.0
3322400017	Ken Rozema	5.3
3323300014	The Communities of the Apostolic Brethren	5.0
3326100003	Utah and Salt Lake Canal Company	4.5
3326100011	Salt Lake County	57.3
3326300001	State Armory Board	15.4
3326300002	Not Available	11.4

Table 3.1.2—Ownership: Recommendation 3.1 C—cont'd

Parcel #	Owner	Acres
3322100009 - east	LLC RAAATS	23.7
3322100009 - west	LLC RAAATS	47.3
3322200004 - east	Utah and Salt Lake Canal Company	15.1
3322200004 - west	Utah and Salt Lake Canal Company	14.3
3322400004 - east	Utah and Salt Lake Canal Company	16.3
3322400004 - west	Utah and Salt Lake Canal Company	3.7
3322400006 - east	Kay Schroeder	7.0
3322400006 - west	Kay Schroeder	2.4
3322400014 - east	J. Harvey	3.6
3322400014 - west	J. Harvey	5.0
3322400019 - east	Wayne Mortimer	7.0
3322400019 - west	Wayne Mortimer	5.2
3322400022 - east	Donna Gibson	0.6
3322400022 - west	Donna Gibson	14.2

Table 3.1.3—Ownership: Recommendation 3.1 D—Paved

Parcel #	Owner	Acres
3315251003	Parry Farms LLC	50.9
3315400001	Utah and Salt Lake Canal Company	5.0
3315400008	Utah Power and Light	1.8
3315400028	Telluride Power Company	12.0
3315401004	R & C Parry Family Properties	2.6
3315456001	Bluffdale City	0.4
3315400014 - northeast	Salt Lake County	0.6
3315400014 - southeast	Salt Lake County	0.5
3315400014 - west	Salt Lake County	6.7

JORDAN RIVER TRAIL MASTER PLAN

Table 3.1.4—Ownership: Recommendation 3.1 E—Alternative

Parcel #	Owner	Acres
3322400016	Wayne Workman	5.0
3322400017	Ken Rozema	5.3
3323300014	The Communities of the Apostolic Brethren	5.0
3326100003	Utah and Salt Lake Canal Company	4.5
3326100011	Salt Lake County	57.3
3322100009 - east	LLC RAAATS	23.7
3322100009 - west	LLC RAAATS	47.3
3322200004 - east	Utah and Salt Lake Canal Company	15.1
3322200004 - west	Utah and Salt Lake Canal Company	14.3
3322400004 - east	Utah and Salt Lake Canal Company	16.3
3322400004 - west	Utah and Salt Lake Canal Company	3.7
3322400006 - east	Kay Schroeder	7.0
3322400006 - west	Kay Schroeder	2.4
3322400014 - east	J. Harvey	3.6
3322400014 - west	J. Harvey	5.0
3322400019 - east	Wayne Mortimer	7.0
3322400019 - west	Wayne Mortimer	5.2
3322400022 - east	Donna Gibson	0.6
3322400022 - west	Donna Gibson	14.2

Table 3.1.5—Ownership: Recommendation 3.1 F—Equestrian

Parcel #	Owner	Acres
3322200001	Utah and Salt Lake Canal Company	4.1
3322200002	Utah and Salt Lake Canal Company	0.6
3322200004	L. Yates	15.1
3322200007	Geneva Rock Products	2.1
3322400005	Utah and Salt Lake Canal Company	3.1
3322400019	Wayne Mortimer	7.1
3323300008	Corporation of the Presiding Elder of the Apostolic United Brethren	9.8
3323300012	Corporation of the Presiding Elder of the Apostolic United Brethren	1.9
3323300013	B & B Resources, Inc.	1.3
3323300015	Corporation of the Presiding Elder of the Apostolic United Brethren	8.9
3323300020	Utah and Salt Lake Canal Company	4.6
3326100011	Salt Lake County	57.3
3322100009 - east	LLC RAAATS	11.5
3322100009 - west	LLC RAAATS	23.7
3322200005 - north	Utah and Salt Lake Canal Company	0.4
3322200005 - south	Utah and Salt Lake Canal Company	3.6
3322400004 - east	Utah and Salt Lake Canal Company	7.6
3322400004 - west	Utah and Salt Lake Canal Company	16.3
3323300003 - east	Utah and Salt Lake Canal Company	7.8
3323300003 - west	Utah and Salt Lake Canal Company	4.6
3326100003 - east	Utah and Salt Lake Canal Company	0.2
3326100003 - west	Utah and Salt Lake Canal Company	31.4

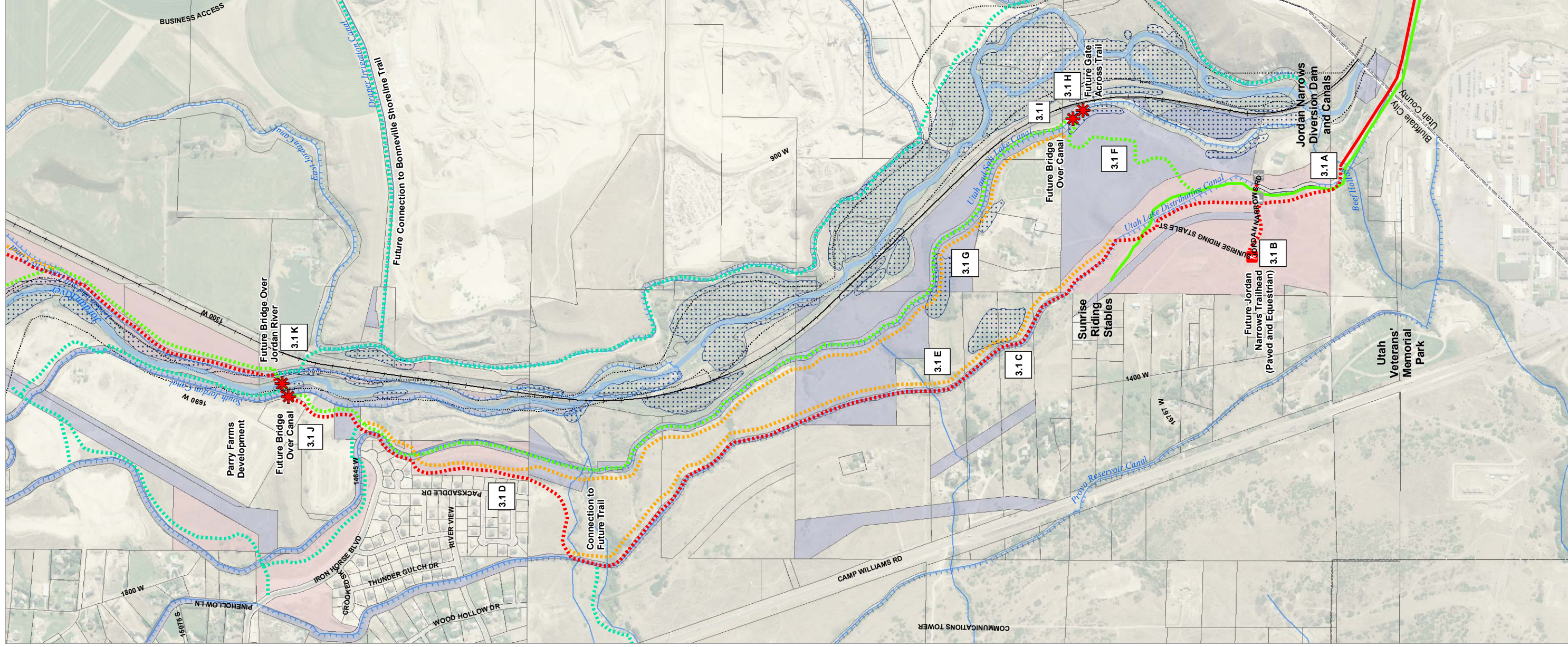
Table 3.1.5—Ownership: Recommendation 3.1 G—Alternative

Parcel #	Owner	Acres
3315400001	Utah and Salt Lake Canal Company	5
3315400028	Telluride Power Company	12
3315456001	Bluffdale City	0.4
3322200001	Utah and Salt Lake Canal Company	4.1
3322200002	Utah and Salt Lake Canal Company	0.6
3322200004	L. Yates	15.1
3322200007	Geneva Rock Products	2.1
3322400005	Utah and Salt Lake Canal Company	3.1
3322400019	Wayne Mortimer	7.1
3323300008	Corporation of the Presiding Elder of the Apostolic United Brethren	9.8
3323300012	Corporation of the Presiding Elder of the Apostolic United Brethren	1.9
3323300013	B & B Resources, Inc.	1.3
3323300015	Corporation of the Presiding Elder of the Apostolic United Brethren	8.9
3323300020	Utah and Salt Lake Canal Company	4.6
3326100011	Salt Lake County	57.3
3315400014 - northeast	Salt Lake County	0.6
3315400014 - southeast	Salt Lake County	0.5
3315400014 - west	Salt Lake County	6.7
3322100009 - east	LLC RAAATS	11.5
3322100009 - west	LLC RAAATS	23.7
3322200005 - north	Utah and Salt Lake Canal Company	0.4
3322200005 - south	Utah and Salt Lake Canal Company	3.6
3322400004 - east	Utah and Salt Lake Canal Company	7.6
3322400004 - west	Utah and Salt Lake Canal Company	16.3
3323300003 - east	Utah and Salt Lake Canal Company	7.8
3323300003 - west	Utah and Salt Lake Canal Company	4.6
3326100003 - east	Utah and Salt Lake Canal Company	0.2
3326100003 - west	Utah and Salt Lake Canal Company	31.4

Jordan River Trail Master Plan

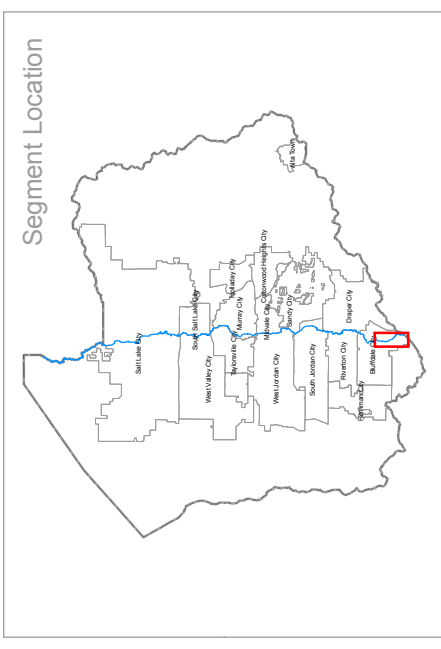
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Trail - Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 1: Bluffdale

Southern County Line to Business Access Road



JORDAN RIVER TRAIL MASTER PLAN

3.2 Segment 2: Bluffdale/Riverton (15400 South to Bangerter Highway)

Existing Conditions

The southern portion of this trail segment lacks paved trail facilities. Access is provided at the Spring View Farms Trailhead, located on 14600 South in Bluffdale. Two trails leave the trailhead parking area. The Jordan River Trail, accessed from the northwest corner of the parking lot, crosses a small footbridge and bends to the northeast.

Spring View Farms Trailhead

- Trailhead Sign & Map
- Restrooms
- Trash Receptacles
- Parking (24 standard, 2 handicapped)



Figure 3.2.1 Spring View Farms Trailhead

There is a paved private access trail to Loomis Village connected to this segment of the trail. The Spring View Farms Trail departs from the eastern edge of the parking lot and turns north, following Spring View Trail. It crosses a small footbridge, and meets up with the Jordan River Trail.

The trail spans another small footbridge and then crosses Peacock Midge Drive before it turns north toward the Jordan River. The trail splits again near a small pond. The Madison River Trail follows the eastern shore of the

Jordan River, ending in less than a quarter of a mile. The Jordan River Trail crosses the Jordan River via a large pedestrian bridge (Bridge B1), and winds its way north, crossing a small footbridge. The trail connects to the other side of Bangerter Highway with a pedestrian underpass. Users report that in times of high water, the underpass is not useable.

The paved Jordan River Trail continues north from the Bangerter Highway underpass and crosses to the east side of the river over a large pedestrian bridge (Bridge B2). The Arrow Trailhead is located west of this bridge, but is not yet connected to the trail corridor. This trailhead is located on 1300 West at approximately 13800 South, and has a gravel parking lot intended for equestrian use and a short segment of trail that does not connect to the Jordan River Trail corridor at this time.



Figure 3.2.2 Bridge B1
10' wide, 100' span, 6' clearance



Figure 3.2.3 Spring View Farms Trail access from trailhead parking lot



Figure 3.2.10 Loomis Village Private Access to trail



Figure 3.2.11 Footbridge along Spring View Farms Trail



Figure 3.2.4 Spring View Farms Trail signage and fence detail



Figure 3.2.5 Jordan River Trail access from trailhead parking lot



Figure 3.2.12 Footbridge along Jordan River Trail



Figure 3.2.13 Small stream crossed by footbridge in Figure 2.11



Figure 3.2.6 Footbridge along Jordan River Trail



Figure 3.2.7 Bench along Jordan River Trail



Figure 3.2.14 Trail looking north along Spring View Trail



Figure 3.2.15 Trail Crossing at Peacock Midge Drive



Figure 3.2.8 Wetlands



Figure 3.2.9 Jordan River Trail



Figure 3.2.16 Wetlands north of Peacock Midge Drive



Figure 3.2.17 Madison River Trail



Figure 3.2.18 View to southeast from Bridge B1



Figure 3.2.19 Footbridge south of Bangerter Highway



Figure 3.2.20 View looking south at footbridge (Figure 2.19)



Figure 3.2.21 Bench overlooking river



Figure 3.2.22 Bangerter Highway Underpass

Issues and Opportunities

- The paved trail has a gap between Jordan Narrows Road in Segment 1 and 14600 South at the Spring View Farms Trailhead. An alignment needs to be identified.
- The equestrian trail ends near the Sunrise Riding Stables in Segment 1, with the next access point just south of Bangerter Highway.
- There is flooding at the Bangerter Highway underpass during peak river flow. The Arrow equestrian trailhead needs to be completed.
- No connection is currently provided to the Arrowhead equestrian trailhead on 1300 West. This trailhead was given

state funding with the belief that equestrian users would be able to head north *and* south on the Jordan River trail.

- Equestrian users would like to see the equestrian trail continue south through the Salt Lake County owned parcel west of the Jordan River.
- The equestrian trail south of Bangerter Highway needs to be identified with signage.

Recommendations: Paved Trail

3.2 A Trail Gap: Parry Farms Development to Spring View Farms

Salt Lake County currently has hired a consultant to plan and design a paved trail from Parry Farms development north to the existing Spring View Farms trailhead. Construction is scheduled to begin Spring 2008. The trail will travel north through the railroad property, then follow 1300 West to 14600 South, where a new street crossing will be installed to connect with the existing Spring View Farms Trail corridor.

3.2 B Flooding at Bangerter Highway Underpass

These areas are subject to periodic flooding during period of peak runoff and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.2 C Arrow Trailhead and Paved Trail

Complete Development of Trailhead at Arrow Trailhead for paved and equestrian trail users. Develop a trail connection from the Trailhead to the existing trails on the west side of the river. The trail would proceed east, through Salt Lake County owned land, through a small section of private land, and through Salt Lake Valley Sewer District land, connecting with the existing trails west of Bridge B2.

Recommendations: Equestrian Trail

3.2 D Equestrian Trail Gap: Parry Farms to Bridge B2

Equestrian trails are not part of the current trail development project between Parry Farms and 14600 South. The County plans on developing an equestrian trail from Parry Farms (shown on Segment 1 map) north to 14600 South in a future phase. This equestrian trail would begin at the future Parry Farms bridge, following north through the railroad property, and then staying closer to the river, following the east bank of the river as it bends to the west and then north again to 14600 South.

The trail would cross 14600 South at a new Street Crossing, continue north around an existing wetland, then east to the Future Equestrian Bridge Crossing over the Jordan River which takes equestrians to the County-owned land on the north side of the Jordan River. The trail would follow the northern boundary of this property, and trace through the County owned strip, eventually meeting up again with the paved trail at the footbridge just south of Bangerter Highway. The equestrian

trail would then parallel the paved trail, with as much separation as possible, pass under Bangerter Highway, and parallel the paved trail to Bridge B2.

A connecting spur to join the existing Arrowhead Equestrian trailhead to the Jordan River Trail corridor is described below in 3.2 E.

Negotiations with private property owners will be required, but the connection is important for a high level of trail corridor functionality.

All new equestrian trails need to be signed according to the signage program discussed in Chapter 5 and properly maintained.

3.2 E Arrow Equestrian Trailhead

Complete development of the Trailhead at Arrow Trailhead as noted in 3.2 C above, and develop an equestrian trail connection to the existing equestrian trail on the west side of the river. The trail would proceed east through County-owned land, a small segment of private land, and South Valley Sewer District land, connecting with the existing equestrian trail west of Bridge B2.

Negotiations with utilities and private property owners will be required, but the connection is important for a high level of trail corridor functionality.

All new equestrian trails need to be signed according to the signage program discussed in Chapter 5 and properly maintained.

3.2 F Equestrian Trail Gap Alternative: Parry Farms to Bridge B2

An alternative equestrian route could continue from the future equestrian trail and bridges at Parry Farms north through the railroad property, along the west side of 1300 West to 14600 South. Equestrian trail users would then use the future 14600 South street crossing that is part of the County's current trail development work in this section. Equestrians would travel north, along the west side of Spring View Farms, then follow the river west to a proposed equestrian bridge crossing, which takes equestrians to the County-owned land on the north side of the Jordan River. The trail would then continue as described above in Section 3.2 D.

3.2 G Equestrian Trail Gap Alternative: Parry Farms to Bridge B2

Another alternative route for this equestrian trail section could begin at the future Parry Farms bridge, following north through the railroad property, and then staying closer to the river, following the east bank of the river as it bends to the west and then north again to 14600 South, as in 3.2 D above. At 14600 South, the trail would travel west closer to the river. There would have to be an additional street crossing just east of the Jordan River. The trail would then continue along the east bank of the Jordan River, to an alternative future equestrian bridge location. Trail users would then follow the west bank of the river northward to the County owned parcel, where the trail would then continue as described above in Section 3.2 D.

JORDAN RIVER TRAIL MASTER PLAN

3.2 H Street Crossing

An at-grade street crossing will be installed to provide a safe crossing point for pedestrians.

3.2 I Street Crossing

An at-grade street crossing will be installed to provide a safe crossing point for equestrians.

3.2 J Bridge

An equestrian bridge will be installed across the Jordan River to accommodate equestrian users.

3.2 K Street Crossing

If needed, an at-grade street crossing will be located in this area to allow equestrian users to safely cross 14600 South near the river.

3.2 J Alternative Bridge

If needed, an equestrian bridge will be installed across the Jordan River.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.2 A	Funded
Signage	3.2 B	\$1,000
Trailhead & Paved Trail	3.2 C	\$418,800
Equestrian Trail	3.2 D	\$665,280
Equestrian Trail	3.2 E	\$52,800
Equestrian Trail	3.2 F	\$73,920
Equestrian Trail	3.2 G	\$31,680
Crossing	3.2 H	\$5,000
Crossing	3.2 I	\$5,000
Equestrian Bridge	3.2 J	\$250,000
Crossing	3.2 K	\$5,000
Equestrian Bridge	3.2 L	\$250,000

Ownership

Ownership for recommended trail alignments is shown in the maps below, followed by detailed ownership information in tables.

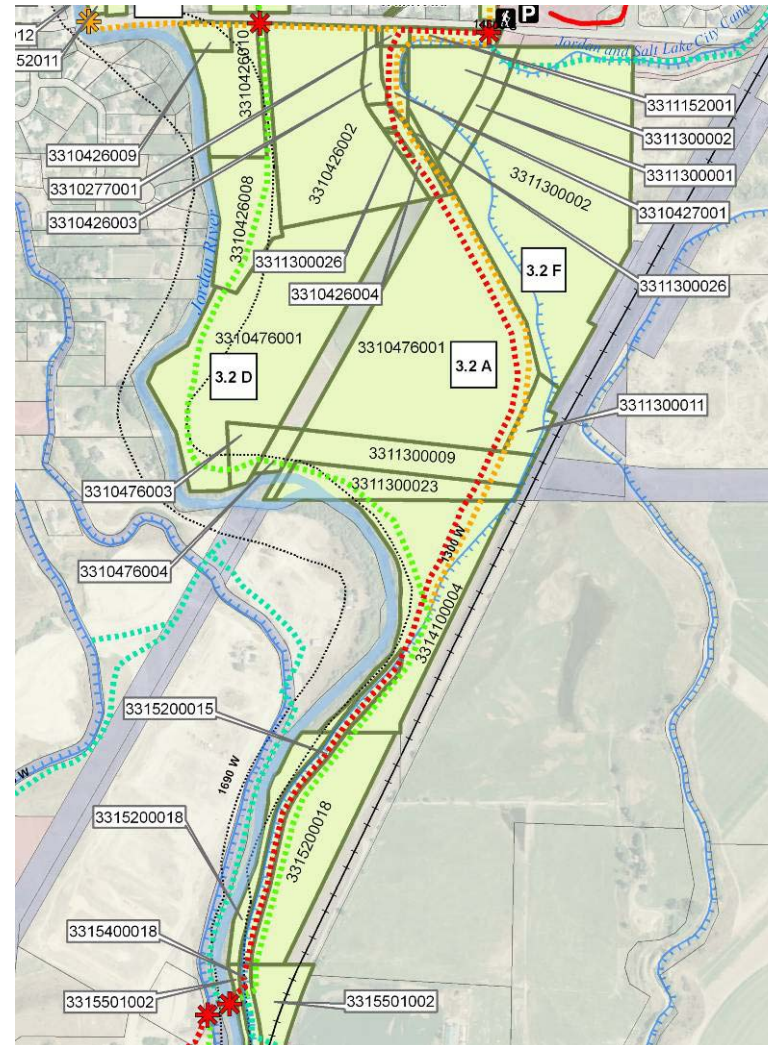


Figure 3.2.23 Ownership: Future Bridge Crossing to 14600 South

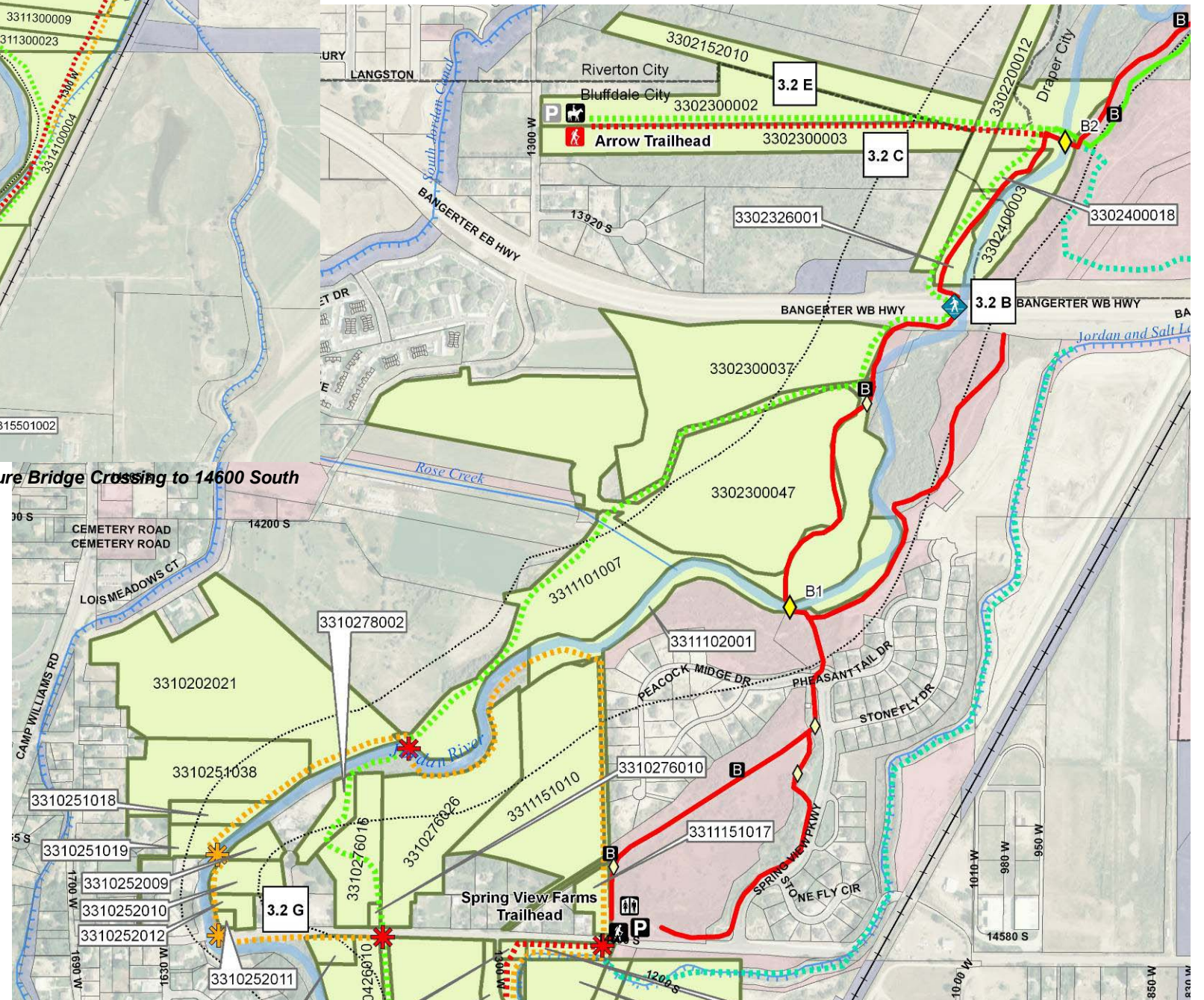


Figure 3.2.24 Ownership: 14600 South to Bridge B2

Table 3.2.1—Ownership: Recommendation 3.2 A—Paved

Parcel #	Owner	Acres
3310277001	Rhonda Hansen	0.4
3310426003	Michael Carlson	1.0
3310426004	Michael Carlson	1.4
3310427001	Rhonda Hansen	1.1
3310476001	Michael Carlson	26.7
3311152001	Francis Black	1.2
3311300001	Francis Black	2.4
3311300002	Francis Black	30.9
3311300002	Francis Black	4.7
3311300009	Utah Power and Light	6.0
3311300011	Michael Carlson	1.7
3311300023	Utah Power and Light	4.9
3314100004	City of Bluffdale	12.1
3315200015	Salt Lake City Corporation	3.2
3315400018	Salt Lake City Corporation	1.9
3311300026 - east	Michael Carlson	0.4
3311300026 - west	Michael Carlson	0.2
3315200018 - east	City of Bluffdale	9.7
3315200018 - west	City of Bluffdale	2.6
3315501002 - east	not available	9.0
3315501002 - west	not available	0.6

Table 3.2.1—Ownership: Recommendation 3.2 C— Paved

Parcel #	Owner	Acres
3302152010	Dan Fischer	8.2
3302400003	not available	9.4
3302200012	South Valley Sewer District	9.5
3302300003	Spring View Capital LLC	9.9
3302300002	Salt Lake County	10.0

Table 3.2.1—Ownership: Recommendation 3.2 D—Equestrian

Parcel #	Owner	Acres
3302300037	Ivan Nielsen	19.0
3302300047	Spring View Capital LLC	37.7
3302326001	Ivan Nielsen	2.2
3302400003	not available	9.4
3302400018	Spring View Capital LLC	1.8
3310276010	Keith Reels	1.2
3310276016	Moyal Anderson	5.2
3310278002	Richard Jones	1.8
3310426002	Michael Carlson	16.3
3310426008	Charmaine Smith	6.4
3310426010	Leesa Clark-Winger	5.4
3310476001	Michael Carlson	23.1
3310476003	Salt Lake County Water Conservancy District	1.6
3310476004	Utah Power and Light	0.9
3311101007	Salt Lake County	23.4
3311300009	Utah Power and Light	6.0
3311300023	Utah Power and Light	4.9
3314100004	Bluffdale City	12.1
3315200015	Salt Lake City Corporation	3.2
3315400018	Salt Lake City Corporation	1.9
3315200018 - east	Bluffdale City	9.7
3315200018 - west	Bluffdale City	2.6
3315501002 - east	not available	9.0
3315501002 - west	not available	0.6

Table 3.2.2—Ownership: Recommendation 3.2 E—Equestrian

Parcel #	Owner	Acres
3302152010	Dan Fischer	8.2
3302200012	South Valley Sewer District	9.5
3302300002	Salt Lake County	10.0
3302300003	Spring View Capital LLC	9.9
3302400003	not available	9.4

Table 3.2.3—Ownership: Recommendation 3.2 F—Equestrian

Parcel #	Owner	Acres
3310277001	Rhonda Hansen	0.4
3310426003	Michael Carlson	1.0
3310426004	Michael Carlson	1.4
3310427001	Rhonda Hansen	1.1
3310476001	Michael Carlson	26.7
3311152001	Francis Black	1.2
3311300001	Francis Black	2.4
3311300002	Francis Black	4.7
3311300002	Francis Black	30.9
3311300009	Utah Power and Light	6.0
3311300011	Michael Carlson	1.7
3311300023	Utah Power and Light	4.9
3314100004	Bluffdale City	12.1
3311300026 - east	Michael Carlson	0.4
3311300026 - west	Michael Carlson	0.2

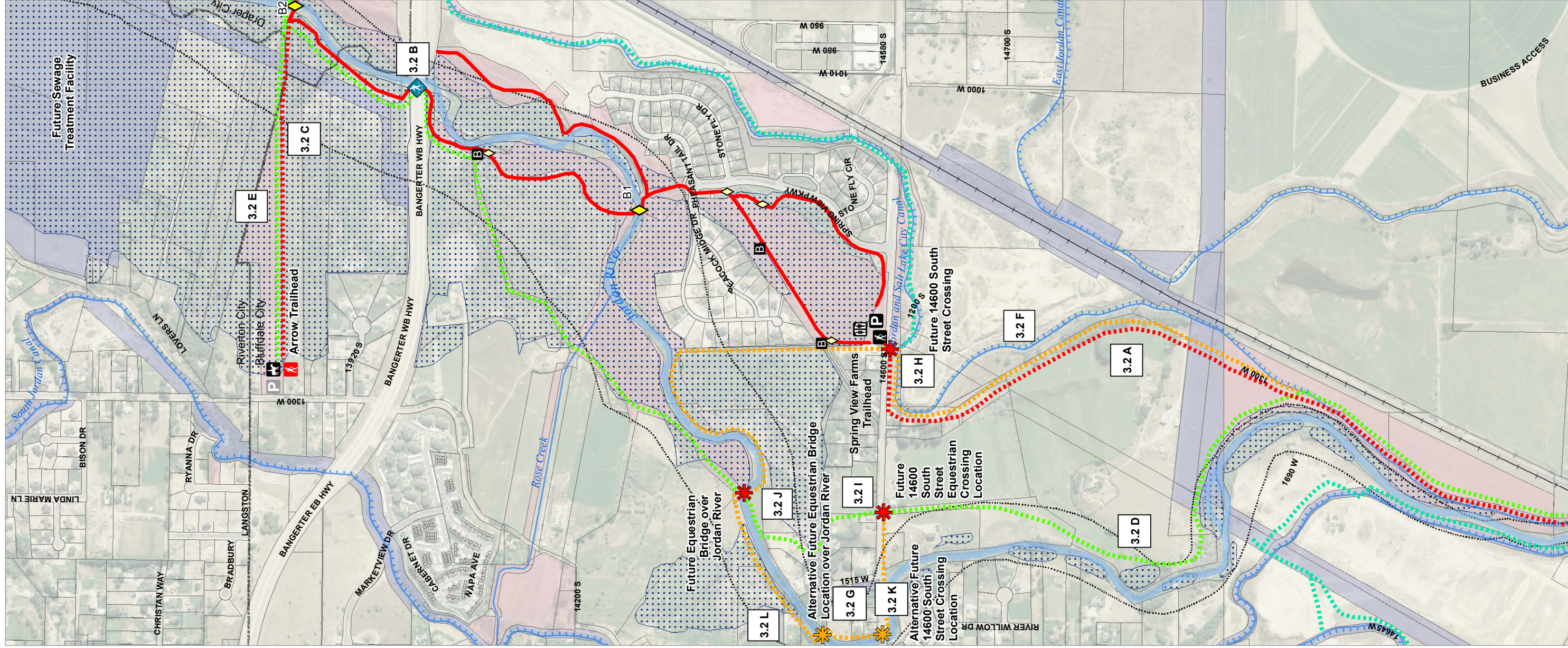
Table 3.2.4—Ownership: Recommendation 3.2 G—Equestrian

Parcel #	Owner	Acres
3310202021	Spring View Capital LLC	24.7
3310251018	Sherm Wayman	1.7
3310251019	Sherm Wayman	1.7
3310251038	Phil Mathews	9.2
3310252009	Russell Utley	1.3
3310252010	Guy Gustaveson	2.0
3310252011	Lorin Crump	0.3
3310252012	Dennis Crump	0.9
3310426009	John Mcomie	1.0
3310426010	Leesa Clark-Winger	5.4

Jordan River Trail Master Plan

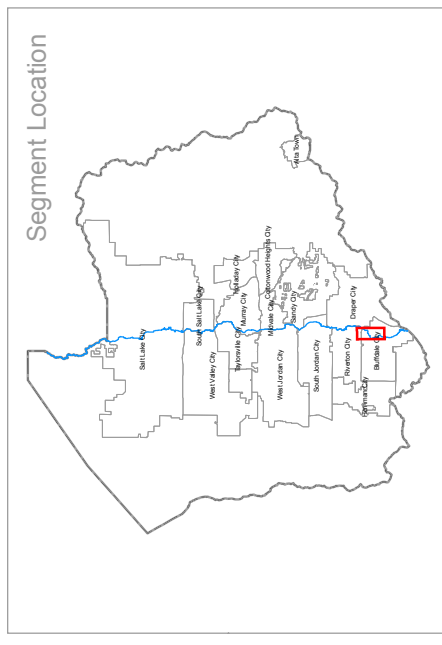
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Unpaved Jordan River Trail
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 2: Bluffdale/Riverton

15400 South to Bangerter Highway



3.3 Segment 3: Bluffdale/Riverton/Draper (Bangerter Highway to Vahe Street)

The paved Jordan River Trail continues north from the Bangerter Highway underpass and crosses to the east side of the river over a large pedestrian bridge (Bridge B2). The trail proceeds north, steeply in some sections, winding past a small hot springs area and an old barn and silo, with several benches along the way. The trail crosses a Corner Canyon Creek with a culvert, and a separate unpaved equestrian trail begins at this point and runs parallel to the paved trail.

The trails diverge in separate directions near Cephus Circle. The equestrian trail proceeds northward, tracing along the western perimeter of the neighborhood and then curves to the northwest following the Galena Canal, where it terminates at 12600 South with no connection to the north.



Figure 3.3.1 Bridge B2
6' wide, 90' span, 20' clearance



Figure 3.3.2 Bridge B3
8' wide, 130' span, 10' clearance

The paved trail, parallel to and separated by an equestrian trail, follows closer to the Jordan River utilizing a sewer easement, and curves to the northwest where it splits. One segment crosses the Jordan River via a pedestrian bridge (Bridge B3). This part of the path changes to a boardwalk for a short segment, passes through a marshy area and then switchbacks up toward Riverbend Park and the Riverbend Recreation Center. Several benches are located along the trail in this section. A formal trailhead and parking lot provide access to the trail near the South County Pool and Riverbend Golf Course. The main Jordan River Trail continues north through an area intended for utility vehicle access. It appears that other people have been removing the access chain and driving across the trail, parking in the open gravel area. The trail passes under 12600 South

Riverbend Park

- Benches
- Open Lawn Area
- Trash Receptacles
- Trailhead Sign



Figure 3.3.3 Riverbend Park

through a pedestrian underpass. A small rest area with a bench is located just north of the 12600 South underpass. The paved trail proceeds to the northeast with a separate unpaved equestrian trail nearby. The Jordan River Rotary Park provides access to the trail with a formal trailhead, where the trail splits. One segment crosses a large vehicular/pedestrian bridge with removable bollards (Bridge B4, also known as Tithing Bridge), passes through Dr. O. Roi Hardy Park, and winds north along the western bank of the Jordan River. This portion of the trail is furnished with many benches along the way and several boardwalks lead users to overlooks, including a gazebo overlooking a wildlife viewing area with a small pond. Access to the neighborhood west of the river is provided by a paved trail and footbridge.



Figure 3.3.4 Bench overlooking river

The other trail segment parallels the eastern bank of the Jordan River, with a separate unpaved equestrian trail following along at varying distances from the paved trail.

The other trail segment parallels the eastern bank of the Jordan River, with a separate unpaved equestrian trail following along at varying distances from the paved trail.



Figure 3.3.5 Old Silo



Figure 3.3.6 Hot Springs



Figure 3.3.7 Hot Springs warning sign



Figure 3.3.8 Equestrian trail beginning at culvert, looking northeast

through a pedestrian underpass.

A small rest area with a bench is located just north of the 12600 South underpass. The paved trail proceeds to the northeast with a separate unpaved equestrian trail nearby. The Jordan River Rotary Park provides access to the trail with a formal trailhead, where the trail splits. One segment crosses a large vehicular/pedestrian

bridge with removable bollards (Bridge B4, also known as Tithing Bridge), passes through Dr. O. Roi Hardy Park, and winds north along the western bank of the Jordan River. This portion of the trail is furnished with many benches along the way and several boardwalks lead users to overlooks, including a gazebo overlooking a wildlife viewing area with a small pond. Access to the neighborhood west of the river is provided by a paved trail and footbridge.

The other trail segment parallels the eastern bank of the Jordan River, with a separate unpaved equestrian trail following along at varying distances from the paved trail.



Figure 3.3.9 Marshy areas west of equestrian trail



Figure 3.3.10 View looking east from trail near Cephus Circle



Figure 3.3.11 View looking south on trail near Cephus Circle



Figure 3.3.12 View looking north toward equestrian trail



Figure 3.3.13 Buried sewer river crossing sign west of trail



Figure 3.3.14 Boardwalk entry into marshes



Figure 3.3.15 View toward Riverbend Golf Course Clubhouse from trail



Figure 3.3.16 View to river, looking southeast, from trail

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.3.17 Boardwalk winding through the wetlands



Figure 3.3.18 View to wetlands from trail, looking east



Figure 3.3.26 Start of equestrian trail north of 12600 South



Figure 3.3.27 View of Jordan River from trail, looking northeast



Figure 3.3.32 Dog Waste Station



Figure 3.3.33 Bridge B4, looking west at Dr. O. Roi Hardy Park



Figure 3.3.19 Benches near Riverbend Trailhead



Figure 3.3.20 Trail access from Riverbend Park Trailhead

Jordan River Rotary Park

- Restrooms—Year Round
- Trash Receptacles
- Dog Waste Station
- Trailhead Sign & Map
- Playground
- Parking (26 standard stalls, 2 handicapped)



Figure 3.3.28 Jordan River Rotary Park



Figure 3.3.34 Custom bench along Riverton segment of trail



Figure 3.3.35 Nesting platform



Figure 3.3.21 Trail access from Riverbend Park Trailhead



Figure 3.3.22 12300 South underpass, looking south



Figure 3.3.29 Bridge B4 24' wide, 73' span, 12' clearance



Figure 3.3.30 Looking south from Bridge B4



Figure 3.3.36 Wildlife viewing in winter



Figure 3.3.37 Paved access to neighborhood



Figure 3.3.24 12600 South underpass detailing



Figure 3.3.25 Bench rest area north of 12600 South

Dr. O. Roi Hardy Park

- Large Pavilion
- Picnic Tables
- Restrooms
- Benches
- Open Lawn Area
- Trash Receptacles
- Parking (50 standard, 2 handicapped)



Figure 3.3.31 Dr. O. Roi Hardy Park



Figure 3.3.38 Gazebo overlook

Issues and Opportunities

- There are steep sections of the trail between Bridge B2 and the Old Silo.
- There is a desire by residents to develop a Nature Center in Draper, near the Old Barn and Silo.
- The land east of the Hot Springs is under a conservation easement.
- There is no equestrian trail south of Bridge B2.
- The equestrian trail between Bangerter Highway and 12600 South needs to be signed and maintained and existing trails need to be preserved as the Galena subdivision develops.
- Flooding occurs at the 12600 South underpass during peak river flow.
- There is an existing utility vehicle access point on the east side of the river on the south side of 12600 South, and that vehicles must drive across part of the paved trail to access this area. It appears that trail users have been removing the access chain, driving across the trail, and parking in an open gravel area east of the river.
- The equestrian trail from Bridge B3 to 12600 South trailhead needs to be identified, marked, and mowed and/or surfaced with bark mulch.
- The equestrian trail from 12600 South trailhead to Alan Point Drive along the east bank of the river needs to be separated from the paved trail (preferably by at least 20'), identified with signage, mowed, and/or surfaced with bark mulch.
- The pump house in Dr. O. Roi Hardy Park encroaches on the equestrian trail west of the Jordan River.

Recommendations: Paved Trail

3.3 A Utah State Department of Natural Resources Land/Steep Trail
If the opportunity arises at some point in the future, the paved trail should be re-routed to relocate the trail to the recommended alignment, but this is not a high priority.

Many trail connections have been proposed through the Utah State Department of Natural Resources lands on the east side of the Jordan River; however, the existing paved trail, including the recommended realignment near the silo, should remain the official Jordan River paved trail. If Draper City wishes to develop additional paved trails, they should be simple and connected, and not over-done. A suggested alignment is shown on the map. These trails will become Draper City trails.

3.3 B 12600 South Informal Trail Access

This maintenance access point needs to restrict unauthorized vehicles more effectively, and should be retrofitted with a gate that can be accessed only by authorized personnel. Gravel areas where people have been parking need to be revegetated and enhanced to blend with the Riverbend Nature Area and Jordan River corridor.

3.3 C Flooding at 12600 South Underpass

These areas are subject to periodic flooding during period of peak run-

off and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

Recommendations: Equestrian Trail

3.3 D Equestrian Trail Opportunity: Utah State Department of Natural Resources Land

Many equestrian trails have been proposed through the Utah State Department of Natural Resources lands on the east side of the Jordan River; however, the existing equestrian trail should remain the official Jordan River Equestrian Trail. Additional equestrian trails are discouraged.

3.3 E Equestrian Trail Gap: North end of Riverbend Nature Area

Develop an equestrian trail that connects the trails surrounding the Riverbend Nature Area, providing a loop trail aligned with 12600 South. The trail should be separated from the roadway and be contained within the Nature Area.

3.3 F Equestrian Trail Alignment: Dr. O. Roi Hardy Park

The new pump house in Dr. O. Roi Hardy park encroaches on the equestrian trail west of the Jordan River. The equestrian trail through the Dr. O. Roi Hardy Park needs to be re-aligned to accommodate equestrian use through this area.

3.3 G Equestrian Trail Maintenance

The equestrian trail between Bangerter Highway and Alan Point Drive (Segment 4) needs to be signed according to the signage program described in Chapter 5, and maintained properly to ensure a high level of functionality for equestrian users.

Construction Costs

Table 3.3.1—Construction Costs: Paved Trail Segment 3

Feature Category	Map Code	CIP Budget Implication
Paved Trail/Policy	3.3 A	\$118,000
Access	3.3 B	\$30,000
Signage	3.3 C	\$1,000
Policy	3.3 D	N/A
Equestrian Trail	3.3 E	\$21,200
Equestrian Trail	3.3 F	\$5,280
Signage	3.3 G	\$3,000

Ownership

Ownership for recommended trail alignments is shown in the map below, followed by detailed ownership information in tables.

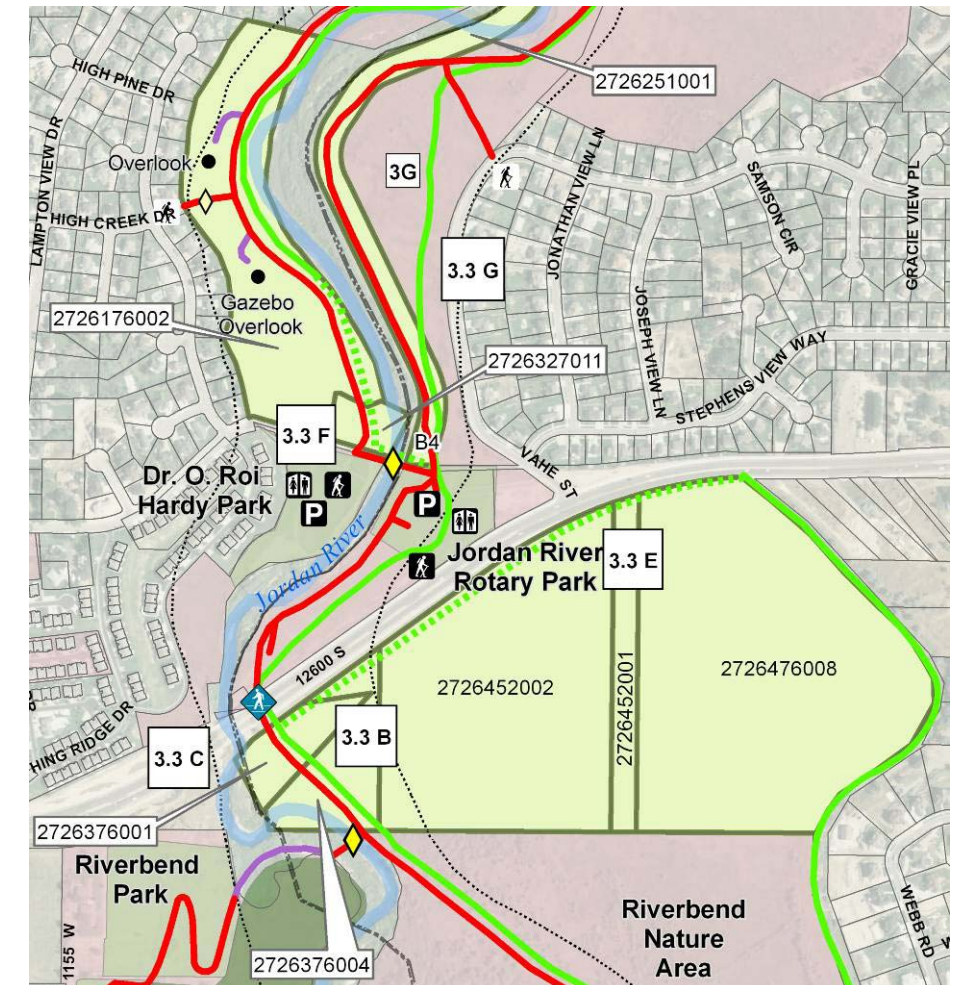


Table 3.3.2—Ownership: Recommendation 3.3 E—Equestrian

Parcel #	Owner	Acres
2726376004	US Government	4.8
2726452001	State of Utah Division Parks/Recreation	3.7
2726452002	US Department of the Interior	26.5
2726476008	US Department of the Interior	34.5
2726976001	Utah Department of Transportation	2.7

Table 3.3.3—Ownership: Recommendation 3.3 F—Equestrian

Parcel #	Owner	Acres
2726176002	Riverton City	16.8
2726251001	Utah Department of Natural Resources	7.5
2726327011	Riverton City	1.4

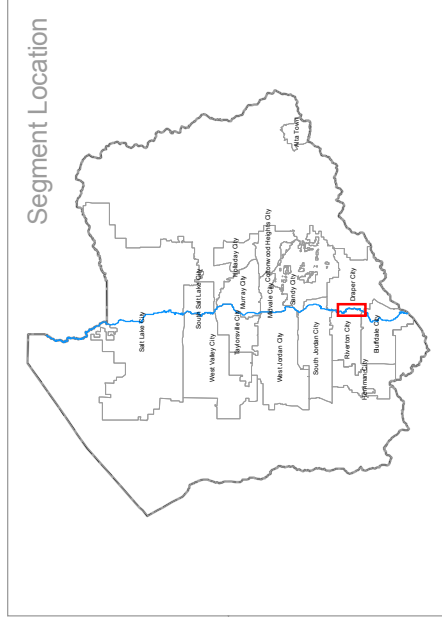
Jordan River Trail Master Plan

Paved Trail Facilities

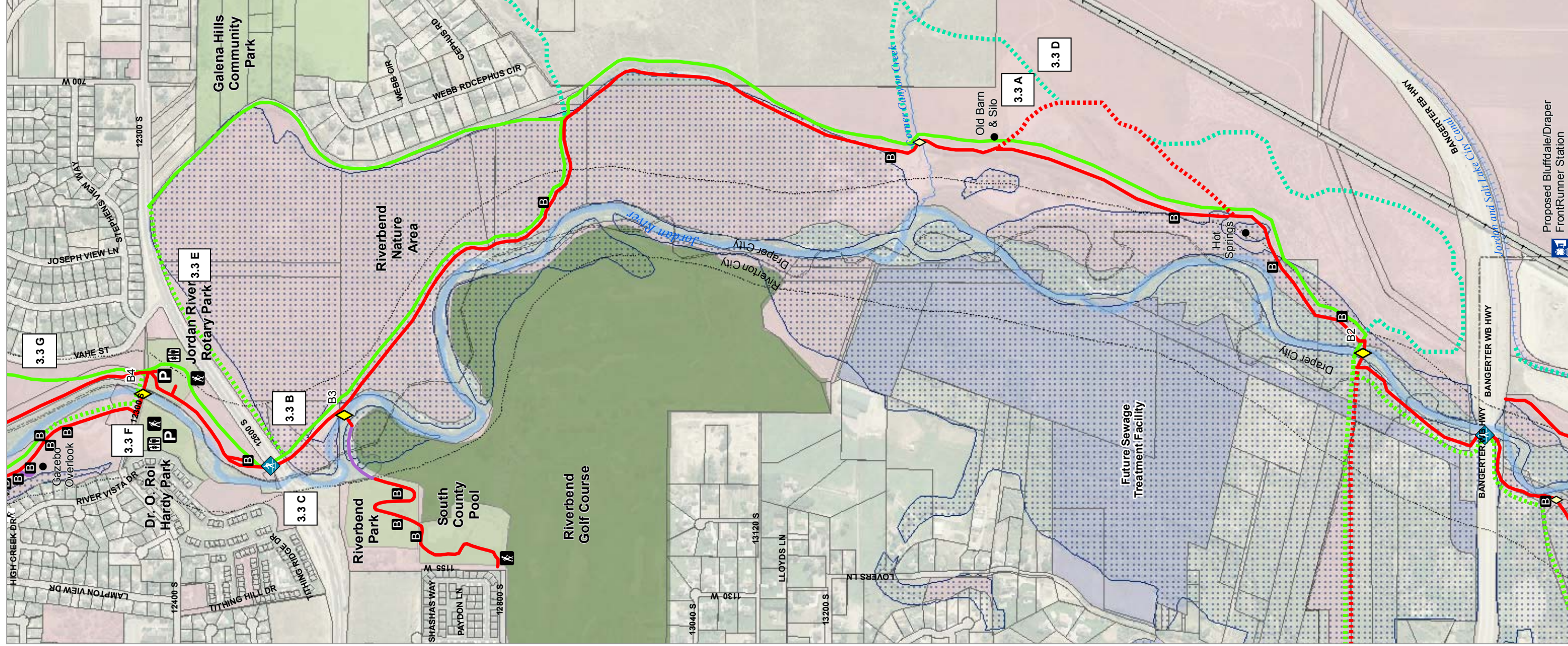
- Future Element**
- Future Element
- Alternative Future Element
- Crossings**
- Crosswalk
- No Safe Crossing
- Signalized Crossing
- Underpass
- Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
- Equestrian Trailhead
- Formal Trailhead
- Informal Trailhead
- Trail Access Point
- Parking - Formal Paved
- Parking - Informal Unpaved
- Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
- Recommended Jordan River Trail
- Existing Unpaved Jordan River Trail
- Existing Unpaved Jordan River Trail - To Be Paved
- Existing Jordan River Equestrian Trail
- Recommended Jordan River Equestrian Trail
- Existing Jordan River Boardwalk
- Alternative Jordan River Trail Alignment
- City Existing Trail
- City Proposed Trail
- Trails to be Abandoned
- Public Transit**
- TRAX Station
- Direction to Nearest TRAX Station
- Proposed FrontRunner Station
- Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
- Bridge Over Jordan River
- Footbridge Over Stream or Canal
- Bench
- Picnic Table
- Trash Receptacle
- City Boundary
- Jordan River
- Streams
- Canals
- Light Rail
- Railroad
- County Boundary
- Meander Corridor (no data north of Surplus Canal)
- Wetlands (WAIDS 2007)
- Golf Course
- Park
- Publicly Owned Land
- Utility Owned Land

Segment 3: Bluffdale/Riverton/ Draper

Bangerter Highway to Yahi Street



June 2008



Proposed Bluffdale/Draper
FrontRunner Station

3.4 Segment 4: Riverton/Draper/South Jordan (Vahe Street to Warm Creek Road)

The eastern trail segment continues north with paved access to the neighborhood via Vahe Street. In a joint effort, the County and Draper City recently completed the paved trail with a 3' shoulder for equestrian use in conjunction with a stream bank stabilization project, which continues the trail north to 11400 South on the east bank of the river. There is a small neighborhood connection to the east, terminating at an emergency turnaround on Alan Point Drive. The equestrian connection continues along Alan Point Drive to 700 West, where it ends.

The western trail segment curves around to the east and heads north again, winding between the Jordan River and several large horse properties. The trail then curves sharply to the west through a narrow trail easement. The trail turns north for a short distance and terminates at Reeves Lane. There is a gap in the Jordan River Trail corridor between Reeves Lane and the Riverfront Park area.

Riverfront Park on the west side at 11100 South offers trail access at several points along Riverfront Trail and provides parking in two separate parking lots. A separate equestrian trail spurs-off of the paved trail near Louise Meadow Drive, tracing along the western bank of the Jordan River. Both trails join together to cross Midas Creek via a small footbridge, and separate again as they wind to the north and east along the river. Several benches are located along the trail, and access to the other side of the South Jordan Trail (10600 South) is provided with a pedestrian/equestrian underpass. As the trail travels through the underpass on the north side of the South Jordan Trail past Mulligan's Golf, the paved trail traces along close to the western bank of the Jordan River with the equestrian trail located to the west.



Figure 3.4.1 Trail along western shore of river, looking north as it begins to wind around horse properties

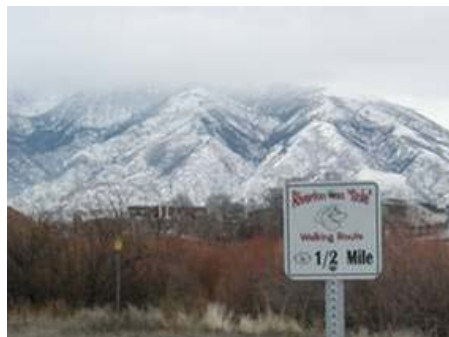


Figure 3.4.2 "Riverton has Sole" walking route sign



Figure 3.4.3 Neighborhood trail access point



Figure 3.4.4 Jordan River Trail access sign



Figure 3.4.5 Designated Wetland sign



Figure 3.4.6 Eastern shore trail, north of parking lot, looking north



Figure 3.4.7 Trail looking east, joined by trail access path from Vahe Drive



Figure 3.4.8 Equestrian trail paralleling paved trail



Figure 3.4.9 Informal Trailhead at Alan Point Drive



Figure 3.4.10 "Beginning" of trail near Rambouillet Drive and Riverfront Trail



Figure 3.4.11 View to north along river

Riverfront Park

- Playground
- Open Play Area
- Pavilions & {Picnic Tables
- Angling Ponds
- Benches & Restrooms
- Trash Receptacles
- Drinking Fountains
- Dog Waste Stations
- Parking (south lot: 19 standard, 1 handicapped; north lot: 31 regular, 2 handicapped)



Figure 3.4.13 Riverfront Park



Figure 3.4.12 Picnic pavilion along trail



Figure 3.4.14 Bench along trail, looking south



Figure 3.4.15 Riverfront Park entrance sign



Figure 3.4.16 Riverfront Park signage and restroom facilities



Figure 3.4.17 Ducks sunbathing at Midas Pond



Figure 3.4.18 Midas Pond



Figure 3.4.19 Footbridge in Riverfront Park



Figure 3.4.20 Equestrian trail paralleling trail



Figure 3.4.27 View to west from 10600 South underpass



Figure 3.4.28 View to north from 10600 South underpass



Figure 3.4.21 Footbridge crossing small stream



Figure 3.4.22 Trail looking north



Figure 3.4.29 View to south from 10600 South underpass



Figure 3.4.30 Trail connection to 10600 South near Mulligan's Golf



Figure 3.4.23 Pavilions in northern part of Riverfront Park



Figure 3.4.24 View looking north from trail



Figure 3.4.31 Trail access from 10600 South near Mulligan's Golf



Figure 3.4.32 Trail looking south near Mulligan's Golf



Figure 3.4.25 Trail looking north



Figure 3.4.26 Trail access from 10600 South Business Park parking lot

Issues and Opportunities

- There is a gap in the paved trail and the equestrian trail on the west side of the River between Reeves Lane and Midas Park Pond.

Recommendations: Paved Trail

3.4 A Trail Gap: Reeves Lane to 114th South (west bank)
Develop a paved trail to connect the existing trail ending near Reeves Lane to the existing trail near 11400 South, in Midas Pond Park. The trail passes through some County-owned land, but also through private property. Negotiations with private property owners will be required, but the connection is important.

Recommendations: Equestrian Trail

3.4 B Trail Alignment and Maintenance: 12300 South to Alan Pointe Drive

The equestrian trail in this section needs to be re-aligned to provide the largest separation possible from the paved trail. Chapter 5 establishes desired separation distances between equestrian and paved trail uses. The trail also needs to be signed according to the signage program described in Chapter 5 and maintained properly to ensure a high level of functionality for equestrian users.

3.4 C Trail Gap: Reeves Lane to 11400 South (west bank)

Develop an equestrian trail to connect the existing equestrian trail ending near Reeves Lane to the existing equestrian trail near 11400 South, in Midas Pond Park. The trail passes through some County-owned land, but also through private property. Negotiations with private property owners will be required, but the connection is important.

3.4 D Bridge: 11400 South

Install a bridge over the Jordan River at 11400 South to accommodate pedestrians and equestrians. Ideally, this should take place at the same time the vehicular bridge is installed at this same location.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.4 A	\$118,800
Equestrian Trail/Signage	3.4 B	\$31,680
Equestrian Trail	3.4 C	\$31,680
Bridge over Jordan River	3.4 D	\$250,000

Ownership

Ownership for recommended trail alignments is shown in the map below, followed by detailed ownership information in tables.

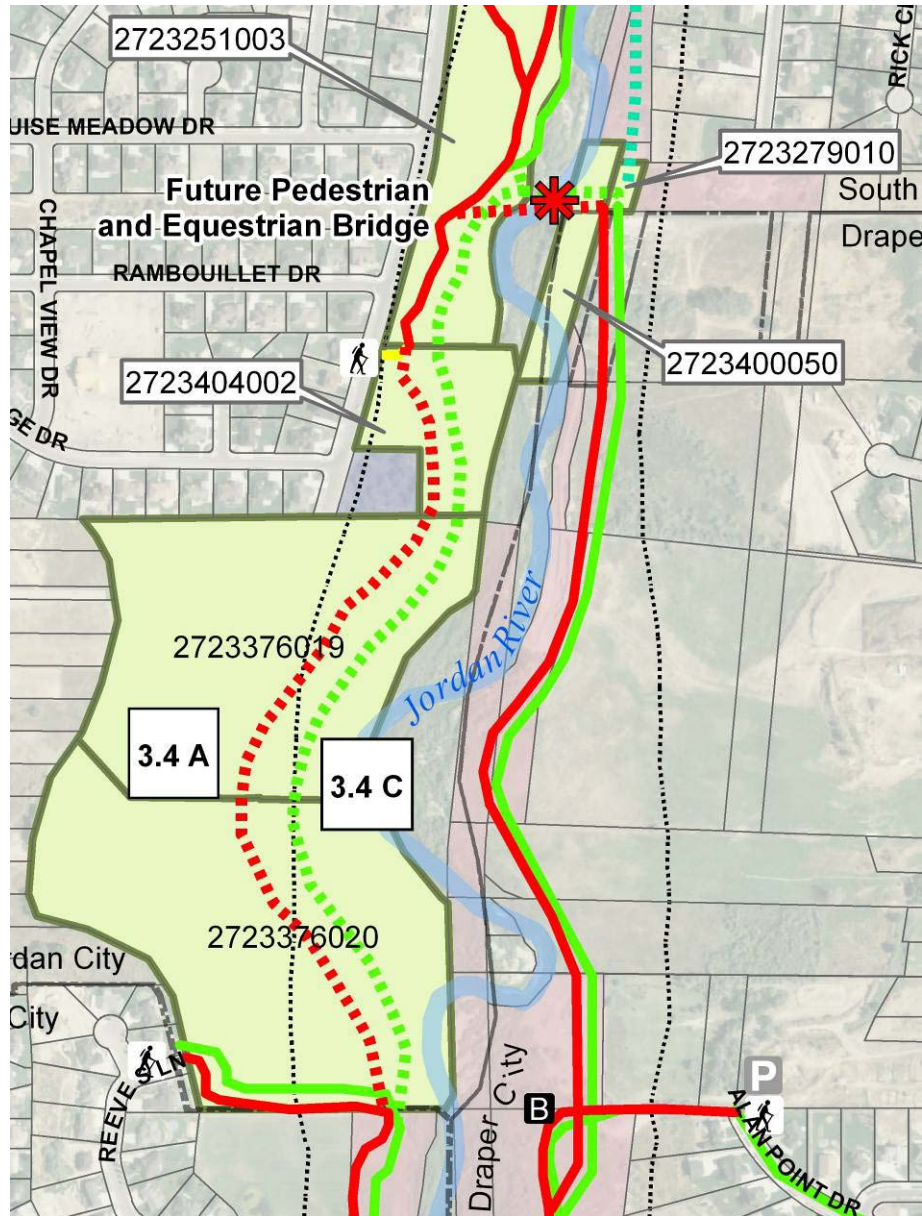


Figure 3.4.33 Ownership: Segment 4

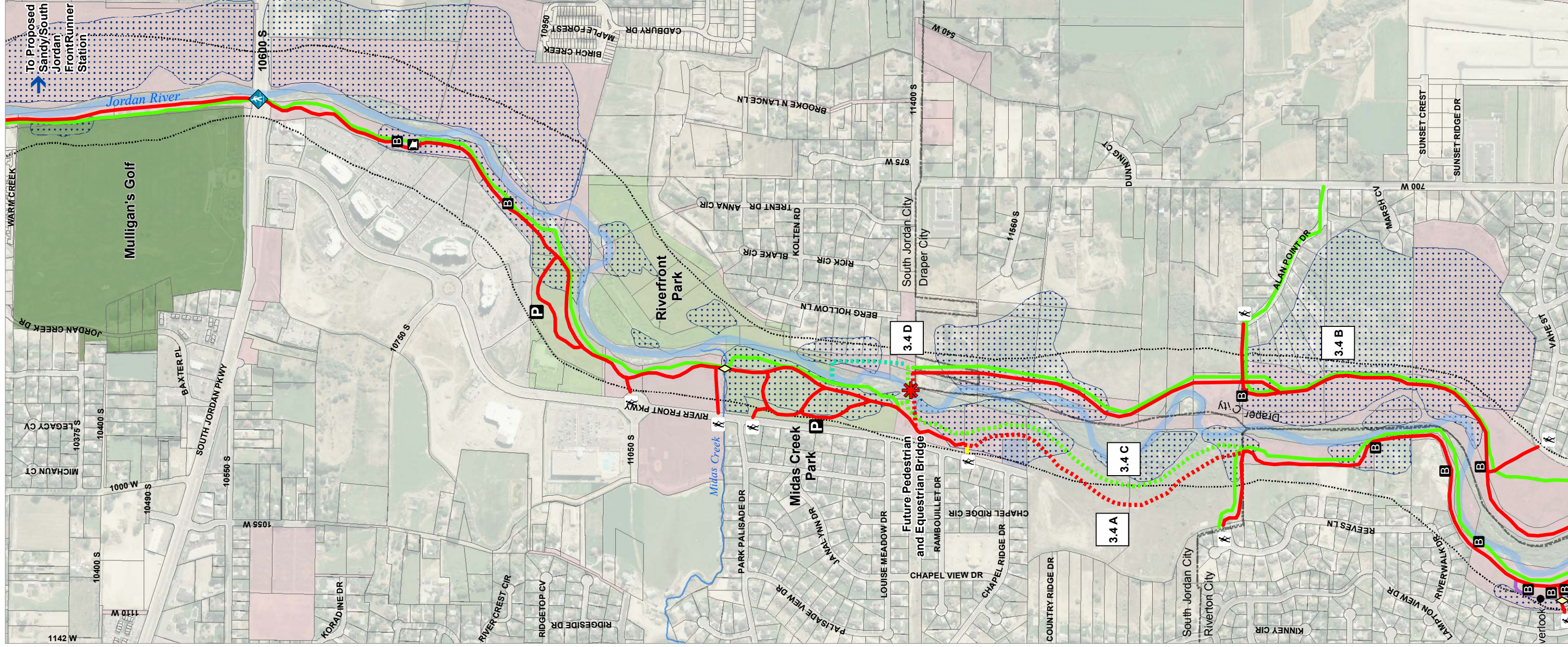
Table 3.4.2–Ownership: Recommendation 3.4 A & 3.4 C

Parcel #	Owner	Acres
2723251003	Provo Jordan River Parkway Authority	15.3
2723279010	South Jordan City	0.3
2723376019	Peter Coats	17.9
2723376020	Peter Coats	20.6
2723400050	not available	1.9
2723404002	South Jordan City	3.8

Jordan River Trail Master Plan

Paved Trail Facilities

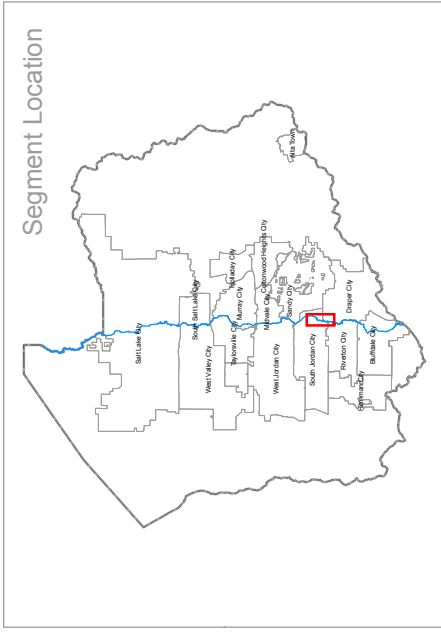
- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 4: Riverton/Draper/ South Jordan

Vahe Street to Warm Creek Road

0 600 1,200
Feet



3.5 Segment 5: South Jordan/ Sandy/West Jordan/Midvale (Warm Creek Road to 8450 South)

The Jordan River paved and equestrian trails follow the curve of the river to the northwest, and pass several benches as they head toward the trailhead just south of Shield's Lane. The equestrian trail ends at the Shield's Lane Trailhead.

The Jordan River paved trail continues north through an underpass, taking it under Shield's Lane (10000 South), and travels a short distance, terminating at the western bank of the Jordan River. There is a gap in the trail corridor between 10000 South and 9400 South, where trail access is provided at the River Oaks Golf Course Trailhead.

The trailhead is accessed from Riverside Drive and separate parking for 15 vehicles is provided for trail users. The trail begins at the south end of the parking lot, near a large pedestrian bridge (Bridge B5), which crosses to an area of the course that is off limits to trail users. Currently, this bridge is the south end of this portion of the trail. The trail heads north from this bridge, following the east bank of the Jordan River, past the River Oaks Golf Course Club House. The trailhead parking lot can be accessed from another path near Bridge B6 and ends at 9000 South, near Bridge B7. The trail appears to take users into areas of the golf course that are off-limits, which is reported as confusing by users. There is no underpass or signed crossing to allow trail users to cross 9000 South, and there is a gap in the trail corridor between 9000 South and approximately 8600 South.

The trail resumes at a trail access point located on 700 West and 8600 South in Midvale. The only parking is informal roadside parallel parking near the trail access point. The paved trail follows the Jordan River to the west and several short trails wind in and out, closer to the river. They are designated with crushed asphalt surfacing, and providing several overlook points with benches. The river and trail curve north again, following the western edge of the Bingham Junction area.



Figure 3.5.2 Equestrian trail paralleling paved trail, looking south



Figure 3.5.3 Equestrian trail running adjacent to paved trail, looking south



Figure 3.5.10 Trail looking east near 10000 South



Figure 3.5.11 Shield's Lane Trailhead



Figure 3.5.4 View of Mulligan's Golf from equestrian trail



Figure 3.5.5 Paved trail looking south



Figure 3.5.12 Shield's Lane pedestrian underpass



Figure 3.5.13 Paved trail ends at Jordan River, north of Shield's Lane



Figure 3.5.6 Equestrian trail looking south



Figure 3.5.7 Equestrian trail running aside paved trail



Figure 3.5.14 View south from trail dead end, towards Shield's lane



Figure 3.5.15 View to north where trail dead ends

Shield's Lane Trailhead (10000 South)

- Trailhead Sign & Map
- Restrooms
- Trash Receptacles
- Parking (12 standard, 2 handicapped, undefined equestrian stalls)



Figure 3.5.1 Shield's Lane Trailhead



Figure 3.5.8 Bench overlooking river



Figure 3.5.9 Trail looking south near Shield's Lane Trailhead



Figure 3.5.16 View to Jordan River, north of Shield's Lane, looking east



Figure 3.5.17 View toward trail, north of Shield's Lane, looking west

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.5.18 Ducks in flight

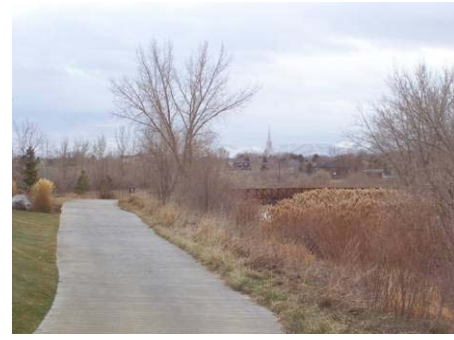


Figure 3.5.19 View looking south near River Oaks Golf Course, towards Bridge B5



Figure 3.5.25 Bridge B6
6' wide, 130' span, 8' clearance



Figure 3.5.26 Bridge B7
10' wide, 130' span, 8' clearance



Figure 3.5.33 View to south east from trail



Figure 3.5.34 Midvale "Gold Medal Mile" marker sign

River Oaks Golf Course Trailhead

- Local Trail Map
- Parking (15 standard)



Figure 3.5.20 River Oaks Trailhead



Figure 3.5.27 View from trail, looking north



Figure 3.5.28 Trail access point at 700 West and 8450 South in Midvale



Figure 3.5.35 Stone bench along crushed asphalt pathway



Figure 3.5.36 Vegetation along Jordan River, looking east



Figure 3.5.21 View from trail near River Oaks Golf Course Club House, looking north



Figure 3.5.22 View from trail towards Bridge B6



Figure 3.5.29 Beginning of trail at 700 West



Figure 3.5.30 Trailhead sign near 700 West

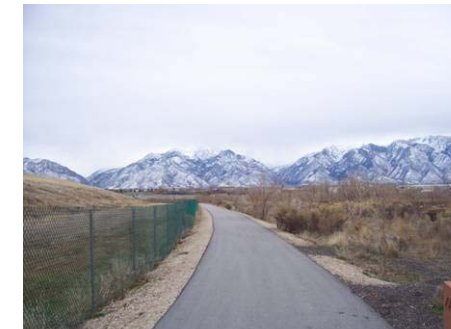


Figure 3.5.37 View to east from trail



Figure 3.5.23 Great Blue Heron wading in Jordan River near River Oaks Golf Course



Figure 3.5.24 Bridge B5
10' wide, 130' span, 9' clearance



Figure 3.5.31 View from trail towards 700 West



Figure 3.5.32 Riparian vegetation along Jordan River near 700 West

Issues and Opportunities

- The equestrian trail ends at the Shields Lane trailhead, with no more equestrian use until Segment 7 in Murray.
- There is a gap in the paved trail between the end just north of Shields Lane and Bridge B5, in River Oaks Golf Course. "No Trespassing" signs in this area are unclear about where the public is allowed.
- There is flooding at the Shields Lane underpass.
- There is a gap in the paved trail between 9000 South and approximately 8500 South 900 West.
- No safe crossing exists for 9000 South.

Recommendations: Paved Trail

3.5 A Flooding at Shields Lane (10000 South) Underpass

These areas are subject to periodic flooding during period of peak run-off and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.5 B Trail Gap: Shields Lane to 8600 South

The trail gap between Shields Lane (10000 South) and Bridge B5 in the middle of River Oaks Golf Course will be paved by the end of Summer 2007. The Utah and Salt Lake Canal Companies are cooperating with Sandy City and South Jordan to make the connection in the 30' wide corridor. This plan recommends continuing the main trail north, from Bridge B5, along the North Jordan Canal Road through the western portion of the River Oaks Golf Course where a safe underpass can be installed under 9000 South.

The existing trailhead and trail along the eastern edge of the golf course should be maintained to provide access to the trail for local residents. Trail users can park at the trailhead and access the main trail by crossing at Bridge B5.

The trail would then continue north, under 9000 South along the canal road and would cross the Jordan River near 8450 South to connect to the existing trail in Midvale.

3.5 C Alternative Route to 9000 South Underpass

A possible alternative would be to add a paved trail segment along the north end of River Oaks Golf Course, just south of 9000 South, where an informal unpaved trail already exists. This would allow trail users quicker access to the new 9000 South underpass if they were headed north on the trail corridor, and would provide a small loop trail around the perimeter of the golf course.

3.5 D Alternative Route from 9000 South to 8600 South

Another alternative route for the paved trail on the north side of 9000 South would continue to the east from the new 9000 South underpass on the North Jordan Canal Road, following the southern boundary of the Rocky Mountain Power substation property. As the trail approaches the Jordan River, it would have to cross the river either at a new, separate pedestrian bridge or the 9000 South vehicular bridge would have to be modified to safely accommodate people crossing the river. The trail would then travel north, paralleling the east bank of the Jordan River through a privately-owned parcel of land falling within unincorporated Salt Lake County. The paved trail then crosses into property owned by West Jordan City, and would join with the existing paved trail just west of 700 West.

3.5 E Unsafe Road Crossing: 9000 South

There is no existing safe crossing near the trail on 9000 South. The County should install a pedestrian underpass along the North Jordan Canal Road, passing under 9000 South along the western edge of River Oaks Golf Course, allowing the trail to continue to the north.

3.5 F Trailhead at River Oaks Golf Course

A trail head is developed at the River Oaks Golf Course which is available to trail users. A path from the parking lot, with a curb cut, to the existing Jordan River Trail is necessary. Currently, there is no pathway to the trail, which is a short distance away, making it unclear and confusing for trail users about whether they are allowed to be in the area and access the trail from this location.

Recommendations: Equestrian Trail

3.5 G Equestrian Trails North of Shields Lane

There are no existing equestrian trails north of Shields Lane in this segment, and no new equestrian trails are proposed in this segment due to the close proximity of the trail corridor to the River Oaks Golf Course and the situation created by the trail corridor running through the golf course from approximately 9400 South to 9000 South. It would be undesirable for all users to mix equestrian, golf, and paved trail uses within such a narrow corridor.

There are no new equestrian trails recommended north of 9000 South due to corridor width limitations further north in segments 6 and 7. It is undesirable to create new segments of equestrian trail that do not have the potential to be linked to other equestrian segments in the future.

Construction Costs

Table 3.5.1- Construction Costs: Paved Trail Segment 5		
Feature Category	Map Code	CIP Budget Implication
Signage	3.5 A	\$1,000
Paved Trail	3.5 B	\$190,080
Paved Trail	3.5C	\$71,280
Paved Trail	3.5 D	\$237,600
Underpass	3.5 E	\$1,000,000
Trailhead	3.5 F	\$5,000
Bridge	3.5 G	\$250,000

Ownership

Ownership for recommended trail alignments is shown in the map below, followed by detailed ownership information in tables.

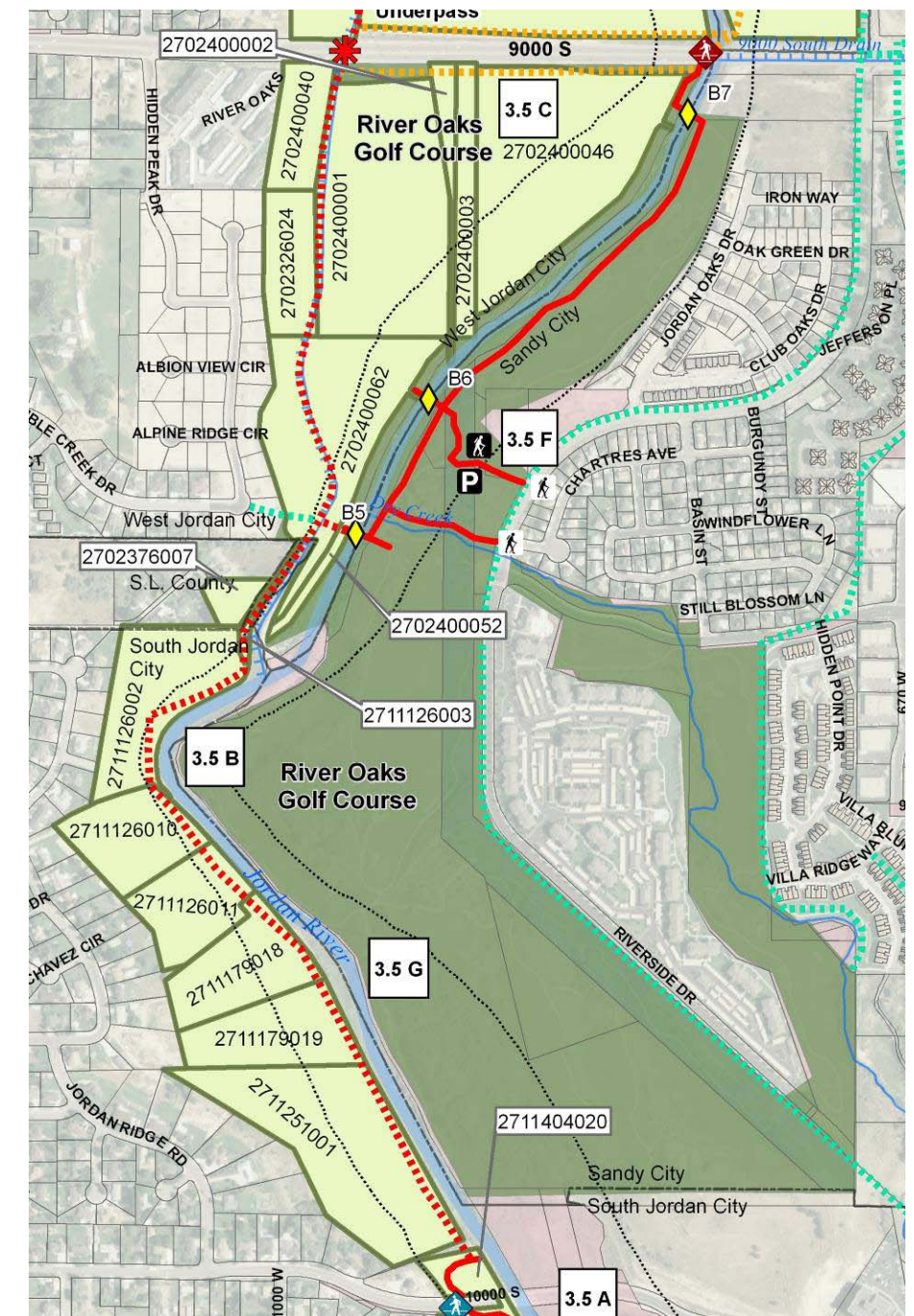
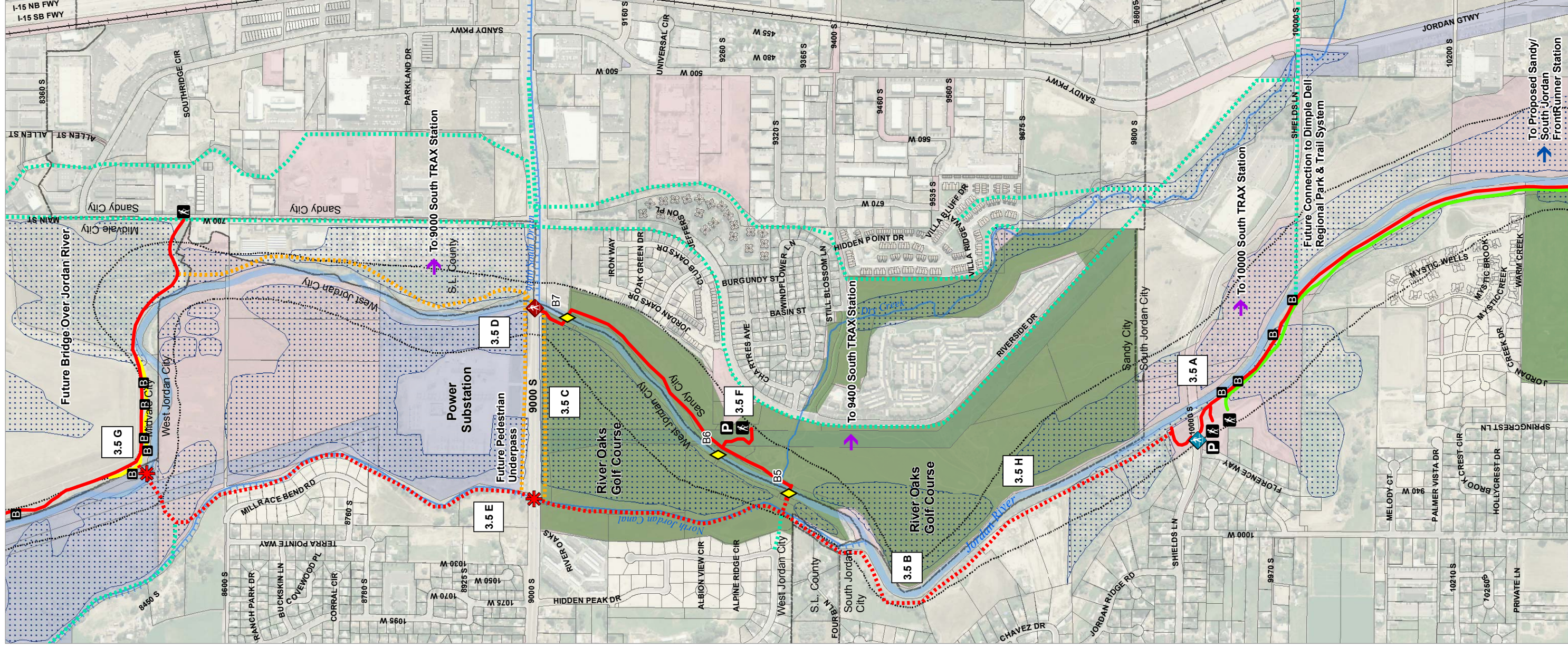


Figure 3.5.38 Ownership: Segment 5, Shield's Lane to 9000 South

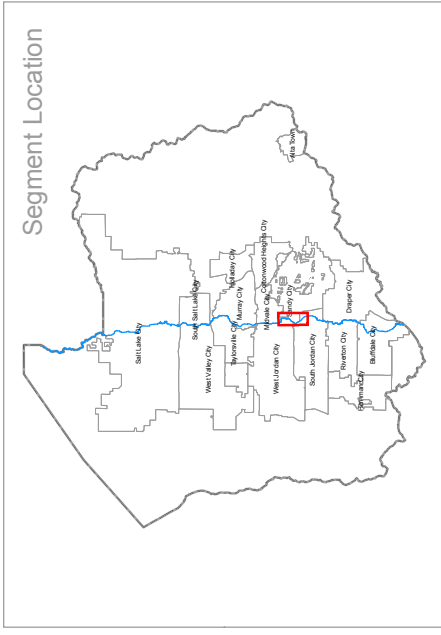
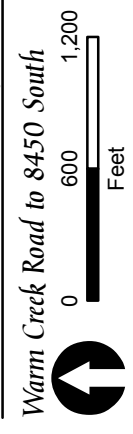
Jordan River Trail Master Plan

Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 5: South Jordan/Sandy/ West Jordan/Midvale



3.6 Segment 6: West Jordan/Midvale/Murray/Taylorville (8450 South to Winchester Street)

The Jordan River paved trail curves to the north, following alongside the Jordan River, with the power line corridor running along the west bank of the river. Several rest areas are located along the trail with benches located near stone-lined drainage swales. A crushed asphalt pathway meanders down closer to the river, with several benches and plaques for interpretive signage along the path. Near Bridge B8, the trail forks. The north fork takes users to the Midvale Center Street (7800 South) Trailhead. The other fork takes users across the Jordan River over the pedestrian bridge (Bridge B8) and to a trail on the west side of the river. A short unpaved segment runs south of the bridge,



Figure 3.6.1 Bridge B8
16' wide, 90' span, 15' clearance

providing views out over a wetland area with several ponds. From the bridge, the paved trail continues north. The trail can be accessed for maintenance via a gate on the south side of 7800 South, but trail users cross 7800 South using a pedestrian underpass, and emerge at the Gardner Village Trailhead.

The paved trail continues north along the west bank of the Jordan

River with several picnic tables, benches, trash receptacles, and an overlook along the way. The trail crosses a small footbridge which is followed by two loop pathways, and rejoins again to pass under a railroad bridge. The trail winds its way north, crossing another footbridge, before it approaches a group of benches near the 7000 South Trailhead.

The trail then travels under 7000 South using a pedestrian underpass, making its way past recently developed homes towards Winchester Street. Another pedestrian underpass takes users to the north side of Winchester Street to Winchester Park.

Gardner Village Trailhead (North Side of 7800 South)

- Trailhead Sign & Map
- Restrooms
- Picnic Tables
- Plaza
- Open Lawn Area
- Trash Receptacles
- Dog Waste Station
- Parking (in Gardner Village Parking Lot)



Figure 3.6.3 Gardner Village Trailhead

7000 South Trailhead

- Benches
- Trash Receptacles
- Trailhead Sign & Map
- Interpretive Signage & Overlook
- Parking (72 standard, 2 handicapped)



Figure 3.6.4 7000 South Trailhead



Figure 3.6.5 Crushed asphalt pathway



Figure 3.6.6 View from trail looking north



Figure 3.6.7 Stone-lined drainage swale along trail



Figure 3.6.8 Trail looking north



Figure 3.6.9 Bench pull-out



Figure 3.6.10 Water running into Jordan River



Figure 3.6.11 Midvale Trail Marker



Figure 3.6.12 Midvale Center Street Trailhead parking lot



Figure 3.6.13 Statue at Midvale Center Street Trailhead



Figure 3.6.14 Beaver damage along trail

Midvale Center Street (South Side of 7800 South)

- Trailhead Sign & Map
- Picnic Tables
- Trash Receptacles
- Dog Waste Station
- Parking (22 standard, 2 handicapped)



Figure 3.6.2 Midvale Center Street Trailhead



Figure 3.6.15 View of Jordan River, looking north near 7800 South, from Bridge B8



Figure 3.6.16 View to south, west of Bridge B8, looking toward unpaved portion of trail



Figure 3.6.17 Maintenance access from 7800 South



Figure 3.6.18 View to ponds



Figure 3.6.25 Trail overlook



Figure 3.6.26 Footbridge



Figure 3.6.33 Railroad Bridge underpass



Figure 3.6.34 Reclaimed Sewer Water warning sign



Figure 3.6.19 7800 South underpass



Figure 3.6.20 7800 South underpass, north side



Figure 3.6.27 Picnic area



Figure 3.6.28 Ducks along river



Figure 3.6.35 View from trail, looking south



Figure 3.6.36 Footbridge



Figure 3.6.21 Gardner Village trailhead



Figure 3.6.22 Picnic table and lawn area at Gardner Village Trailhead



Figure 3.6.29 Bird's nest along trail



Figure 3.6.30 Beaver damage



Figure 3.6.37 View to Jordan River from trail



Figure 3.6.38 Benches at 7000 South Trailhead



Figure 3.6.23 Trail north of Gardner Village trailhead, looking north



Figure 3.6.24 Trail winding through vegetation, looking south



Figure 3.6.31 Trail loop



Figure 3.6.32 High water sign



Figure 3.6.39 7000 South Trailhead sign



Figure 3.6.40 7000 South underpass



Figure 3.6.41 Footbridge north of 7000 South



Figure 3.6.42 View to north from trail



Figure 3.6.50 Winchester Street underpass



Figure 3.6.51 Winchester Street river crossing, looking north



Figure 3.6.43 View to east from trail



Figure 3.6.44 7000 South overlook & interpretive signage



Figure 3.6.46 Small neighborhood park adjacent to trail



Figure 3.6.47 Trail looking north



Figure 3.6.48 View to south from trail near 6400 South



Figure 3.6.49 Cyclist using trail

Issues and Opportunities

- There are no equestrian trails in this section of the Jordan River Corridor.
- Some equestrian users have expressed a desire to have a continuous equestrian trail corridor between Bluffdale and Murray at a minimum. The existing unpaved trail south of Bridge B8 dead-ends.
- Flooding occurs at the 7800 South underpass during peak river flow. There is flooding at the railroad bridge underpass north of Gardner Village during peak river flow.
- The 7800 South underpass is steep and has sharp turns - this is very dangerous for in-line skaters and pedestrians with strollers.
- There is no safe bicycle access to the trail along 7200 South traveling from the east. A gated community on the north side of the road does not allow public access to the trail corridor through their development.

Recommendations: Paved Trail

3.6 A Midvale Slag and Sharon Steel Site Development

The Midvale Slag and Sharon Steel sites are redeveloping. If Midvale City and the developers wish to develop river-side trails as part of their development, it will be their responsibility. The existing paved trail through this segment should remain the official Jordan River Trail.

For the safety and enjoyment of watercraft users on the river, there should be no additional bridges developed in this section. Trail users can access the trail corridor on the west side of the river at Gardner Village or the Midvale Trailhead using Center Street (7800 South) and the 7200 South Trailhead.

3.6 B Flooding at Center Street (7800 South) Underpass

These areas are subject to periodic flooding during period of peak run-off and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.6 C Dangerous Underpass at Center Street (7800 South)

This underpass is steep with sharp turns, which is very dangerous for in-line skaters and high traffic conditions. This will likely require an engineering solutions, perhaps when the 7800 South bridge is replaced or improved.

3.6 D Flooding at Railroad Bridge Underpass

These areas are subject to periodic flooding during period of peak run-off and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.6 E Streambank Erosion

Portions of the trail between Gardner Village and the railroad bridge have been washing away with high water levels this year. The County plans to eliminate the portions of these two loops that run closest to the river to avoid having to repair these sections continually in the future.

Recommendations: Equestrian Trail

3.6 F Equestrian Trails

There are no existing equestrian trails in this segment of the Jordan River Corridor, and no new equestrian trails are proposed due to width limitations within the corridor.

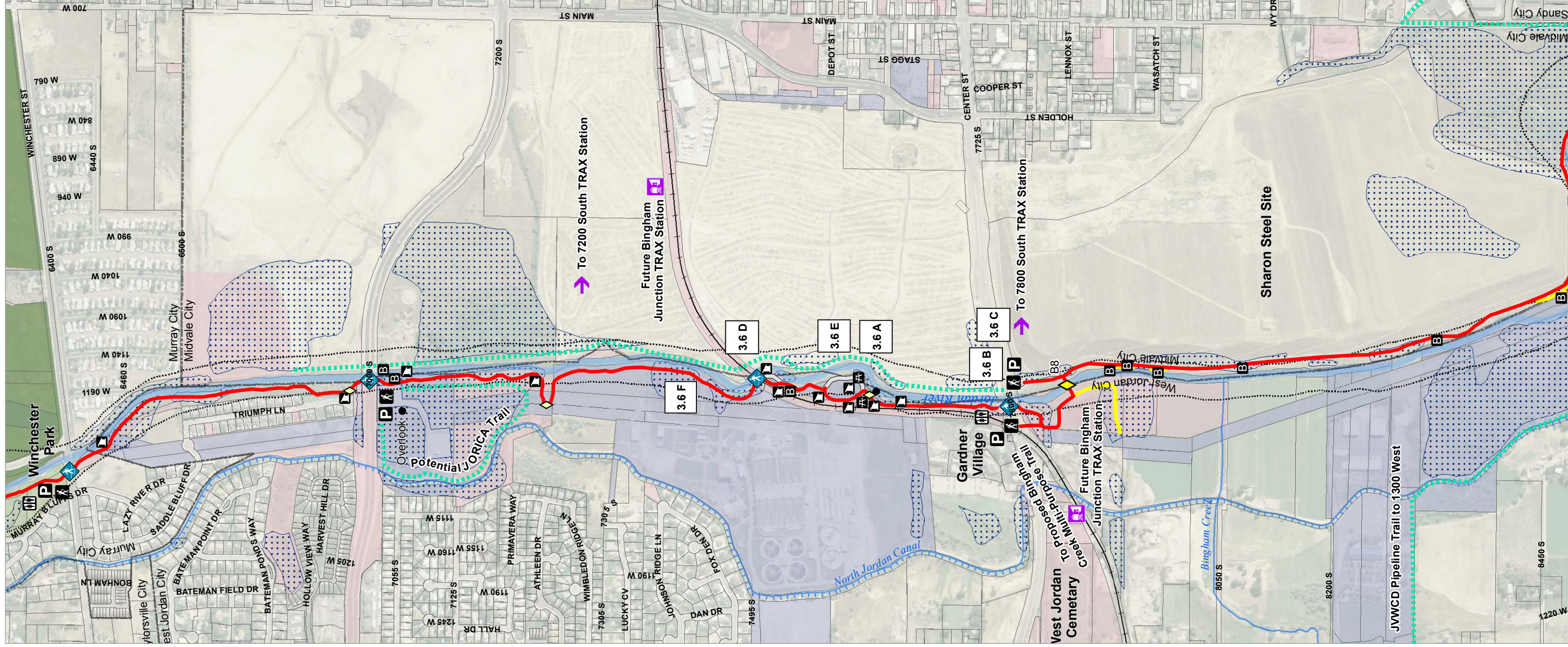
Construction Costs

Table 3.6.1– Construction Costs: Paved Trail Segment 6		
Feature Category	Map Code	CIP Budget Implication
Policy	3.6 A	N/A
Signage	3.6 B	\$1,000
Underpass	3.6C	\$100,000
Railroad Underpass	3.6 D	N/A
Trail Abandonment	3.6 E	\$5,000
Policy	3.6 F	N/A

Jordan River Trail Master Plan

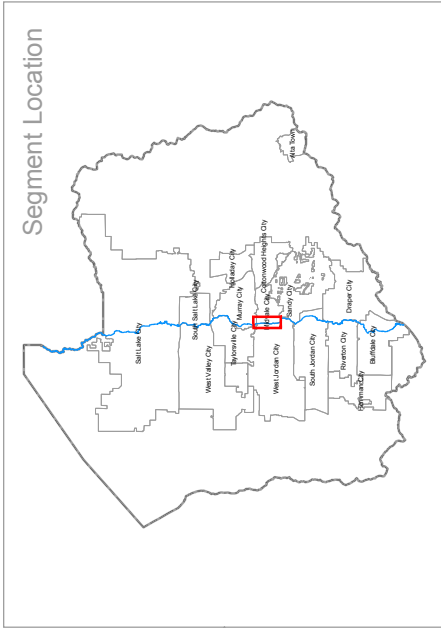
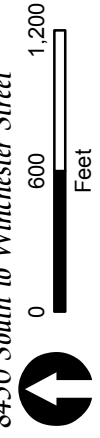
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
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- Miscellaneous**
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 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 6: West Jordan/Midvale/ Murray/Taylorsville

8450 South to Winchester Street



3.7 Segment 7: Murray/Taylorville (Winchester Street to Murray Taylorville Road)

The Jordan River Trail winds north through Winchester Park, looping through wetlands along the western bank of the Jordan River, with a view of the Murray Parkway Golf Course across the river to the east. Following the edge of Crystal River Drive, the trail continues north past Bridge B9 to a neighborhood access near Interstate 215. The main trail crosses the Jordan River at Bridge B9, where the trail can also be accessed from Murray Parkway Avenue. Trail users pass under Interstate 215 and follow along large looped pathways through a wooded area, and north to Cottonwood Grove Park. Equestrian access begins at Cottonwood Grove Park, with both trails crossing a small neighborhood street using an at-grade crossing.

Winchester Park

- Pavilion
- Restrooms
- Playground
- Picnic Tables
- Benches
- Trash Receptacles
- Parking (55 standard, 4 handicapped, 10 equestrian)



Figure 3.7.1 Winchester Park

The equestrian trail follows the edge of the Jordan River, with the paved trail running to the east, closer to the adjacent neighborhoods. The trail corridor can be accessed from several trail access points in these neighborhoods. The trails curve to the northeast as the river bends, and lead to Walden Park. Access to the neighborhood on the west side of the river is provided just south of 5400 South with Bridge B10.

The equestrian and paved trails pass under 5400 South with separate pathways along the river. The equestrian trail continues to follow the eastern edge of the Jordan River, passing Millrace Park across the river to the west, and features several bench and picnic rest areas along the way. The trail is washed out in two locations and has been rerouted. Horse riders continue over a couple of footbridges, past



Figure 3.7.2 View north from trail near Winchester Park



Figure 3.7.3 Feeding ducks in Winchester Park

Bridge B12 and Germania Park, and end their ride near the Kennecott Environmental Center.

Users of the paved trail continue north from the 5400 South underpass, and may access the trail from the 5400 South parking lot or from Millrace Park via the pedestrian bridge (Bridge B11). The paved pathway follows the curve of Murray Parkway Avenue leading users through Germania Park and past interpretive signage. Users can access the trail using Bridge B12, which provides paved access from a small parking lot on the south side of 4800 South, and from a private neighborhood access to the west. The trail continues to the northeast, running by the Kennecott Environmental Center, overlooks, and interpretive signage, then transitions to a recycled lumber boardwalk running through the wetlands. An informal, unpaved pathway continues from the end of the equestrian trail, following the Jordan River more closely, taking users through a wooded area along the south bank.



Figure 3.7.4 Winchester Park



Figure 3.7.5 "Do Not Feed the Ducks" warning sign



Figure 3.7.6 Winchester Park pond



Figure 3.7.7 Nesting Box

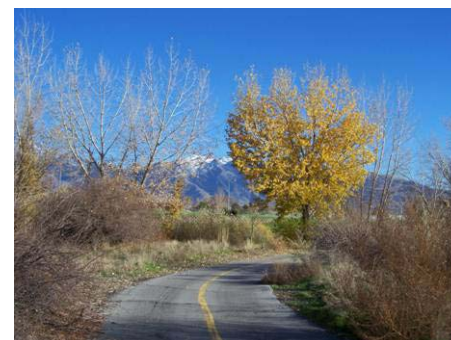


Figure 3.7.8 View looking east from trail



Figure 3.7.9 Wetlands north of Winchester Park



Figure 3.7.10 View from trail toward Murray Parkway Golf Course



Figure 3.7.11 Bridge B9
6' wide, 120' span, 16' clearance



Figure 3.7.12 View to south from Bridge B9

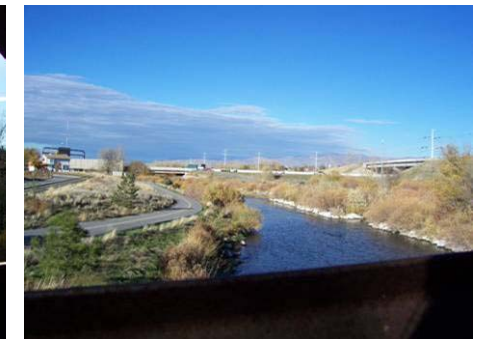


Figure 2.7.13 View to north from Bridge B9



Figure 3.7.14 View toward neighborhood access east of Bridge B9



Figure 3.17.15 View to south, near Interstate 215



Figure 3.7.16 Interstate 215 pedestrian underpass, looking north



Figure 3.7.17 In-line skater



Figure 3.7.18 Woods north of Interstate 215



Figure 3.7.19 Cottonwood Grove Park



Figure 3.7.25 Paved neighborhood access path



Figure 3.7.26 Trail looking north



Figure 3.7.32 View of playground from equestrian trail, west of Walden Park



Figure 3.7.33 5400 South vehicular crossing

Cottonwood Grove Park

- Open Lawn Area
- Trash Receptacles
- Parking (12 standard, 2 handicapped)



Figure 3.7.20 Cottonwood Grove Park



Figure 3.7.27 Paved neighborhood access path



Figure 3.7.28 Sand Volleyball at Walden Park



Figure 3.7.34 5400 South pedestrian and equestrian underpass



Figure 3.7.35 Germania Park Trailhead, north of 5400 South



Figure 3.7.21 View to north from Cottonwood Grove Park



Figure 3.7.22 At-grade crossing, north of Cottonwood Grove Park



Figure 3.7.29 Open lawn area with picnic tables at Walden Park



Figure 3.7.30 Bridge B10, 6' wide, 121' span, 15' clearance

Millrace Park

- Dog Park with small pavilions, benches, trash receptacles, and dog waste stations
- 2 Pavilions
- Playground
- Restrooms
- Baseball Diamonds
- Open Play Area
- Benches
- Picnic Tables
- Bike Rack
- Trash Receptacles
- Parking (98 standard, 7 handicapped)



Figure 3.7.36 Millrace Park



Figure 3.7.23 "Equestrian Trail Ends" sign near Cottonwood Grove Park



Figure 3.7.24 Equestrian trail running parallel to paved trail

Walden Park

- Restrooms
- Pavilion
- Picnic Tables
- Trash Receptacles
- Playground
- Sand Volleyball
- Benches
- Bike Rack
- Drinking Fountain
- Parking (35 standard, 2 handicapped)



Figure 3.7.31 Walden Park

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.7.37 Bench north of 5400 South



Figure 3.7.38 Bridge B11 8' wide, 132' span, 10' clearance



Figure 3.7.44 Picnic table along equestrian trail, west of Germania Park



Figure 3.7.45 Footbridge west of Germania Park



Figure 3.7.52 Informal unpaved trail that continues from the end of the equestrian trail, north of the Kennecott Environmental Center



Figure 3.7.53 Paved trail continuing north from 5400 South



Figure 3.7.39 Playground in Millrace Park, north of 5400 South

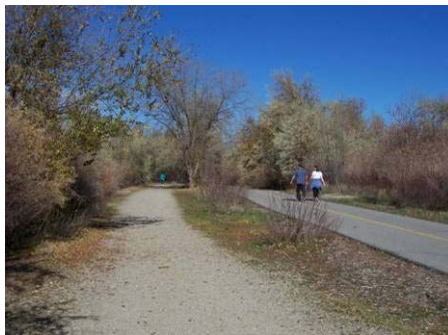


Figure 3.7.40 Equestrian trail running parallel to paved trail, north of 5400 South



Figure 3.7.46 Re-routed equestrian trail, washed out



Figure 3.7.47 Paved trail crosses equestrian trail near Bridge B12

Kennecott Environmental Center Trailhead

- Picnic Tables
- Drinking Fountain
- Bike Rack
- Trash Receptacles
- Parking (15 standard, 2 handicapped)




Figure 3.7.54 Kennecott Environmental Center Trailhead



Figure 3.7.41 Equestrian trail west of Germania Park



Figure 3.7.42 Washed out trail edge, west of Germania Park



Figure 3.7.48 Equestrian trail paralleling paved trail near Kennecott Environmental Center



Figure 3.7.49 Equestrian trail winding along river, near Kennecott Environmental Center

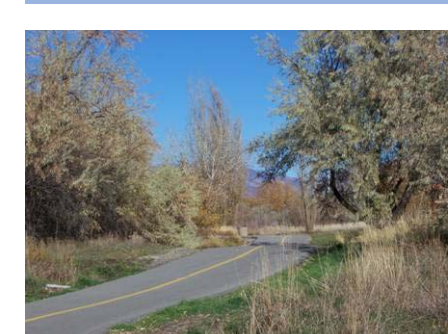


Figure 3.7.55 Trail continuing north



Figure 3.7.56 Interpretive signage near Germania Park

Germania Park Trailhead

- Open Play Area
- Picnic Tables
- Trash Receptacles
- Parking (64 standard, 4 handicapped, 10 equestrian)




Figure 3.7.43 Germania Park Trailhead



Figure 3.7.50 Bridge B12 8' wide, 122' span, 8' clearance



Figure 3.7.51 End of equestrian trail near Kennecott Environmental Center



Figure 3.7.57 Open lawn area near Germania Park



Figure 3.7.58 Footbridge near Germania Park



Figure 3.7.59 Play fields at Germania Park



Figure 3.7.60 Neighborhood trail access, north of Germania Park

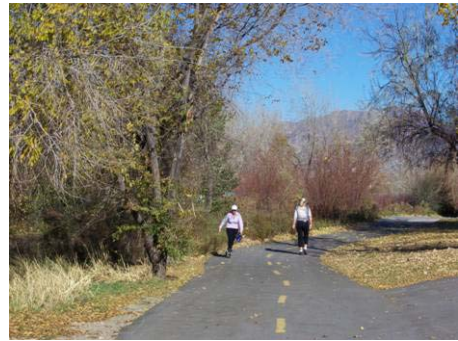


Figure 3.7.61 Trail near Kennecott Environmental Center



Figure 3.7.62 Kennecott Environmental Center Trailhead



Figure 3.7.63 Overlook east of Kennecott Environmental Center



Figure 3.7.64 Boardwalk through wetlands

Recommendations: Equestrian Trail

3.7 B Equestrian Trails

There are existing equestrian trails in this segment of the Jordan River Trail corridor from Bullion Street to approximately 4900 South. No new equestrian trails are proposed in this segment due to width limitations within the corridor.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.7 A	\$23,760
Policy	3.7 B	N/A

Issues and Opportunities

- There are no equestrian facilities in this section south of Bullion Street.
- The neighborhood access road west of Bridge B12 is not paved.

Recommendations: Paved Trail

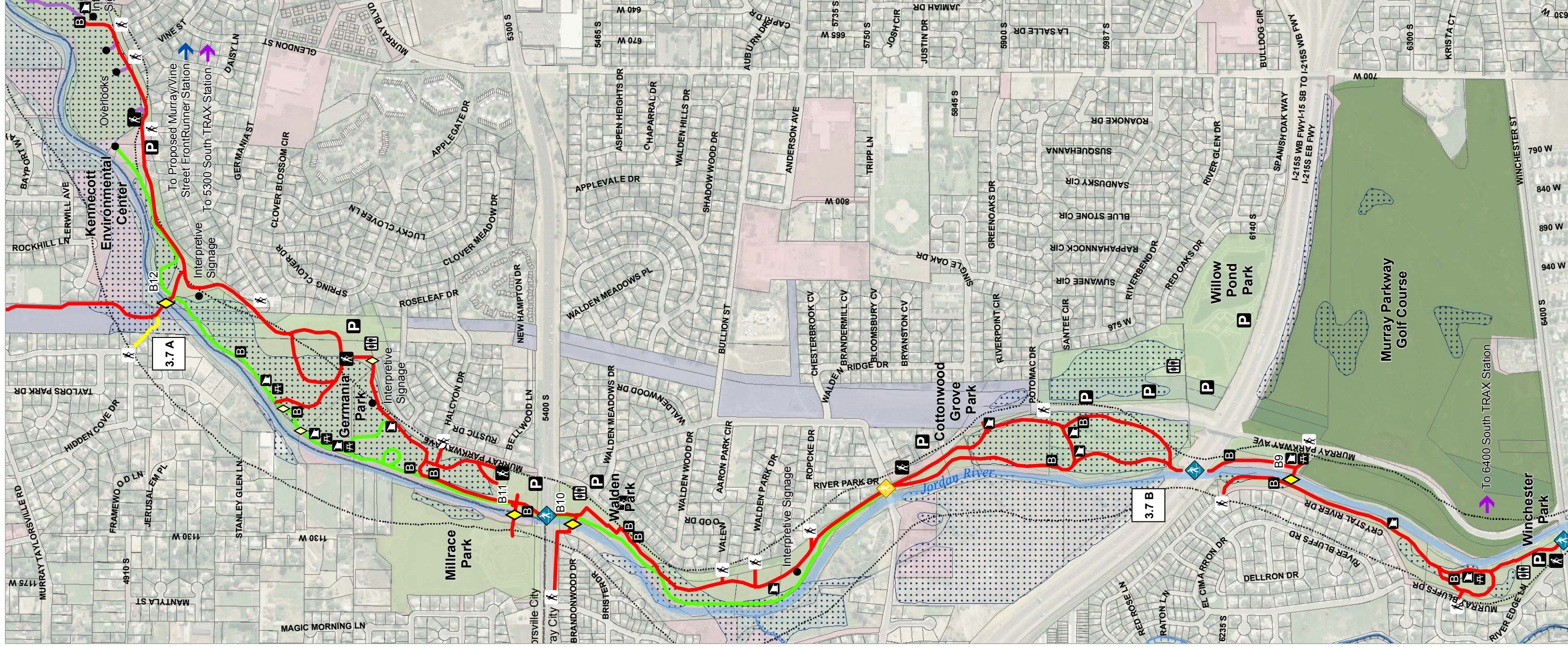
3.7 A Neighborhood Access Unpaved: Hidden Cove Drive

The unpaved trail from the trailhead at Hidden Cove Dr. in Taylorsville should be paved to connect to the Jordan River paved trail. This is a Murray City responsibility.

Jordan River Trail Master Plan

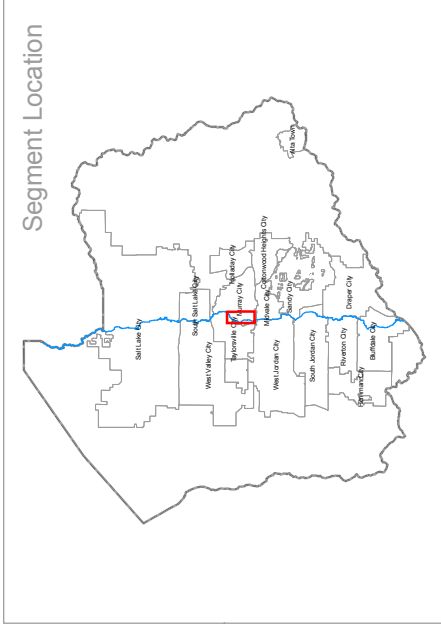
Paved Trail Facilities

- Future Element**
 - Future Element
 - Alternative Future Element
- Crossings**
 - Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
 - Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
 - Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
 - TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
 - Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 7: Murray/Taylorville

Winchester Street to Murray Taylorville Road



3.8 Segment 8: Murray/Taylorville/West Valley/South Salt Lake (Murray Taylorville Road (4800 South) to 3300 South)

The Jordan River paved trail continues north from the boardwalk through Arrowhead Park. The park is located on both the north and south sides of 4800 South, and a pedestrian underpass provides access to trail users. The trail crosses to the west bank of the Jordan River using Bridge B13. From here, the trail proceeds north, winding past a large apartment complex to the Taylorville Freedom Shrine. The trail crosses the Taylorville Expressway (4700 South) via a pedestrian underpass.

Following the course of the Jordan River, the trail curves to the northwest, bordering a large apartment complex near the Meadowbrook Golf Course. A couple of neighborhood trail access points connect to local neighborhoods. Just north of the Meadowbrook Golf Course, the trail crosses back to the east bank of the Jordan River with Bridge B14. The trail then crosses Meadow Brook Expressway (4100 South) via a new elevated pedestrian skywalk which connects trail users to General Holm Park on the north side of 4100 South. The path snakes through a large wooded natural space and passes a couple of benches on the way north to James Madison Park.



Figure 3.8.3 Arrow Park (South)



Figure 3.8.4 Arrow Park (South)

Taylorville Freedom Shrine

- Veterans/War Memorial
- Benches
- Drinking Fountain
- Trash Receptacles



Figure 3.8.11 Taylorville Freedom Shrine



Figure 3.8.5 4800 South underpass



Figure 3.8.6 Bridge B13
8' wide, 92' span, 14' clearance



Figure 3.8.12 Taylorville Freedom Shrine



Figure 3.8.13 4700 South pedestrian underpass

Arrowhead Park (South of 4800 South)

- Picnic Tables
- Benches
- Interpretive Signage
- Restrooms
- Bike Racks
- Trash Receptacles
- Parking (18 standard, 2 handicapped)



Figure 3.8.1 Arrow Park (South)



Figure 3.8.7 View to south from Bridge B13

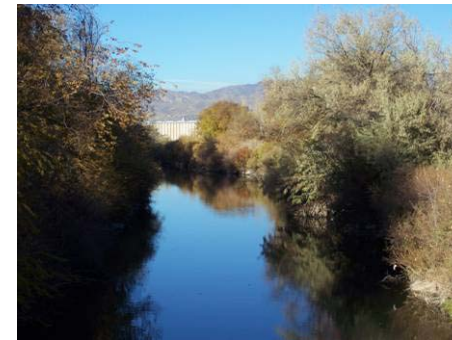


Figure 3.8.8 View to north from Bridge B13



Figure 3.8.14 Trail looking east toward pedestrian underpass



Figure 3.8.15 Trail north of 4700 South

Arrowhead Park (North of 4800 South)

- Picnic Tables
- Benches
- Trash Receptacles
- Parking (27 standard, 2 handicapped)



Figure 3.8.2 Arrow Park (North)



Figure 3.8.9 Trail north of Bridge B13, looking south



Figure 3.8.10 Trail looking north, near Taylorville Freedom Shrine



Figure 3.8.16 Trail adjacent to apartment complex north of 4800 South

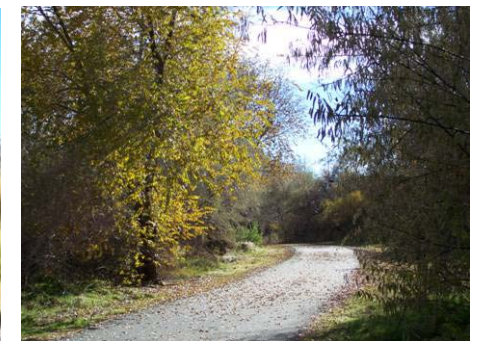


Figure 3.8.17 Trail east of Meadowbrook Golf Course



Figure 3.8.18 View of Jordan River from trail



Figure 3.8.19 Trail north of Meadowbrook Golf Course



Figure 3.8.25 Winding through woods north of General Holm Park



Figure 3.8.26 Bench along path north of General Holm Park



Figure 3.8.33 Trail looking north



Figure 3.8.34 Improvised seating



Figure 3.8.20 Bridge B14
8' wide, 105' span, 11' clearance



Figure 3.8.21 3900 South pedestrian sky bridge



Figure 3.8.27
Conservation Area sign



Figure 3.8.28 View from trail looking north



Figure 3.8.35 Bollard/chain gate south of James Madison Park

General Holm Park – M.G. Holm WWII Veterans Commemorative Trailhead

- Pavilion
- Restrooms
- Playground
- Interpretive Signage
- Picnic Tables
- Benches
- Trash Receptacles
- Parking (23 standard, 2 handicapped)



Figure 3.8.22 General Holm Park



Figure 3.8.29 View to west bank of Jordan River from trail



Figure 3.8.30 View to south from trail

Recommendations: Paved Trail

3.8 A Proposed Trails west of Jordan River in West Valley
Trails have been proposed in West Valley City north of Meadow Brook Expressway to 3300 South. The existing Jordan River Trail on the east side of the river should remain the official trail. If West Valley City wishes to develop a trail on the west side, it should be a West Valley City trail and not a Jordan River Trail.

For the safety and enjoyment of watercraft users on the river, there should be no bridges developed in this section. Users may use Meadow Brook Trail and 3300 South Street to access the Jordan River Trail corridor on the east side of the river in South Salt Lake City.

Recommendations: Equestrian Trail

3.8 C Equestrian Trails

There are no existing equestrian trails in this segment of the Jordan River Trail corridor, and no new equestrian trails are proposed due to width limitations within the corridor.



Figure 3.8.23 General Holm Park



Figure 3.8.24 General Holm Park Trailhead



Figure 3.8.31 Access path to UTA facility



Figure 3.8.32 Bench along trail

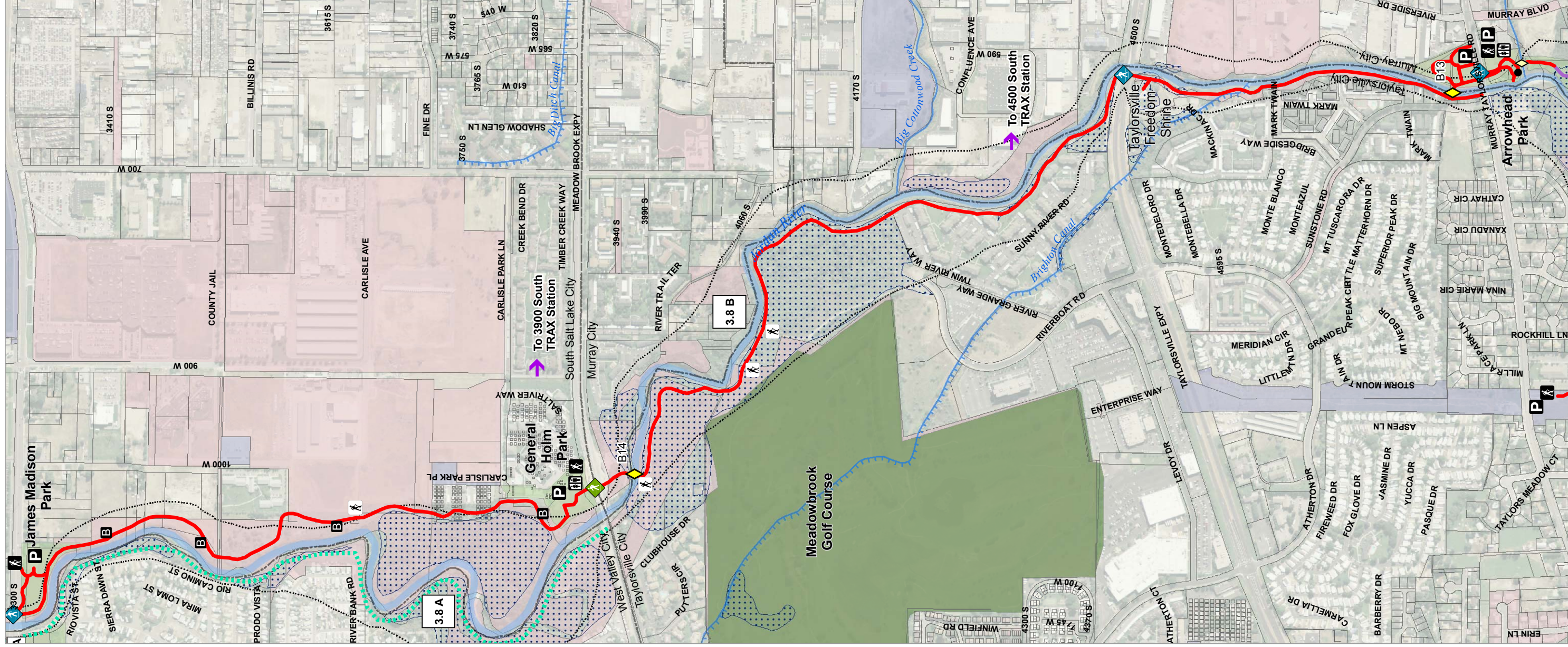
Construction Costs

Table 3.8.1– Construction Costs: Paved Trail Segment 8		
Feature Category	Map Code	CIP Budget Implication
Policy	3.8 A	N/A
Policy	3.8 B	N/A

Jordan River Trail Master Plan

Paved Trail Facilities

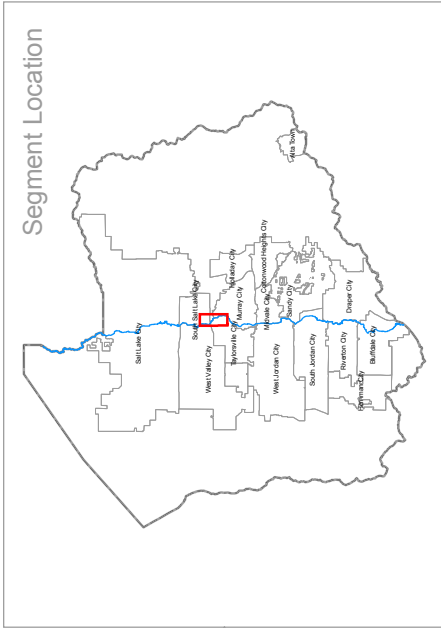
- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 8: Murray/Taylorville/ West Valley/S. Salt Lake

Murray Taylorville Road to 3300 South

0 600 1,200 Feet



3.9 Segment 9: West Valley/South Salt Lake/Salt Lake City (3300 South to 1700 South)

The paved trail travels north through James Madison Park, on the south side of 3300 South, and continues under the street via a pedestrian underpass. The trail follows the large bend in the river, passing the Oxbow Jail and Utah Cultural Celebration Center, through large open spaces of vegetation. The Oxbow Trailhead, north of the jail, provides trail access and parking.

The Jordan River Trail curves back to the north, and forks just north of the Oxbow Trailhead near Bridge B15. The east fork continues north along the east bank of the Jordan River, crossing a footbridge and passing a light industrial area. The paved portion of this trail ends at Paul Workman Ball Park, near Bridge B16. A short unpaved trail continues north from here, stopping at SR-201.

The west fork crosses the Jordan River at Bridge B15 and travels north to the Redwood Nature Area. There is an informal gravel trail to an overlook north of the substation. The main trail loops through the Redwood Nature area, a large wooded area, with several benches scattered throughout. The south end of this loop takes users to the Redwood Multipurpose Center just off of Redwood Road and 3100 South. The north side of this loop takes users to a neighborhood trail access point just off of Castell Street. The trail continues north along the western bank of the Jordan River, past several small hobby farms, and through the Redwood Trailhead Park, where users can access Paul Workman Ball Park on the east side of the river using Bridge B16.

James Madison Park

- Restrooms
- Picnic Tables
- Trash Receptacles
- Open Play Area
- Parking (26 standard, 1 handicapped)



Figure 3.9.1 James Madison Park

The trail travels north from Redwood Trailhead Park through a small open space and passes under SR-201 and 2100 South using pedestrian underpasses. Glendale Golf Course is located just north of 2100 South on the west side of the Jordan River. The paved trail travels through the golf course for a short section, going around a dam and crossing a large canal using a footbridge. The trail takes users past a small peninsula of open meadow surrounded by a bend in the river, and then the trail crosses to the eastern bank of the river using Bridge B17. The trail is lighted north of this bridge throughout most of the Salt Lake City portion.

Continuing north past industrial areas, the trail crosses the railroad tracks with a special gated crossing which forces users to slow down and be aware as they cautiously move across the tracks and travel towards Raging Waters.



Figure 3.9.4 View towards James Madison Park

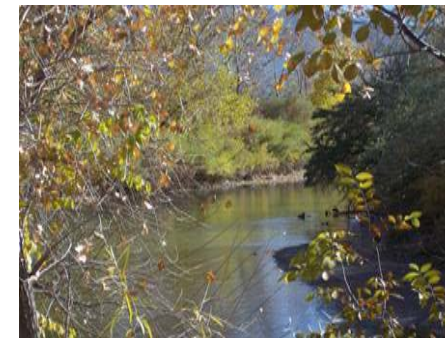


Figure 3.9.5 View to Jordan River from James Madison Park



Figure 3.9.6 Trail north of 3500 South underpass, looking south



Figure 3.9.7 Trail looking north, near Oxbow Jail

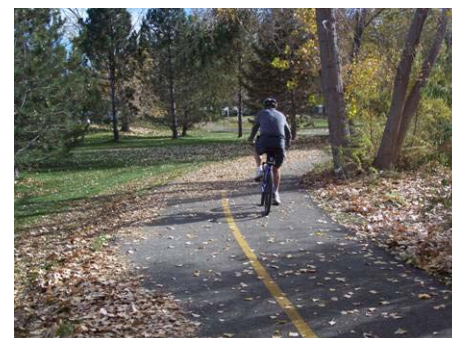


Figure 3.9.8 Cyclist near Oxbow Jail



Figure 3.9.9 View of Jordan River from trail



Figure 3.9.10 Trail through wooded area north of Oxbow Jail



Figure 3.9.11 Parking at Oxbow Trailhead

Oxbow Trailhead

- Trailhead Sign & Map
- Parking for approximately 4 vehicles (not striped)



Figure 3.9.12 Oxbow Trailhead



Figure 3.9.13 View to south from east side of Bridge B15



Figure 3.9.14 Bridge B15 10' wide, 120' span, 15' clearance



Figure 3.9.2 3500 South pedestrian underpass, looking north



Figure 3.9.3 James Madison Trailhead sign



Figure 3.9.15 East fork of trail north of Bridge B15



Figure 3.9.16 East fork of trail, looking north

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.9.17 Footbridge near 2900 South industrial area



Figure 3.9.18 View from footbridge towards dredging piles along the banks of the Jordan River



Figure 3.9.25 Ducks near Redwood Nature Area



Figure 3.9.26 Open space near Redwood Nature Area



Figure 3.9.33 "Restoration Area" sign near Lester Street Trailhead



Figure 3.9.34 Trail to Redwood Paved Center



Figure 3.9.19 Dredging piles along Jordan River near Redwood Nature Area



Figure 3.9.20 Hastings Cutoff historical marker along east fork of trail



Figure 3.9.27 Bench in Redwood Nature Area



Figure 3.9.28 Picnic table in Redwood Nature Area

Redwood Multipurpose Center/Lester Street Trailhead

- Community Center
- Baseball Diamonds
- Trailhead Sign & Map
- Roadside Parking



Figure 3.9.35 Redwood Multipurpose Center/Lester Street Trailhead



Figure 3.9.21 View to northwest, near Redwood Nature Area



Figure 3.9.22 View to north from Bridge B15



Figure 3.9.29 Open meadow in Redwood Natural Area



Figure 3.9.30 Nesting box near Power Substation



Figure 3.9.36 Trail through west end of Redwood Nature Area



Figure 3.9.37 Canal running through Redwood Nature Area



Figure 3.9.23 Bench near Bridge B15



Figure 3.9.24 Trail looking north, near Bridge B15



Figure 3.9.31 Oxbow Trailhead sign



Figure 3.9.32 Trail Crossing west of Power Substation



Figure 3.9.38 Drop-down bridge crossing canal in Redwood Nature Area



Figure 3.9.39 View towards Castell Street trail access point



Figure 3.9.40 Dredging piles and garbage east of Redwood Nature Area



Figure 3.9.41 Llama at hobby farm north of Redwood Nature Area, along trail



Figure 3.9.47 View of boat ramps at Redwood Trailhead Park



Figure 3.9.48 Bridge B16, 10' wide, 165' span, 11' clearance



Figure 3.9.54 Pedestrian underpass, 2100 South, looking north



Figure 3.9.55 Maintenance access footbridge north of 2100 south, no public access



Figure 3.9.42 Horse property along trail north of Redwood Nature Center



Figure 3.9.43 Neighborhood trail access north of Redwood Nature Area

Paul Workman Ball Park

- Ball Diamond
- Soccer Field
- Restrooms
- Concession Building
- Drinking Fountain
- Bike Rack
- Trash Receptacles
- Parking (20 standard, 1 handicapped)



Figure 3.9.49 Paul Workman Ball Park



Figure 3.9.56 Diversion Dam near Glendale Golf Course



Figure 3.9.57 Footbridge through Glendale Golf Course

Redwood Trailhead Park

- Pavilion
- Playground
- Picnic Tables
- Restrooms
- Drinking Fountain
- Trash Receptacles
- Parking (35 standard, 1 handicapped)



Figure 3.9.44 Redwood Trailhead Park



Figure 3.9.50 Redwood Trailhead Park trailhead sign



Figure 3.9.51 Trail north of Redwood Trailhead Park, looking north



Figure 3.9.58 View from footbridge toward diversion dam and 2100 South overpass



Figure 3.9.59 Bridge B17, 10' wide, 100' span, 8' clearance



Figure 3.9.45 Trail near Redwood Trailhead Park, looking south



Figure 3.9.46 Redwood Trailhead Park



Figure 3.9.52 Trail near SR 201, looking east



Figure 3.9.53 Pedestrian underpass SR 201



Figure 3.9.60 Trail north of diversion dam, looking northeast



Figure 3.9.61 Open meadow in horseshoe bend north of 2100 South



Figure 3.9.62 Open meadow in horseshoe bend



Figure 3.9.63 Trail north of horseshoe bend



Figure 3.9.64 Gated railroad crossing for bikes and pedestrians



Figure 3.9.65 Trail south of Raging Waters, looking south

Issues and Opportunities

- Large dredge piles east of the Redwood Nature Area have been deposited on both banks of the Jordan River. These areas are collection grounds for garbage and weeds, and create an unattractive environment for trail users.
- Thickly vegetated areas near the Redwood Nature Center and Oxbow Jail are common locations for crime and other unacceptable behaviors.
- The trail pavement has been broken up for several years in this segment from root growth and has not been repaired.
- Trailheads at Paul Workman Ballpark and Redwood Trailhead Park will serve both Jordan River Trail users and Parley's Trail users in the future, and need to accommodate those needs in the future as they arise.

Recommendations: Paved Trail

3.9 A Flooding at 3300 South Underpass

These areas are subject to periodic flooding during period of peak runoff and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.9 B Proposed Trails in West Valley

Trails have been proposed in West Valley City between 3300 South and Bridge B15 at approximately 3100 South. The existing Jordan River Trail on the east side of the river will remain the official Jordan River Trail, and a portion of trail on the west side of the river from the bridge south to the property line for the Utah Cultural Celebration Center should be paved. West Valley City intends to develop a trail on the west side through the Utah Cultural Celebration Center.

For the safety and enjoyment of watercraft users on the river, there should be no additional bridges developed in this section. Users may use 3300 South Street to access the Jordan River Trail corridor on the east side of the river in South Salt Lake City.

3.9 C Maintenance Issues

The section of trail in South Salt Lake City between 2900 South and Paul Workman Park has dredge materials, weeds, and debris that should be cleaned-up.

Thickly vegetated areas near Redwood Nature Center and Oxbow Jail need to be controlled as this is an area where crime and unacceptable behaviors are common.

The trail pavement has been broken up and should be repaired.

Recommendations: Equestrian Trail

3.9 E Equestrian Trails

There are no existing equestrian trails in this segment of the Jordan River Trail, and no new equestrian trails are proposed due to width limitations within the corridor. It is undesirable to create new segments of equestrian trail that do not have the potential to be linked to other equestrian segments in the future.

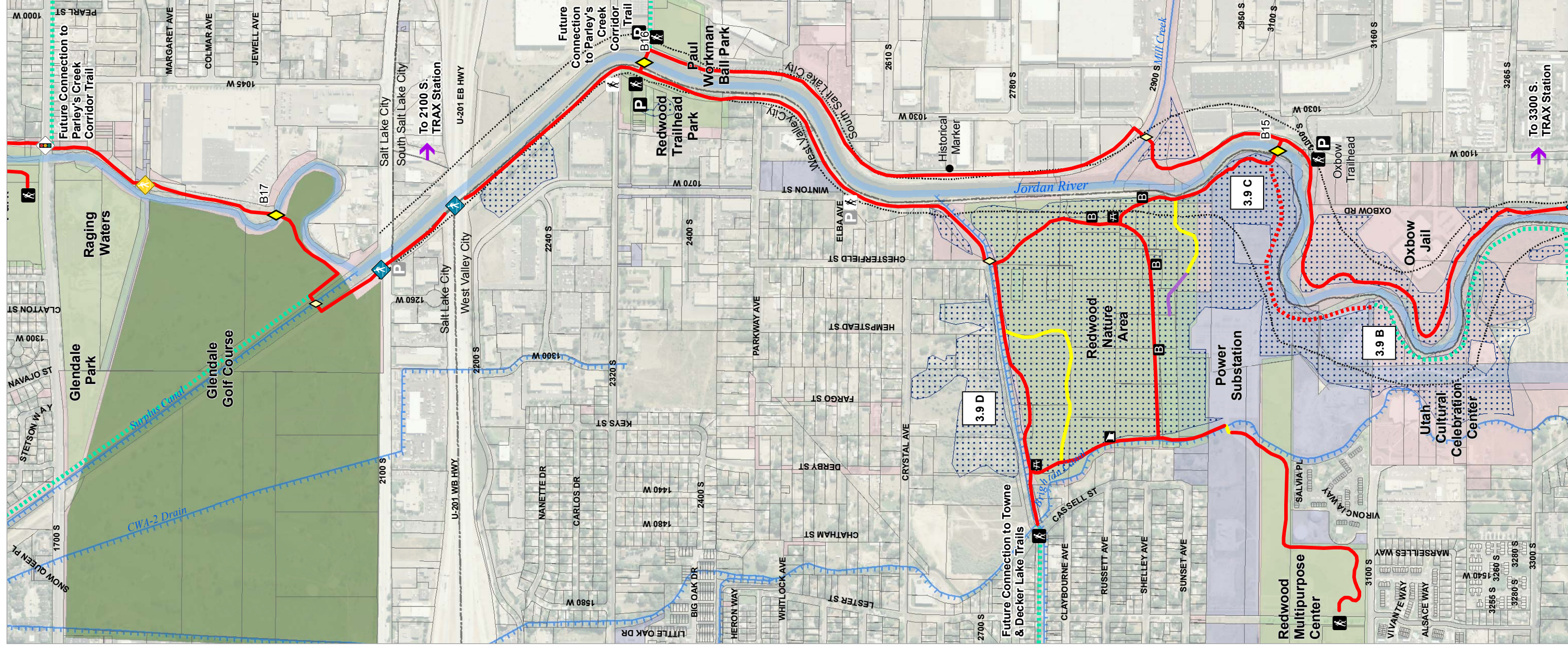
Construction Costs

Feature Category	Map Code	CIP Budget Implication
Signage	3.9 A	\$1,000
Policy	3.9 B	N/A
Maintenance	3.9 C	\$15,000
Policy	3.9 D	N/A

Jordan River Trail Master Plan

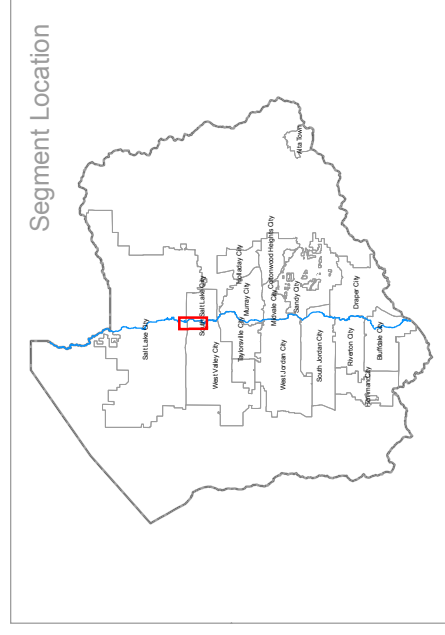
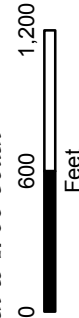
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 9: West Valley/South Salt Lake/Salt Lake City

3300 South to 1700 South



3.10 Segment 10: Salt Lake City (1700 South to Interstate 80)

The Jordan River Trail continues north towards Raging Waters. The Canoe Marina donated by the Exchange Club is located along the eastern edge of the Raging Waters parking lot. The trail then crosses 1700 South via a signalized crosswalk to Glendale Park. One trail segment begins in the parking lot and traces the west bank of the Jordan River along the east end of the park, and curves to the west as the river bends. This portion of the trail ends at a trail access point along 1200 West.



Figure 3.10.1 Canoe Marina donated by the Exchange Club, Raging Waters parking lot



Figure 3.10.2 Trail east of Canoe Marina, looking south

The main trail segment follows the eastern bank of the river, passing an informal neighborhood park featuring the “River of Words”

Peace Labyrinth and an informal pathway trail. This pathway trail also accesses the neighborhood on 1200 West via Bridge B18 which is not ADA accessible, along unpaved pathways. The trail continues north through residential neighborhoods, and crosses to the west side of the river just south of California Avenue at Bridge B19. The trail then crosses California Avenue with a signalized crosswalk, passes through a small neighborhood park/trailhead, and winds back to the east.

The trail then curves back around to the north and passes through a small neighborhood park. As the river bends to the west, one segment of the trail continues along the south side of the river, heading west through Modesto Nature Park/Trailhead, then follows the river as it

Glendale Park

- Restrooms
- Soccer Fields
- Small Pavilions
- Picnic Tables
- Trash Receptacles
- Parking (28 standard, 1 handicapped)



Figure 3.10.3 Glendale Park

turns north again. This trail segment ends at the sidewalk on Fremont Avenue. The main trail segment crosses the river in the neighborhood park at Bridge B20, and follows the Jordan River through the “Bend in the River Park”.



Figure 3.10.4 Glendale Park

The Jordan River Trail continues across Fremont Drive on the east side of the river with a non-signalized crosswalk where warning flashers will be installed soon, and travels north along the west edge of Jordan Park and the International Peace Gardens.

Neighbors can access the trail at Brooklyn Avenue and travel north along the trail, which crosses back to the east bank of the river at Bridge B22, or they can cross Bridge B21 to access the trail along the east bank. The trail traces the west edge of the park and curves along its northern end. The trail then ends near Montague Avenue where a utility building and raised railroad tracks present an unsafe crossing for trail users. The trail picks up again north of the railroad tracks on 900 South.

The trail passes through the Ninth South Park along the eastern bank of the river, where neighbors can access the trail using Bridge B22. Continuing north, the trail crosses 800 South using a signalized crosswalk and switches to the west bank of the river. From here, the trail continues north for a short distance where it crosses the river again at Bridge B23, then crosses 700 South using a crosswalk. The trail can be accessed from 600 South via Bridge B24 and from a small neighborhood park along Jake Gam Boulevard. The trail then crosses 500 South using an un-signalized crosswalk, passing through a small “Olympic” themed mini park, and travels north crossing 400 South (signalized) and 300 South (crossing flags) with crosswalks. The trail then winds to the west, following the curves of the Jordan River, through the small



Figure 3.10.6 Neighborhood Trail Access at Bridge B18



Figure 3.10.7 Non-accessible path to Bridge B18



Figure 3.10.5 “Salt Lake City Gets Fit Together” sign



Figure 3.10.8 Bridge B18 4' wide, 69' span, 10' clearance



Figure 3.10.9 River looking north from 1700 South

Alzheimer’s Association Wildlife Grove, and ends at Interstate 80.



Figure 3.10.11 View from trail looking west toward Glendale Park



Figure 3.10.11 Trail west of Glendale Park, near Riverside Drive



Figure 3.10.12 Utility Structure near informal Labyrinth path



Figure 3.10.13 “River of Words” Peace Labyrinth



Figure 3.10.14 Mosaic benches surround Labyrinth



Figure 3.10.15 Trail north of Labyrinth



Figure 3.10.16 River north of Labyrinth



Figure 3.10.17 Bench near Bridge B19



Figure 3.10.23 View of river and trail along 1300 South, looking east



Figure 3.10.24 View of trail along 1300 South near 1000 West



Figure 3.10.29 Art wall and seating in Neighborhood Park



Figure 3.10.30 Overhead structure in Neighborhood Park



Figure 3.10.18 Bridge B19
10' wide, 100' span, 11' clearance



Figure 3.10.19 California Avenue overpass



Figure 3.10.25 Trail along 1300 south where river bends to the north



Figure 3.10.26 View from trail at 1300 South, looking east

Bend in the River Park

- Urban Tree House and Green Space
- Benches
- Drinking Fountain
- Boardwalk
- Sculpture
- Parking (9 standard, 1 handicapped)



Figure 3.10.32 Bend in the River Park

Neighborhood Park/Trailhead (North of California Ave.)

- Benches
- Picnic Table
- Bike Rack
- Drinking Fountain
- Information Kiosk
- Parking (4 standard, 1 handicapped)



Figure 3.10.20 Neighborhood Park

Modesto Nature Park/Trailhead

- Boat Launch
- Playground
- Picnic Tables
- Benches
- Bike Rack
- Canvas Awning
- Marshlands
- Informal Pathways
- Open Play Area
- Drinking Fountain
- Flag Pole



Figure 3.10.27 Neighborhood Park



Figure 3.10.28 Boat ramp in Neighborhood Park



Figure 3.10.32 Bridge B20
10' wide, 100' span, 6' clearance



Figure 3.10.33 Urban Tree House, north of Bridge B20



Figure 3.10.21 Pedestrian traffic signal at California Avenue



Figure 3.10.22 Pedestrians near neighborhood park

Jordan Park/International Peace Gardens

- Baseball Diamonds
- Horseshoe Pits
- Pavilions
- Tennis Courts
- Playgrounds
- Skate Park
- Sand Volleyball
- Picnic Tables
- Restrooms
- Trash Receptacles



Figure 3.10.34 Jordan Park

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.10.35 Jordan River Trail north of Fremont Ave, in Jordan Park



Figure 3.10.36 Trail along western edge of International Peace Gardens



Figure 3.10.43 Non-accessible railroad crossing south of 900 South



Figure 3.10.44 Trail starting point dead-ends at railroad tracks, south of 900 south



Figure 3.10.50 Bridge B23 10' wide, 100' span, 11' clearance



Figure 3.10.51 Trail north of 700 South



Figure 3.10.37 Bridge B21 10' wide, 100' span, 6' clearance



Figure 3.10.38 Pier west of International Peace Gardens



Figure 3.10.45 Trail access point, 900 South looking north



Figure 3.10.46 Trail north of 800 South



Figure 3.10.52 Small linear park near Jake Garn Boulevard



Figure 3.10.53 Bridge B24 7' wide, 77' span, 6' clearance



Figure 3.10.39 Pathway to Jordan River Trail from Peace Gardens

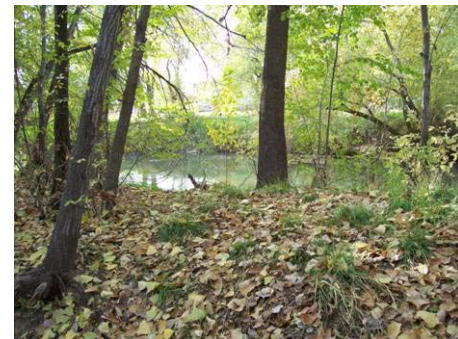


Figure 3.10.40 View to river from trail

- Ninth South Park**
- Picnic Tables
 - Restrooms
 - Trash Receptacles
 - Parking (18 standard, 2 handicapped)



Figure 3.10.47 Ninth South Park



Figure 3.10.54 View of river near Jake Garn Boulevard



Figure 3.10.55 Trail looking north towards 500 South crossings



Figure 3.10.41 View to railroad crossing north of Peace Gardens



Figure 3.10.42 Improvised footpath north of Peace Gardens



Figure 3.10.48 Bridge B22 10' wide, 70' span, 10' clearance

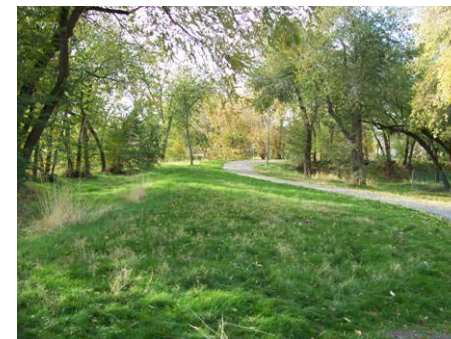


Figure 3.10.49 Trail south of 700 South



Figure 3.10.56 "Trail Crossing" sign along 500 South

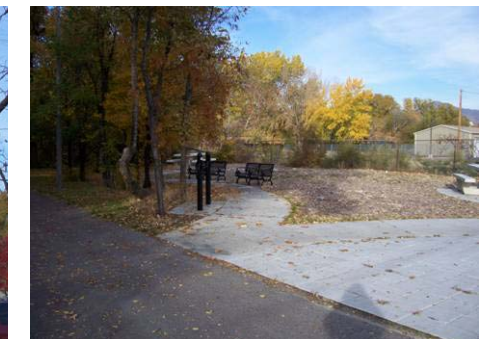


Figure 3.10.57 Olympic Legacy Park, north of 500 South

Olympic Legacy Park (500 South)

- Benches
- Picnic Table
- Bike Rack
- "Prometheus Fire Bearing" Sculpture
- Olympic Themed Site Furniture



Figure 3.10.58 500 South Olympic Park

Alzheimer's Association Wildlife Grove

- Benches
- Picnic Table
- Trash Receptacle



Figure 3.10.65 Alzheimer's Association Wildlife Grove

- The paved trail ends just south of Interstate 80, with a short portion of unpaved trail continuing to the Interstate 80 overpass and does not resume until trail access is provided at North Temple Street in Segment 11.

Recommendations: Paved Trail

3.10 A Unpaved Trail: "River of Words" Peace Labyrinth

The northern unpaved trail at the "River of Words" Peace Labyrinth should be considered part of the Jordan River Trail corridor. The trail should be paved in this area as part of the request by the surrounding community for the park to be upgraded to an Urban Fishery. The southern portion of the trail should remain unpaved.

The neighborhood access from Bridge B28 should be paved from the park all the way to the sidewalk on 1200 West, making it ADA accessible and a connection should be made to the paved trail that traces the northern boundary of Glendale Park.

3.10 B Paved trail Gap East Bank: 900 South Railroad Tracks

There is a short gap in the east bank trail as it crosses 900 South Street and the railroad. The railroad track along 900 South is being abandoned and will be removed in 2008. The abandoned right-of-way will become the property of Salt Lake City. The trail should be connected across 900 South to connect Jordan River trails in Jordan Park to Ninth South Park.

A trail access point should be developed at this point with signage, which would also serve as a trail corridor connection to a future 900 South trail heading east to Liberty Park developed by Salt Lake City.

For the safety and enjoyment of watercraft users on the river, there should be no additional bridges developed in this section.

The trail needs signage directing users to continue west across Bridge B21.

3.10 C Unpaved trail: Just South of I-80

The unpaved trail just south of I-80 should be paved.

3.10 D Interstate 80 Underpass

Salt Lake City has secured funding to develop a pedestrian underpass at Interstate 80 that will take users under the freeway, although the paved trail currently ends just south of Interstate 80 and does not resume until North Temple. The funding is only for the underpass design and construction, not to continue the trail to North Temple.

Recommendations: Equestrian Trail

3.10 E Equestrian Trails

There are no existing equestrian trails in this segment of the Jordan River Trail, and no new equestrian trails are proposed due to width limitations within the corridor.



Figure 3.10.59 Trail north of Olympic Legacy Park



Figure 3.10.60 Trail looking north toward 400 South crossing



Figure 3.10.66 View of trail, north of 300 South



Figure 3.10.67 Monument sign in Alzheimer's Association Wildlife Grove



Figure 3.10.61 400 South overpass



Figure 3.10.62 "Jordan River Parkway" sign north of 400 South



Figure 3.10.68 Trail south of Interstate 80



Figure 3.10.69 View under Interstate 80 overpass



Figure 3.10.63 Trail north of 400 South, looking north



Figure 3.10.64 300 South crossing

Issues and Opportunities

- The area near the "River of Words" Peace Labyrinth is largely undeveloped, and the community is pushing for it to be developed as an Urban Fishery.
- There is no paved trail on either end of Bridge B18, and narrow paths with sizeable grade changes make this bridge inaccessible.
- There is a lack of proper signage directing trail users to the main trail across bridge B21 on the west bank of the river. The east bank trail stops at the north end of Jordan Park/International Peace Gardens near a utility building and the railroad tracks make this portion of the trail inaccessible. There is no pedestrian rail barrier at this location.

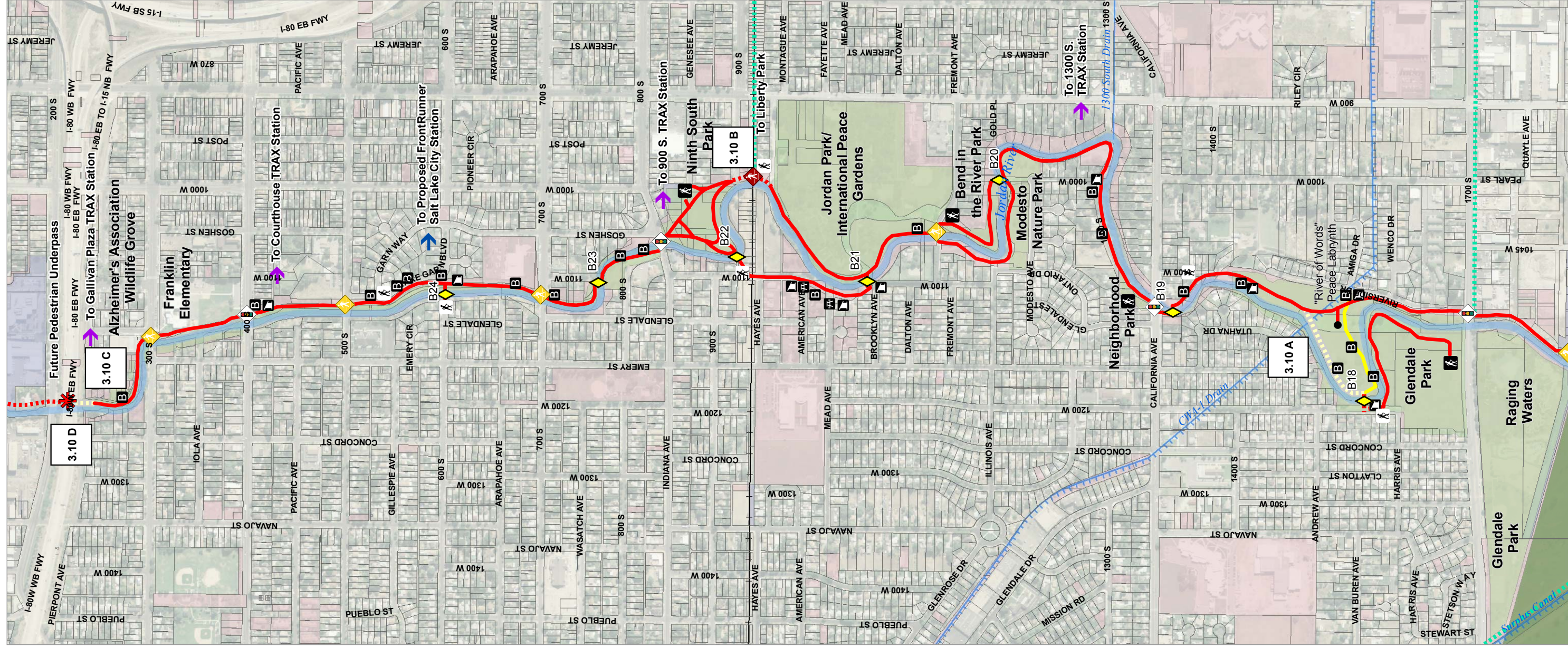
Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.10 A	\$47,520
Paved Trail/Trail Access	3.10 B	\$25,760
Paved Trail	3.10 C	\$23,760
Underpass	3.10 D	Funded
Policy	3.10 E	N/A

Jordan River Trail Master Plan

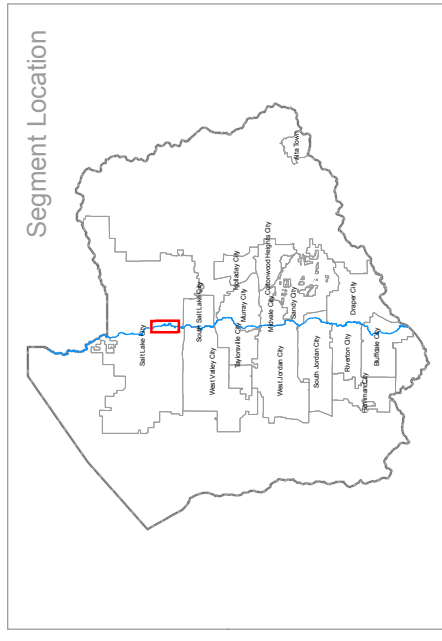
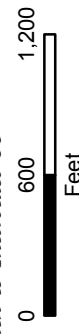
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
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 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Unpaved Jordan River Trail
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 10: Salt Lake City

1700 South to Interstate 80



3.11 Segment 11: Salt Lake City (Interstate 80 to DuPont Avenue)

There is a gap in the Jordan River Trail corridor between Interstate 80 and North Temple Street. The trail resumes just west of the Utah State Fair Park at a trail access point on the north side of North Temple Street. The trail follows along the eastern bank of the Jordan River for a short distance, before crossing at Bridge B25 to the western bank of the river. The trail continues north for a small segment where it splits. An equestrian trail continues north along the western edge of the river, passing Bridge B27, ending in Cottonwood Park.

The paved trail once again crosses the river at Bridge B26, back to the eastern bank of the Jordan River, bypassing the private Utah State Fair Park pathways. This segment curves along the southwestern edge of Constitution Park, which provides trail access, through linear open space past Bridge B27, and winds through a small portion of Cottonwood Park located on the north side of the Jordan River. The trail connects to the main area of Cottonwood Park using Bridge B28. The Jordan River Trail continues north past the Cottonwood Park area, across 500 North with an un-signalized crosswalk. A pedestrian warning flasher will soon be installed at this location.

The trail continues north past a bench and trash receptacle on the north side of 500 North to Bridge B29, where the trail splits. The paved trail crosses Bridge B29 to the eastern bank of the Jordan River, traveling past Backman Elementary School where trail access is provided, and crosses 700 North with a signalized crosswalk. The trail continues north, tracing the western edge of Riverside Park, Past Bridge B30 which crosses the river and provides access to the equestrian trail on the opposite bank. The trail then follows the river as it bends eastward through a large open space near Cornell Street, across the river from the Day Riverside Library.

The equestrian trail resumes at Bridge B29 and follows the western bank of the Jordan River, crossing 700 North at a signalized intersection, and continues north, winding behind the Day Riverside Library. The equestrian trail crosses 1000 North at a signalized crosswalk, where the trail then switches to the eastern bank of the Jordan River, paralleling the paved trail. There is a sign posted on the

western bank on a chain link gate stating that this portion of the Jordan River Trail is temporarily closed.

The trails then travel north, across the river from the Jordan River Golf Course. There is a short portion of unpaved trail that runs along the western bank of the river, from the north edge of the Jordan River Golf Course to the Rose Park Golf Course, but does not connect to 1000 North.

Cottonwood Park

- Amphitheatre
- Large Pavilion
- Small Pavilions
- Playground
- Basketball Court
- Sand Volleyball
- Baseball Diamond
- Trash Receptacles
- Parking (Street-side)



Figure 3.11.9 Cottonwood Park



Figure 3.11.1 Trail access point on North Temple near Utah State Fair Park



Figure 3.11.2 Trail and trash receptacle north of North Temple



Figure 3.11.3 Bridge B25
6' wide, 75' span, 9' clearance



Figure 3.11.4 View from Bridge B25 towards Utah State Fair Park



Figure 3.11.10 Bridge B27
4' wide, 70' span, 8' clearance

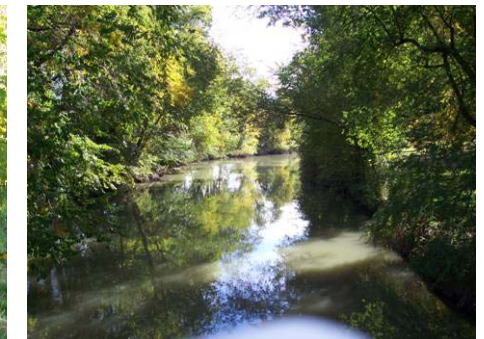


Figure 3.11.11 View from Bridge B27, looking south



Figure 3.11.5 Trail along west bank of Jordan River, west of Utah State Fair Park



Figure 3.11.6 "Utah State Fairpark/Jordan River Parkway" wall



Figure 3.11.12 View to south, looking toward Utah State Fair Park trail



Figure 3.11.13 View to north, east of Bridge B26



Figure 3.11.7 View towards unpaved trail west of Bridge B26



Figure 3.11.8 Bridge B26
8' wide, 77' span, 9' clearance

Constitution Park

- Northwest Salt Lake Community Center/ Senior Citizens Center
- Small Pavilions
- Tennis Courts
- Playgrounds
- Baseball Diamonds
- Picnic Tables
- Soccer Fields
- Trash Receptacles
- Parking (113 standard, 8 handicapped, 2 reserved, 1 bus only)



Figure 3.11.13 Constitution Park



Figure 3.11.20 View from unpaved trail west of Constitution Park



Figure 3.11.21 Unpaved trail near Cottonwood Park



Figure 3.11.28 Trail north of 500 North, looking south



Figure 3.11.29 Unpaved trail west of Backman School



Figure 3.11.14 Constitution Park, looking north



Figure 3.11.15 Trail access from Constitution Park



Figure 3.11.22 Street-side parking for Cottonwood Park



Figure 3.11.23 Trail through Cottonwood Park



Figure 3.11.30 Bridge B29 4' wide, 69' span, 6' clearance



Figure 3.11.31 View across Jordan River to Backman School, looking east



Figure 3.11.16 Paved trail north of Constitution Park



Figure 3.11.17 View of Cottonwood Park, north of Jordan River



Figure 3.11.24 Rest area west of Cottonwood Park



Figure 3.11.25 Bridge B28 8' wide, 70' span, 8' clearance



Figure 3.11.32 700 North overpass



Figure 3.11.33 Unpaved paved trail west of Riverside Park



Figure 3.11.18 View of river from Bridge B28



Figure 3.11.19 unpaved trail north of Bridge B26



Figure 3.11.26 Trail north of Cottonwood Park, looking north



Figure 3.11.27 Crosswalk at 500 North

Riverside Park

- Soccer Fields
- Baseball Diamonds
- Picnic Tables
- Horse Shoe Pits
- Tennis Courts
- Basketball Courts
- Restrooms
- Playgrounds
- Pavilions
- Trash Receptacles
- Parking (87 standard, 4 handicapped)



Figure 3.11.34 Riverside Park



Figure 3.11.35 View across Jordan River to Riverside Park



Figure 3.11.36 View from equestrian trail toward Day Riverside Library



Figure 3.11.37 Bridge B30
4' wide, 70' span, 6' clearance



Figure 3.11.38 "Trail Open on East Side of River" sign on east side of 1000 North

Mini Park

- Picnic Tables
- Open Play Areas



Figure 3.11.39 Mini Park



Figure 3.11.40 Unpaved trail north of 1000 North



Figure 3.11.41 Trail east of Jordan River Golf Course

Issues and Opportunities

- There is a gap in the paved trail between Interstate 80 and North Temple Street.
- There is no barrier provided where the trail intersects with the rail lines in this gap.
- No formal trailhead facilities exist on North Temple Street.
- Signs indicate that equestrian trail access at 1000 North along the west side of the river is temporarily closed off. The greens at the Jordan River Golf Course go up to the edge of the river bank, indicating that this closure may be permanent. A small portion of this trail can be accessed from Bridge B31 in the Rose Park Golf Course, but continues south only to northern boundary of the Jordan River Golf Course.

Recommendations: Paved Trail

3.11 A Paved trail Gap: Just south of I-80

The small gap just south of I-80 should be paved. The negotiations for rights-of-way through the land north of I-80 are nearing completion.

3.11 B Underpass: I-80

Salt Lake City is planning to extend the trail under I-80 with a pedestrian underpass.

3.11 C Paved Trail Gap: I-80 to North Temple

Salt Lake City is completing negotiations to secure rights-of-way through the properties north of I-80 to North Temple along the Jordan River. The trail will need to cross rail lines crossing the river and trail corridor. The rail lines are currently being consolidated into a single track. An at-grade trail crossing for the double track is in negotiations with the railroad company, which will include a pedestrian-operated barrier. The area is also designed to include flood control and major drainage structures that will include a connecting trail into the Gateway area of Salt Lake City via the City Creek Trail.

3.11 D Trailhead: North Temple

Salt Lake City is teaming up with UDOT to develop a trailhead on the south side of North Temple, on the west bank of the Jordan River.

3.11 E Trailhead: South Side of 1000 North, East of River

A trailhead should be developed at 1000 North. It should serve both urban water trail users and surface trail users. (See Section 4.4 for more information on urban water trail facilities).

3.11 F Signage

Signage indicating that the west side of the Jordan River Trail is temporarily closed between 1000 North and Bridge B31 in Rose Park Golf Course should be replaced with signage indicating this area is for maintenance access only. The section of trail from Bridge B31 south to the northern boundaries of the Jordan River Golf Course should be signed as maintenance access only.

Recommendations: Equestrian Trail

3.11 G Equestrian Trails

No new equestrian trails are proposed due to width limitations within the corridor.

Construction Costs

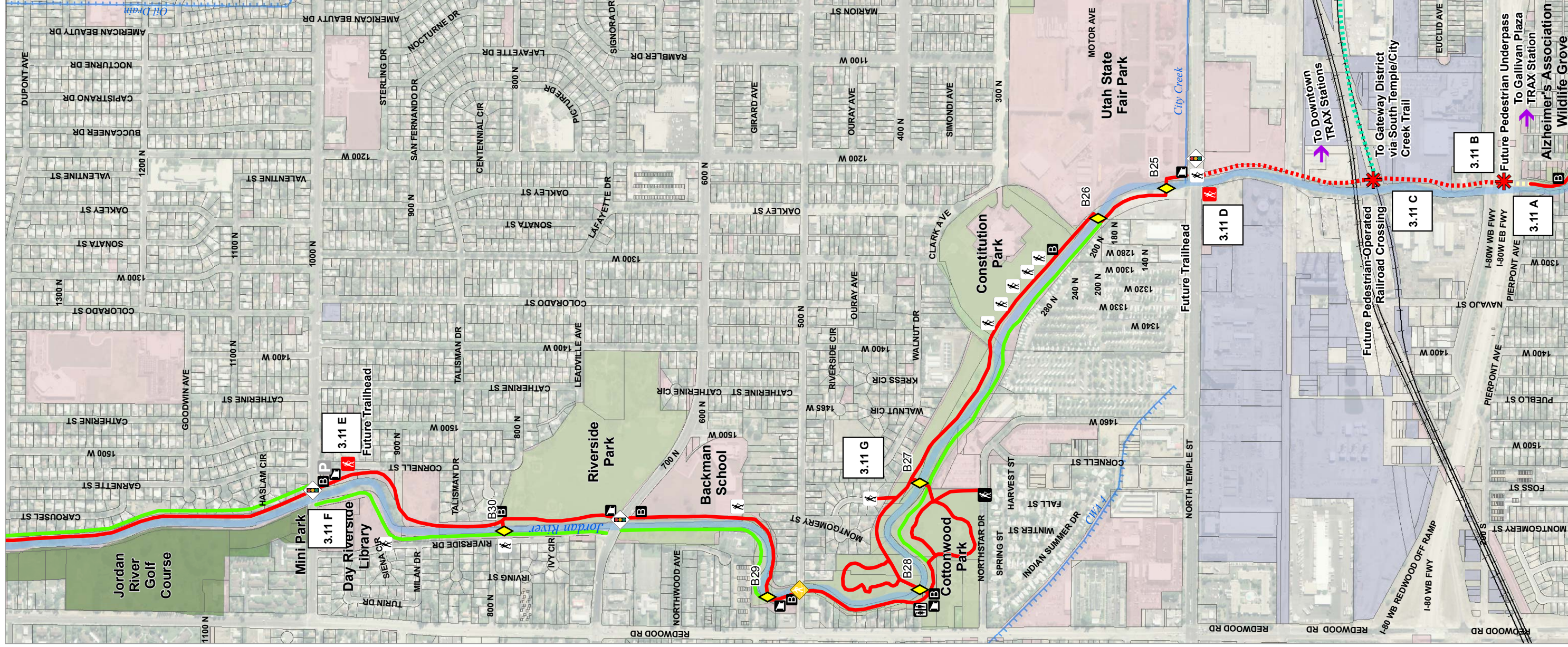
Table 3.11.1– Construction Costs: Paved Trail Segment 11

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.11 A	\$23,760
Underpass	3.11 B	\$300,000
Trail & Railroad Crossing	3.11 C	\$10,000,000
Trailhead	3.11 D	Funded
Trailhead	3.11 E	\$185,000
Signage	3.11 F	\$2,000
Policy	3.11 G	N/A

Jordan River Trail Master Plan

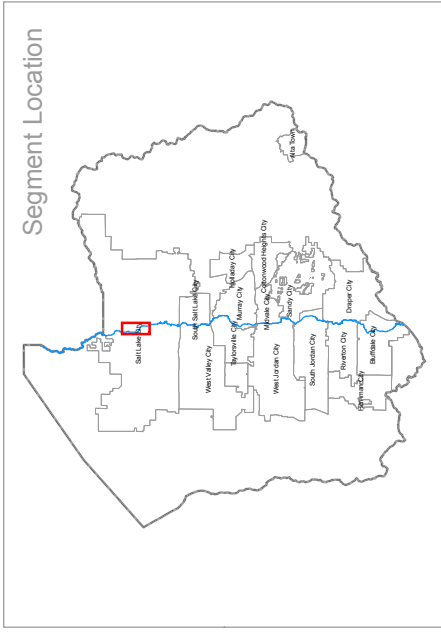
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Unpaved Jordan River Trail
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
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 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment II: Salt Lake City

Interstate 80 to DuPont Avenue



3.12 Segment 12: Salt Lake City/Salt Lake County (DuPont Avenue to Interstate 215)

The paved and equestrian trails continue north along the eastern bank of the Jordan River. A rest area is provided at a trail access point on Sunset Drive. The trails then enter the Rose Park Golf Course, where they cross Bridge B31 to the west bank of the river. A small portion of unpaved trail heads south from this point, but stops when it reaches the boundary of the Jordan River Golf Course. From Bridge B31, the an unpaved trail and the equestrian trail continue north, winding through the center of the Rose Park Golf Course, following the Jordan River. The river and trails curve to the west as they approach the north end of the golf course. There is a small rest area and a boat launch just east of Redwood Road. There is no crosswalk, bridge, or underpass provided to get trail users safely across Redwood Road to the remaining segments of the trail.

An informal unpaved parking area with room for maybe two vehicles is located on the west side of Redwood Road, but access is dangerous. The trails head to the northwest where Bridge B32 provides access across the river. A small segment of the trails follow the west bank of the river, dead-ending in thick vegetation. There is another trail that forks off of this just north of Bridge B32, and continues to the west. The main trails cross the bridge, and traces along the east bank of the Jordan River, west of industrial uses.

As the trails approach the county line, they dip down into a large marshy meadow, and curve back to the east, where the trails can be accessed from an informal parking area located next to Redwood Road at approximately 2700 North. The trails then follow the river back towards the west, becoming very rough and undefined, until they basically becomes unusable as they approaches Interstate 215.



Figure 3.12.4 Trail entering Rose Park Golf Course



Figure 3.12.5 Jordan River Trail information sign



Figure 3.12.12 Informal parking west of Redwood Road



Figure 3.12.13 Old car frame in meadow adjacent to trail



Figure 3.12.1 Rest spot and trail access on Sunset Drive



Figure 3.12.6 Trail heading north from Bridge B31



Figure 3.12.7 Trail through Rose Park Golf Course



Figure 3.12.14 Trail segment continuing along west bank of Jordan River



Figure 3.12.15 Open fields to the west of the trail



Figure 3.12.2 Neighborhood access to Jordan River Trail

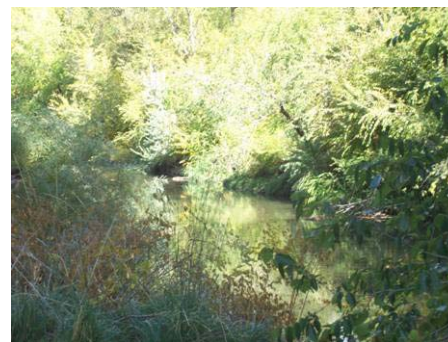


Figure 3.12.8 View of Jordan River from trail north of Rose Park Golf Course

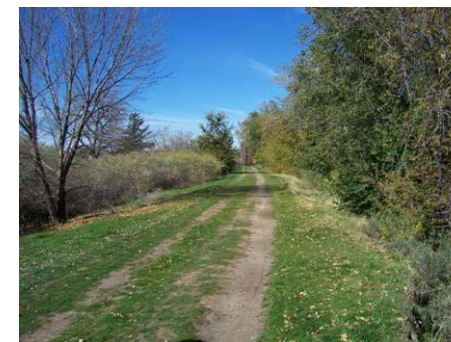


Figure 3.12.9 Trail north of Rose Park Golf Course, looking north



Figure 3.12.16 Bridge B32 8.5' wide, 88' span, 6' clearance



Figure 3.12.17 End of trail segment along western bank of Jordan River



Figure 3.12.3 Bridge B31 12' wide, 54' span, 6' clearance



Figure 3.12.10 Rest area east of Redwood Road



Figure 3.12.11 Redwood Road crossing, looking southeast



Figure 3.12.18 View to Redwood Road overpass from Bridge B32



Figure 3.12.19 View to Jordan River from Bridge B32, looking north



Figure 3.12.20 Trail west of Bridge B32, looking northwest



Figure 3.12.21 "Provo/Jordan River Parkway" sign



Figure 3.12.28 Structure spanning river



Figure 3.12.31 View across river to Utah State OHV Park



Figure 3.12.38 Garbage along trail



Figure 3.12.29 View of Jordan River from trail south of Redwood Road /2700 North access point



Figure 3.12.22 Trail access north of Bridge B32



Figure 3.12.23 Trail north of Bridge B32



Figure 3.12.32 Trail north of Redwood access point



Figure 3.12.33 View to river from trail



Figure 3.12.30 Redwood Road/2700 North trail access point



Figure 3.12.39 "Storm Drain" sign along trail



Figure 3.12.24 View of river from trail



Figure 3.12.25 Garbage along trail



Figure 3.12.34 Garbage along trail



Figure 3.12.35 Concrete remnants spanning river



Figure 3.12.40 Oil dumped along trail near river



Figure 3.12.41 Trail as it narrows approaching I-215



Figure 3.12.26 Approach to marsh



Figure 3.12.27 Trail through marsh



Figure 3.12.36 View to river from trail



Figure 3.12.37 Trail north of Redwood access point



Figure 3.12.42 Trail becomes blocked by vegetation



Figure 3.12.43 Interstate 215 overpass

Issues and Opportunities

- Paved trail only exists to Bridge B31 in the Rose Park Golf Course.
- There is no pedestrian crossing provided at Redwood Road.
- The existing informal parking area is dangerous for drivers trying to re-enter traffic.
- Portions of the trails around 2300 north are muddy due to high water table.
- There is a lot of garbage, including buckets of used motor oil, deposited along the trail corridor between Redwood Road and the current end of the trail.
- The trails dead-end about a quarter of a mile from Interstate 215.

Recommendations: Paved Trail

3.12 A Paved trail Gap: Bridge B31 to I-215

The trail on the west side of the river from Bridge B31 in the Rose Park Golf Course to approximately Riverview Avenue and Midland Street will be paved Summer 2008.

The short section of trail on the west side of the river in the future sports complex will be abandoned. Salt Lake City has no plans to extend the trail on the west side of the river, or to abandon the other fork of the City trail that heads west from this area. A paved trail should be developed on the east side of the river from the Redwood Road crossing to the Davis County line. The paved trail will be adjacent to the existing equestrian trail.

Davis County officials believe there is an easement along the east side of the river for trail development which currently has an unpaved trail. This trail would ultimately tie into the trail development related to the Legacy Parkway development. Portions of this section may require boardwalks to elevate the trail through high water table areas.

3.12 B Redwood Road Street Crossing

Funds have been acquired for a new pedestrian crossing at Redwood Road at approximately 1800 North, but Salt Lake City plans to keep the informal parking area west of this crossing for emergency access purposes, and may develop a trailhead at this location sometime in the future.

3.12 C Maintenance

All of the garbage and used motor oil needs to be cleaned up in this section, and the recommendations in Chapter 5 should be implemented.

Recommendations: Equestrian Trail

3.12 D Equestrian Trails

Access to the Utah State Fairpark needs to be maintained for equestrian users. No new equestrian trails are proposed due to width limitations within the corridor.

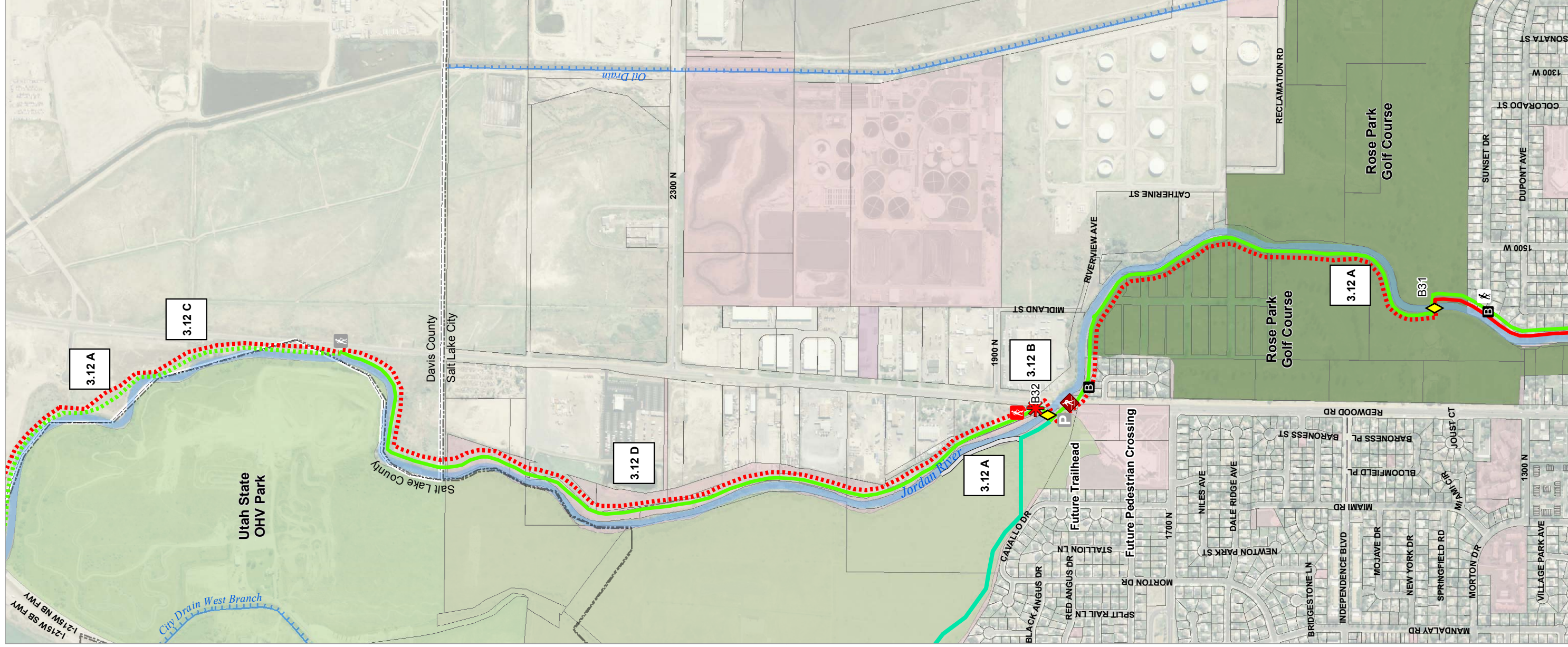
Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.12 A	Funded
Street Crossing	3.12 B	Funded
Maintenance	3.12 C	\$10,000
Bridge	3.12 D	\$140,000-\$220,000

Jordan River Trail Master Plan

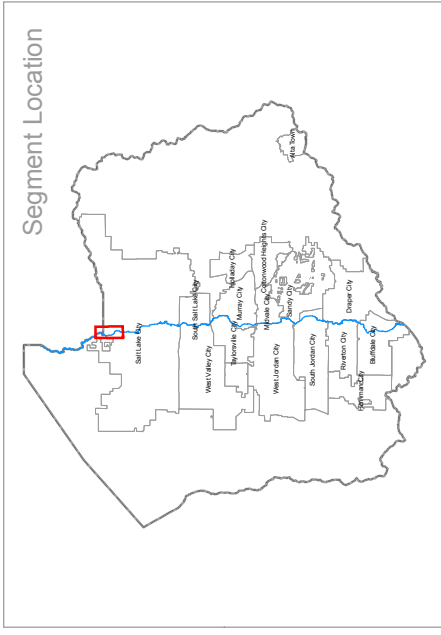
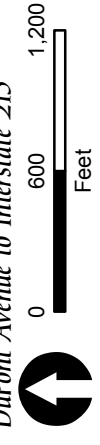
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
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 - Direction to Nearest Proposed FrontRunner Station
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 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 12: Salt Lake City/ Salt Lake County

DuPont Avenue to Interstate 215



June 2008

3.0 Paved and Equestrian Trail

Several issues were identified at the outset of this project in meetings with Salt Lake County staff members, and more were recognized during field surveys, from meetings with the stakeholder group, and through the analysis of existing conditions. Many pertain to the corridor as a whole, while others pertain only to specific sections of the trail corridor.

Issues relating to the paved and equestrian trail corridor as a whole are discussed below and include references for more detailed information to be found in this planning document. Following the general corridor issues, detailed existing conditions, issues and opportunities, and recommendations are explored for each of the twelve corridor segments in sections 3.1 through 3.12 of this chapter.

Overall Issues, Opportunities and Recommendations

Trail Gaps

Discontinuous trail segments are the primary factor preventing trail users from enjoying the full spectrum of recreation activities offered along the Jordan River Trail. Trail users that are not familiar with the location of these gaps set out to explore the river corridor, only to find that their trip is cut short by the end of the pavement. There are currently eleven gaps in the Jordan River Trail corridor, as shown on the “Existing Paved and Equestrian Trail Gaps Map” following this section. Some of these gaps are for paved trails, some for equestrian trails, and others represent gaps in both types of trails in a particular segment.

One of the most important keys to improving the functional level of the trail corridor as a whole is to fill in these missing trail gaps, in accordance with the trail standards established in Chapter 5. As this planning process has been underway, several municipalities have been working to close some of the gaps in their areas by the end of 2007. Detailed information about each gap and the current status is provided in sections 3.1 through 3.12 in this chapter.

Signage

The existing signage throughout the Jordan River Trail Corridor currently represents a hodgepodge of designs, information content, and purposes, and presents trail users with incomplete, and sometimes confusing, information about the trail facilities. Mileage markers are sporadic and inconsistent from city to city, and the trail corridor lacks consistent location reference signs for information and emergency purposes.

The Jordan River Parkway Foundation in conjunction with Salt Lake County, recently developed a standard trailhead sign, which has been installed at several trailheads along the river. This plan recommends the implementation of a comprehensive signage program to improve the function level and safety of the trail. Detailed recommendations can be found in section 5.4 of this plan.

Trail Maps

The lack of a comprehensive, easy-to-read, readily available map prevents many trail users from traveling beyond their familiar areas and exploring other portions of the Jordan River Trail corridor. As discussed in Chapter 5, a comprehensive trail map should be incorporated as part of the Salt Lake County standard trailhead sign design. In addition, the County should make mapping available online, and in a printed format that paved trail users can utilize to plan trips, and to take with them on their visits to the Jordan River. These maps could take the form of a guide booklet or large foldout map, and could include information on the history of the river, flora and fauna found along the river, and cultural information. These printed guides should be made available online and at County facilities throughout the Salt Lake Valley. An excellent example is the “Willamette River Water Trail Guide,” which can be viewed and downloaded at www.willamettewatertrail.org.

Maintenance

Another major factor that can affect the functional level of the Jordan River Trail corridor is maintenance. Proper trail maintenance must be provided or the functionality of facilities is negatively impacted. An example of a common problem is the difficulty cyclists have with puncture vines. If they are allowed to thrive, the small goat head shaped seeds cause flat tires, sometimes even in tires protected with tire liners.

When trails are installed according to the trail design standards described in Chapter 5.0, proper trail maintenance becomes easier and more cost-effective.

In addition, simple maintenance measures such as keeping trash cans emptied and dog waste station bags refilled encourages trail users to take advantage of these features, which in turn, helps keep the trail corridor clean and reduces cleanup efforts required by maintenance crews.

Cycling Design Requirements

Thoughtful design and location of curb cuts for cyclists is needed, especially when the trail dead-ends into a parking area (as in Germania Park and Walden Park, Segment 7). In some locations, the trail enters a park and merges with a sidewalk. Sometimes these areas present cyclists with difficult navigation through bollards with immediate sharp 90 degree turns, and no curb cuts straight ahead if cyclists can't make the turn. The official trail through parks can sometimes be unclear, leaving cyclists to roam sidewalks, picnic areas, and parking lots to find where the trail continues on the other side of the park.

Curb cuts need to be included where the trail intersects parks, trailheads, and streets. In addition, directional signage should be included as part of the signage program in section 5.4 to assist cyclists with safe navigation along the trail.

Equestrian Trail Design

Equestrians are less common, but equally important, users of the Jordan River Trail corridor, and special considerations must be applied when designing, implementing, and maintaining trails for equestrian use.

Due to equipment and transportation needs, equestrian trails should be loops wherever possible so that riders can travel back to the original location of their horse trailers. In addition, these trails should be separated from the paved trail by as much distance as possible to minimize user conflicts presented by the higher-traffic paved trail.

Chapter 5 provides specific standards for equestrian trail design and maintenance within the river corridor.

Flooding & Erosion Issues

When Utah Lake reaches a set high water level known as its “compromise level,” more water is released into the Jordan River. This is usually the peak flow of the river, and presents flooding issues at many underpasses throughout the trail corridor, and increases bank erosion at several locations in the river corridor.

It is the existing policy of Salt Lake County that portions of the trail that are covered due to flooding are closed, and official trail detours are not provided. This plan recommends the continuation of that policy and recommends that all areas subject to flooding are signed in accordance with the Signage Program discussed in Chapter 5. Specific underpasses that are subject to flooding are addressed in sections 3.1 through 3.12 of this chapter.












In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

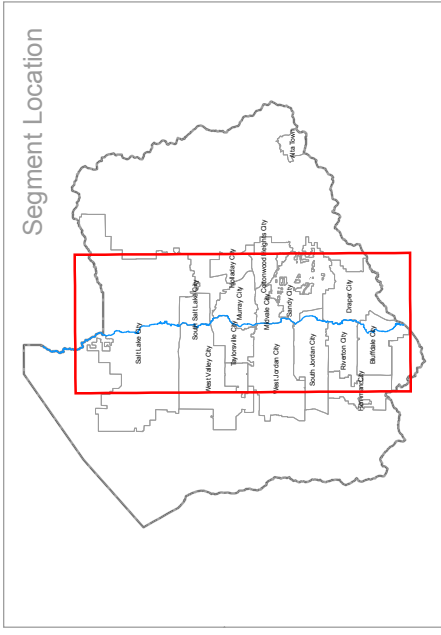
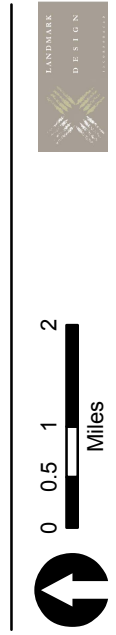
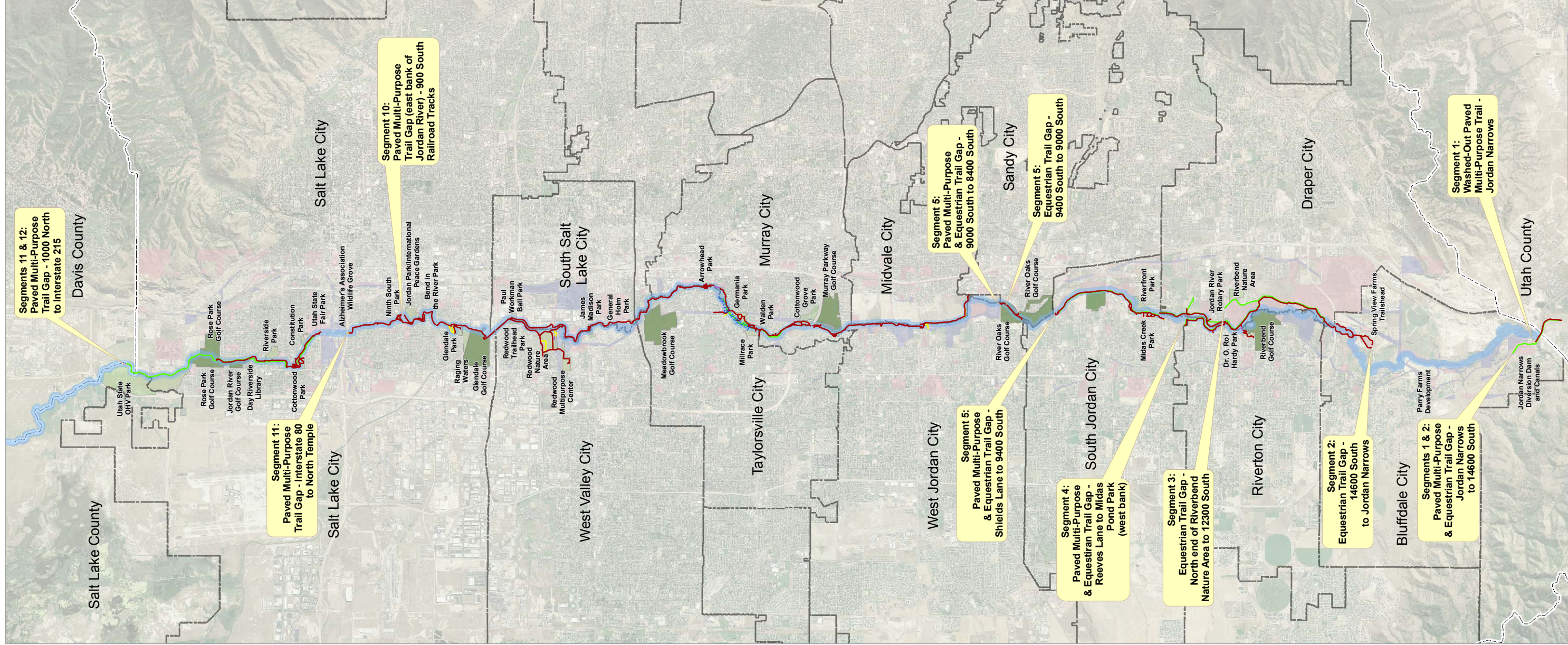
The issue of stream bank erosion presents serious challenges to Salt Lake County, and adds significant expense due to the need for stream bank stabilization and trail relocation. This plan recommends that all new trail connections be established outside of the official meander corridor (the zone within which the river channel may be expected to migrate within the next 100 years) wherever possible, and that, where there is an opportunity to move a trail further away from the river, the County should pursue this effort.

Trail Access

Convenient access to the Jordan River Trail from surrounding residential areas and businesses is key to increasing trail use. Increased trail use raises the fitness level of County residents, improves the safety of trail facilities by adding more “eyes” to the corridor, and encourages more concern by the public for the condition of the Jordan River corridor. When jurisdictions are reviewing development plans for areas near the river, access to the Jordan River Trail should be a key requirement for development approval.

Jordan River Trail Master Plan Trail Gaps

-  Existing Paved Jordan River Trail
-  Existing Unpaved Jordan River Trail
-  Existing Jordan River Equestrian Trail
-  Existing Jordan River Boardwalk
-  Jordan River
-  County Boundary
-  City Boundary
-  Golf Course
-  Park
-  Publicly Owned Land
-  Utility Owned Land



3.1 Segment 1: Bluffdale (Southern County Line to 15400 South)

Existing Conditions

The Salt Lake County portion of the Jordan River Trail begins at the southern county line in Bluffdale, Utah, east of the Utah Veteran's Memorial Park. The asphalt trail continues north and west from Utah County, overlooking the Jordan Narrows Diversion Dam and Canal area, and is paralleled along the south side by a narrow equestrian trail. An interpretive sign along the path explains the history of and need for the Jordan Narrows Diversion Dam and Canals.



Figure 3.1.1 Salt Lake/Utah County line, looking southeast towards Utah County

The trail continues for a short distance, where the asphalt paving ends at the top of a steep hill. An unpaved path continues at this point, traveling down the slope and curving northward. Beef Hollow to the west provides a view of structures in the Utah Veteran's Memorial Park from this portion of the trail. The asphalt trail picks up again for a short distance, ending at an informal, unpaved trailhead located on the south side of Jordan

Narrows Road. There is a small, unpaved area on the north side of the road with room for approximately 3 vehicles. This is the end of the official trail in this segment;

however, an unofficial equestrian trail continues from the unpaved parking area north of the trailhead, winding up a short, steep hill, crossing Sunrise Riding Stable Street, and terminating at the Sunrise Riding Stables.



Figure 3.1.2 Interpretive signage: Jordan Narrows Diversion Dam and Canals

The northern portion of this segment does not have developed trails or access to the river. New development on the west side of the river comes relatively close to the river and the canal that parallels it, but there is no access to the river.

The Porter Rockwell portion of the Mountain View Corridor Highway may pass through this segment at some point in the future, but planning is just in the beginning stages. If the Corridor passes through this area in the future, it would require a new bridge crossing over the Jordan River.



Figure 3.1.3 View to northwest, overlooking Jordan Narrows Diversion Dam and Canals



Figure 3.1.4 Equestrian trail paralleling asphalt trail above Jordan Narrows



Figure 3.1.5 Top of steep slope where asphalt trail begins again



Figure 3.1.6 View from unpaved trail through ravine towards Utah State Veteran's Memorial Park



Figure 3.1.7 Asphalt trail, looking north

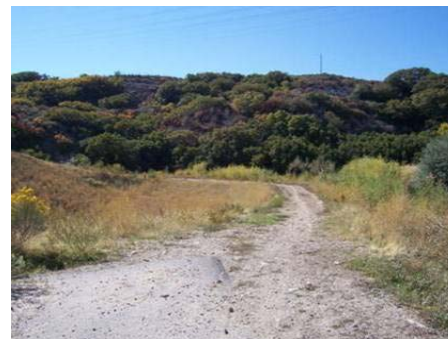


Figure 3.1.8 Asphalt trail ends - narrow unpaved trail continues up steep slope



Figure 3.1.9 Jordan Narrows



Figure 3.1.10 Parking north of Trailhead



Figure 3.1.11 Horse Crossing Sign near Sunrise Riding Stable Street



Figure 3.1.12 Jordan River paralleled by canal in northern portion of Segment 1

Issues and Opportunities

- There is a short gap in the paved trail from a mudslide. This segment is very steep.
- No formal parking or trailhead exists at Jordan Narrows.
- The paved trail has a gap between Jordan Narrows Road and 14600 South at the Spring View Farms Trailhead in Segment 2.
- The equestrian trail ends near the Sunrise Riding Stables, with the next access point just south of Bangerter Highway in Segment 2.
- It is not desirable to have trail users accessing the Jordan Narrows Diversion Dam and Canals area.

Recommendations: Paved Trail

3.1 A Mudslide Damaged Trail Segment

The short, steep section of trail that was damaged by a mudslide is scheduled to be repaired by spring of 2008 by Salt Lake County and Bluffdale City.

3.1 B Jordan Narrows Trailhead

Salt Lake County currently has a contract in place to plan and design a trailhead park at Jordan Narrows, with construction beginning Spring 2008. This facility will feature restrooms, and paved and unpaved parking to accommodate both paved and equestrian trail users. A future phase of this trailhead should add more accommodations for equestrian users.

3.1 C Trail Gap: Jordan Narrows Road to South End of Parry Farms Development

Develop a paved trail along the Utah Lake Distributing Canal from the future Jordan Narrows trailhead for approximately 1.5 miles to Wood Hollow natural drainage. Follow the drainage east to the southern edge of the subdivision, just east of where Packsaddle Drive ends.

3.1 D Trail Gap: South end of Parry Farms Development to Future Bridge Crossing

From the south end of the Parry Farms Development, the trail should continue north for 1/2 mile through Salt Lake County land and privately

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owned land to cross the South Jordan Canal at the location of an existing bridge abutment developed as part of the Parry Farms residential development. This section will require an easement from Rocky Mountain Power Company (Listed as Utah Power and Light in the Parcel Data). At this location a new bridge for pedestrians and equestrians will be required to cross the river and connect to the East Jordan Canal right-of-way on the east side of the river. This section of trail is currently under design by Salt Lake County, in association with Recommendation 3.2 A in the next section.

The Parry Farms Development has a proposed trail which loops around the property and ties into the Jordan River Trail corridor at the bridges.

These paved trail alignments will connect to future trails east of the Jordan River. These trails will be required as part of the development approvals in Bluffdale or will be developed by Bluffdale City with grant monies.

3.1 E Trail Gap Alternative: Jordan Narrows Road to North End of Parry Farms Development

An alternative paved trail route could take the form of an easement directly adjacent to the canal, but not on the canal right-of-way. This trail would parallel the Utah Lake Distributing Canal from the future Jordan Narrows trailhead for approximately 1.5 miles to Wood Hollow natural drainage. The trail would then follow the drainage east to the southern edge of the subdivision, just east of where Packsaddle Drive ends, and continue to the north end of the subdivision, where Iron Horse Boulevard ends.

Recommendations: Equestrian Trail

3.1 F Equestrian Trail Gap: Sunrise Riding Stables to Parry Farms
Salt Lake County intends to develop an equestrian trail from the future Jordan Narrows Trailhead to Parry Farms in a future phase. The separated equestrian trail should begin at the future Jordan Narrows Trailhead, and could meander down the hillside through the Utah and Salt Lake Canal Company property, to the Utah and Salt Lake Canal road. At this point a fence would be needed to keep equestrian users and others from going south on the canal road toward the Jordan Narrows Diversion Dam.

The trail would then follow the Utah and Salt Lake Canal north to where it crosses the Wood Hollow drainage, and continue north to eventually intersect at the South Jordan Canal and Jordan River crossings. The equestrian trail would connect to the future equestrian trail to be developed east of the Jordan River. Implementing this trail segment will require negotiation with Rocky Mountain Power and the Canal Companies.

3.1 G Equestrian Trail Gap Alternative (and small portion of Paved Trail): Jordan Narrows to North End of Parry Farms Development

An alternative equestrian trail route could take the form of an easement directly adjacent to the canal, but not on the canal right-of-

way. The trail would parallel the Utah and Salt Lake Canal east of the Sunrise Riding Stables near the river, remaining on the west side of the canal. It would continue north, crossing the Wood Hollow drainage, past the subdivision. As it nears the northern end of the subdivision, there is also a possibility to align the paved and equestrian trails closely along this alternative route to avoid the canal right-of-way, to the north end of the subdivision.

3.1 H Gate Across Canal Road

Implementing recommendation 3.1 C will require a gate across the canal road to keep equestrian trail users from traveling south on the canal road.

3.1 I Equestrian Bridge Across Utah and Salt Lake Canal

Implementing recommendation 3.1 C will require an equestrian bridge across the Utah and Salt Lake Canal.

3.1 J Bridge Across Utah Lake Distribution Canal

Implementing 3.1D and 3.1 F will require a pedestrian/equestrian bridge across the Utah Lake Distribution canal at the north end of the Parry Farms development.

3.1 K Bridge Across Jordan River

Implementing 3.1D and 3.1 F will require a pedestrian/equestrian bridge across the Utah Lake Distribution canal at the north end of the Parry Farms development.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.1 A	Funded
Trailhead	3.1 B	Funded
Paved Trail	3.1 C	\$178,200
Paved Trail	3.1 D	\$415,800
Paved Trail	3.1 E	\$308,880
Equestrian Trail	3.1 F	\$121,440
Equestrian/Paved Trail	3.1 G	\$113,520
Gate	3.1 H	\$2,000
Bridge	3.1 I	\$150,000
Bridge	3.1 J	\$150,000
Bridge	3.1 K	\$250,000

Ownership

Ownership for recommended trail alignments is shown in the maps below, followed by detailed ownership information in tables.

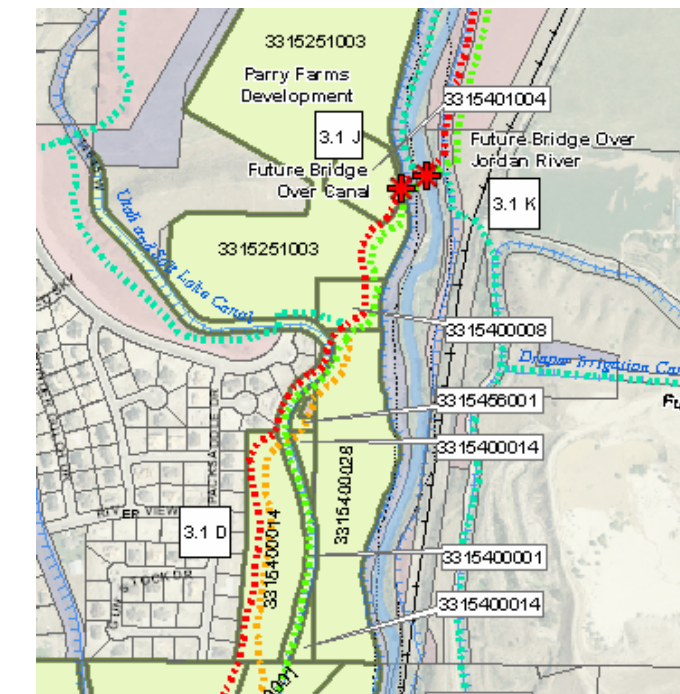


Figure 3.1.13 Ownership: Parry Farms to Future Bridge Crossing

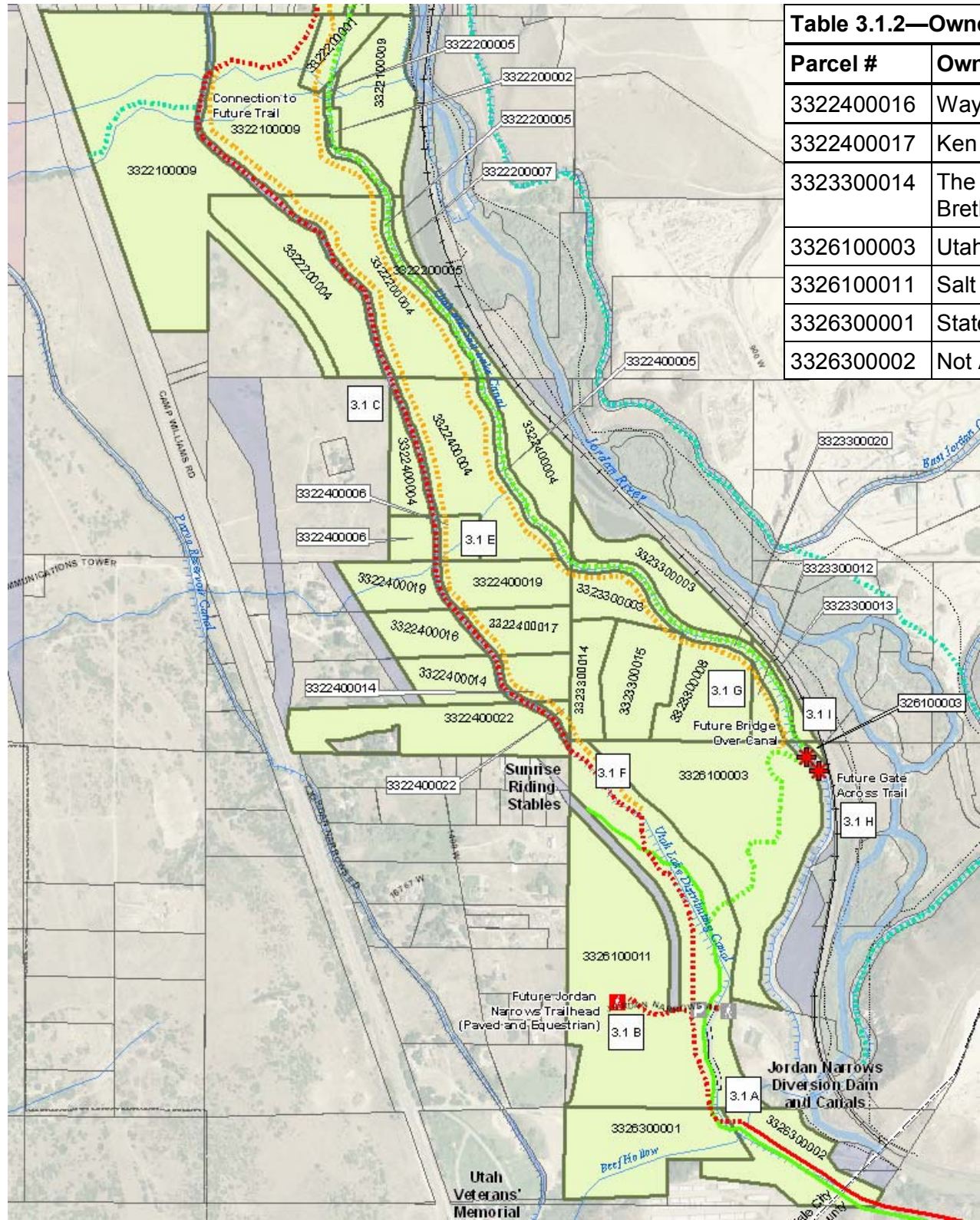


Figure 3.1.14 Ownership: Jordan Narrows to Parry Farms

Table 3.1.2—Ownership: Recommendation 3.1 C—Paved

Parcel #	Owner	Acres
3322400016	Wayne Workman	5.0
3322400017	Ken Rozema	5.3
3323300014	The Communities of the Apostolic Brethren	5.0
3326100003	Utah and Salt Lake Canal Company	4.5
3326100011	Salt Lake County	57.3
3326300001	State Armory Board	15.4
3326300002	Not Available	11.4

Table 3.1.2—Ownership: Recommendation 3.1 C—cont'd

Parcel #	Owner	Acres
3322100009 - east	LLC RAAATS	23.7
3322100009 - west	LLC RAAATS	47.3
3322200004 - east	Utah and Salt Lake Canal Company	15.1
3322200004 - west	Utah and Salt Lake Canal Company	14.3
3322400004 - east	Utah and Salt Lake Canal Company	16.3
3322400004 - west	Utah and Salt Lake Canal Company	3.7
3322400006 - east	Kay Schroeder	7.0
3322400006 - west	Kay Schroeder	2.4
3322400014 - east	J. Harvey	3.6
3322400014 - west	J. Harvey	5.0
3322400019 - east	Wayne Mortimer	7.0
3322400019 - west	Wayne Mortimer	5.2
3322400022 - east	Donna Gibson	0.6
3322400022 - west	Donna Gibson	14.2

Table 3.1.3—Ownership: Recommendation 3.1 D—Paved

Parcel #	Owner	Acres
3315251003	Parry Farms LLC	50.9
3315400001	Utah and Salt Lake Canal Company	5.0
3315400008	Utah Power and Light	1.8
3315400028	Telluride Power Company	12.0
3315401004	R & C Parry Family Properties	2.6
3315456001	Bluffdale City	0.4
3315400014 - northeast	Salt Lake County	0.6
3315400014 - southeast	Salt Lake County	0.5
3315400014 - west	Salt Lake County	6.7

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Table 3.1.4—Ownership: Recommendation 3.1 E—Alternative

Parcel #	Owner	Acres
3322400016	Wayne Workman	5.0
3322400017	Ken Rozema	5.3
3323300014	The Communities of the Apostolic Brethren	5.0
3326100003	Utah and Salt Lake Canal Company	4.5
3326100011	Salt Lake County	57.3
3322100009 - east	LLC RAAATS	23.7
3322100009 - west	LLC RAAATS	47.3
3322200004 - east	Utah and Salt Lake Canal Company	15.1
3322200004 - west	Utah and Salt Lake Canal Company	14.3
3322400004 - east	Utah and Salt Lake Canal Company	16.3
3322400004 - west	Utah and Salt Lake Canal Company	3.7
3322400006 - east	Kay Schroeder	7.0
3322400006 - west	Kay Schroeder	2.4
3322400014 - east	J. Harvey	3.6
3322400014 - west	J. Harvey	5.0
3322400019 - east	Wayne Mortimer	7.0
3322400019 - west	Wayne Mortimer	5.2
3322400022 - east	Donna Gibson	0.6
3322400022 - west	Donna Gibson	14.2

Table 3.1.5—Ownership: Recommendation 3.1 F—Equestrian

Parcel #	Owner	Acres
3322200001	Utah and Salt Lake Canal Company	4.1
3322200002	Utah and Salt Lake Canal Company	0.6
3322200004	L. Yates	15.1
3322200007	Geneva Rock Products	2.1
3322400005	Utah and Salt Lake Canal Company	3.1
3322400019	Wayne Mortimer	7.1
3323300008	Corporation of the Presiding Elder of the Apostolic United Brethren	9.8
3323300012	Corporation of the Presiding Elder of the Apostolic United Brethren	1.9
3323300013	B & B Resources, Inc.	1.3
3323300015	Corporation of the Presiding Elder of the Apostolic United Brethren	8.9
3323300020	Utah and Salt Lake Canal Company	4.6
3326100011	Salt Lake County	57.3
3322100009 - east	LLC RAAATS	11.5
3322100009 - west	LLC RAAATS	23.7
3322200005 - north	Utah and Salt Lake Canal Company	0.4
3322200005 - south	Utah and Salt Lake Canal Company	3.6
3322400004 - east	Utah and Salt Lake Canal Company	7.6
3322400004 - west	Utah and Salt Lake Canal Company	16.3
3323300003 - east	Utah and Salt Lake Canal Company	7.8
3323300003 - west	Utah and Salt Lake Canal Company	4.6
3326100003 - east	Utah and Salt Lake Canal Company	0.2
3326100003 - west	Utah and Salt Lake Canal Company	31.4

Table 3.1.5—Ownership: Recommendation 3.1 G—Alternative

Parcel #	Owner	Acres
3315400001	Utah and Salt Lake Canal Company	5
3315400028	Telluride Power Company	12
3315456001	Bluffdale City	0.4
3322200001	Utah and Salt Lake Canal Company	4.1
3322200002	Utah and Salt Lake Canal Company	0.6
3322200004	L. Yates	15.1
3322200007	Geneva Rock Products	2.1
3322400005	Utah and Salt Lake Canal Company	3.1
3322400019	Wayne Mortimer	7.1
3323300008	Corporation of the Presiding Elder of the Apostolic United Brethren	9.8
3323300012	Corporation of the Presiding Elder of the Apostolic United Brethren	1.9
3323300013	B & B Resources, Inc.	1.3
3323300015	Corporation of the Presiding Elder of the Apostolic United Brethren	8.9
3323300020	Utah and Salt Lake Canal Company	4.6
3326100011	Salt Lake County	57.3
3315400014 - northeast	Salt Lake County	0.6
3315400014 - southeast	Salt Lake County	0.5
3315400014 - west	Salt Lake County	6.7
3322100009 - east	LLC RAAATS	11.5
3322100009 - west	LLC RAAATS	23.7
3322200005 - north	Utah and Salt Lake Canal Company	0.4
3322200005 - south	Utah and Salt Lake Canal Company	3.6
3322400004 - east	Utah and Salt Lake Canal Company	7.6
3322400004 - west	Utah and Salt Lake Canal Company	16.3
3323300003 - east	Utah and Salt Lake Canal Company	7.8
3323300003 - west	Utah and Salt Lake Canal Company	4.6
3326100003 - east	Utah and Salt Lake Canal Company	0.2
3326100003 - west	Utah and Salt Lake Canal Company	31.4

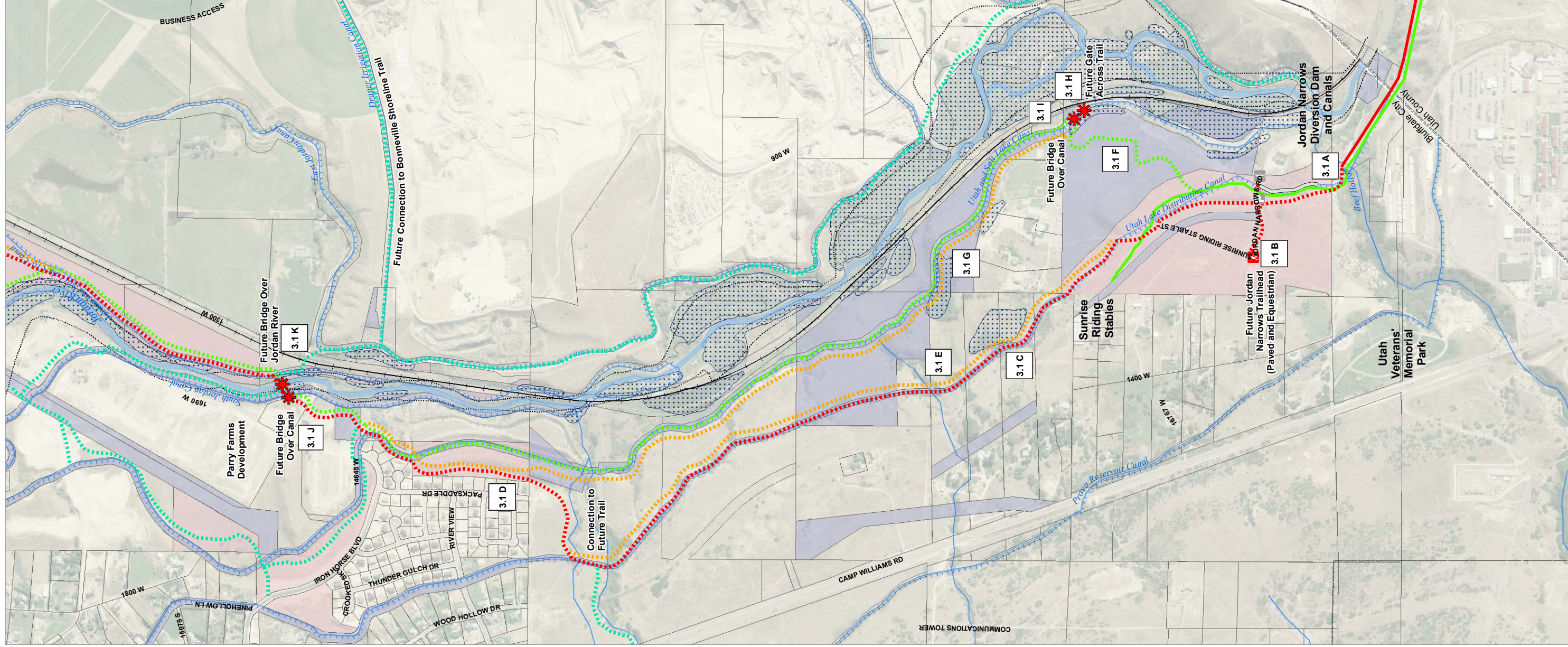
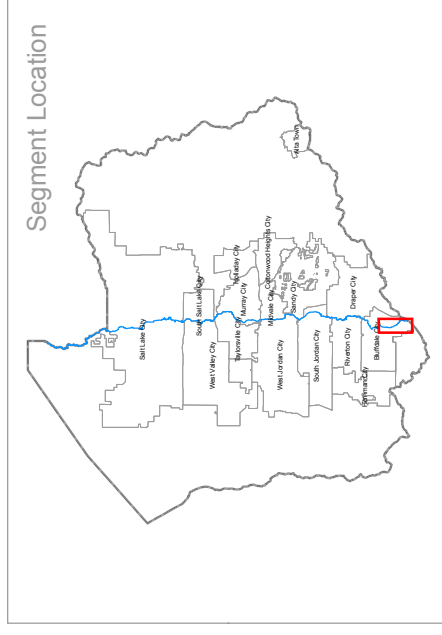
Jordan River Trail Master Plan

Paved Trail Facilities

- Future Element**
- Future Element
- Alternative Future Element
- Crossings**
- Crosswalk
- No Safe Crossing
- Signalized Crossing
- Underpass
- Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
- Equestrian Trailhead
- Formal Trailhead
- Informal Trailhead
- Trail Access Point
- Parking - Formal Paved
- Parking - Informal Unpaved
- Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
- Recommended Jordan River Trail
- Existing Unpaved Jordan River Trail
- Existing Unpaved Jordan River Trail - To Be Paved
- Existing Jordan River Equestrian Trail
- Recommended Jordan River Equestrian Trail
- Existing Jordan River Boardwalk
- Alternative Jordan River Trail Alignment
- City Existing Trail
- City Proposed Trail
- Trails to be Abandoned
- Public Transit**
- TRAX Station
- Direction to Nearest TRAX Station
- Proposed FrontRunner Station
- Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
- Bridge Over Jordan River
- Footbridge Over Stream or Canal
- Bench
- Picnic Table
- Trash Receptacle
- City Boundary
- Jordan River
- Streams
- Canals
- Light Rail
- Railroad
- County Boundary
- Meander Corridor (no data north of Surplus Canal)
- Wetlands (WAIDS 2007)
- Golf Course
- Park
- Publicly Owned Land
- Utility Owned Land

Segment 1: Bluffdale

Southern County Line to Business Access Road



3.2 Segment 2: Bluffdale/Riverton (15400 South to Bangerter Highway)

Existing Conditions

The southern portion of this trail segment lacks paved trail facilities. Access is provided at the Spring View Farms Trailhead, located on 14600 South in Bluffdale. Two trails leave the trailhead parking area. The Jordan River Trail, accessed from the northwest corner of the parking lot, crosses a small footbridge and bends to the northeast.

Spring View Farms Trailhead

- Trailhead Sign & Map
- Restrooms
- Trash Receptacles
- Parking (24 standard, 2 handicapped)



Figure 3.2.1 Spring View Farms Trailhead

There is a paved private access trail to Loomis Village connected to this segment of the trail. The Spring View Farms Trail departs from the eastern edge of the parking lot and turns north, following Spring View Trail. It crosses a small footbridge, and meets up with the Jordan River Trail.

The trail spans another small footbridge and then crosses Peacock Midge Drive before it turns north toward the Jordan River. The trail splits again near a small pond. The Madison River Trail follows the eastern shore of the

Jordan River, ending in less than a quarter of a mile. The Jordan River Trail crosses the Jordan River via a large pedestrian bridge (Bridge B1), and winds its way north, crossing a small footbridge. The trail connects to the other side of Bangerter Highway with a pedestrian underpass. Users report that in times of high water, the underpass is not useable.

The paved Jordan River Trail continues north from the Bangerter Highway underpass and crosses to the east side of the river over a large pedestrian bridge (Bridge B2). The Arrow Trailhead is located west of this bridge, but is not yet connected to the trail corridor. This trailhead is located on 1300 West at approximately 13800 South, and has a gravel parking lot intended for equestrian use and a short segment of trail that does not connect to the Jordan River Trail corridor at this time.



Figure 3.2.2 Bridge B1
10' wide, 100' span, 6' clearance



Figure 3.2.3 Spring View Farms Trail access from trailhead parking lot



Figure 3.2.10 Loomis Village Private Access to trail



Figure 3.2.11 Footbridge along Spring View Farms Trail



Figure 3.2.4 Spring View Farms Trail signage and fence detail



Figure 3.2.5 Jordan River Trail access from trailhead parking lot



Figure 3.2.12 Footbridge along Jordan River Trail



Figure 3.2.13 Small stream crossed by footbridge in Figure 2.11



Figure 3.2.6 Footbridge along Jordan River Trail



Figure 3.2.7 Bench along Jordan River Trail



Figure 3.2.14 Trail looking north along Spring View Trail



Figure 3.2.15 Trail Crossing at Peacock Midge Drive



Figure 3.2.8 Wetlands



Figure 3.2.9 Jordan River Trail



Figure 3.2.16 Wetlands north of Peacock Midge Drive



Figure 3.2.17 Madison River Trail



Figure 3.2.18 View to southeast from Bridge B1



Figure 3.2.19 Footbridge south of Bangerter Highway



Figure 3.2.20 View looking south at footbridge (Figure 2.19)



Figure 3.2.21 Bench overlooking river



Figure 3.2.22 Bangerter Highway Underpass

Issues and Opportunities

- The paved trail has a gap between Jordan Narrows Road in Segment 1 and 14600 South at the Spring View Farms Trailhead. An alignment needs to be identified.
- The equestrian trail ends near the Sunrise Riding Stables in Segment 1, with the next access point just south of Bangerter Highway.
- There is flooding at the Bangerter Highway underpass during peak river flow. The Arrow equestrian trailhead needs to be completed.
- No connection is currently provided to the Arrowhead equestrian trailhead on 1300 West. This trailhead was given

state funding with the belief that equestrian users would be able to head north *and* south on the Jordan River trail.

- Equestrian users would like to see the equestrian trail continue south through the Salt Lake County owned parcel west of the Jordan River.
- The equestrian trail south of Bangerter Highway needs to be identified with signage.

Recommendations: Paved Trail

3.2 A Trail Gap: Parry Farms Development to Spring View Farms

Salt Lake County currently has hired a consultant to plan and design a paved trail from Parry Farms development north to the existing Spring View Farms trailhead. Construction is scheduled to begin Spring 2008. The trail will travel north through the railroad property, then follow 1300 West to 14600 South, where a new street crossing will be installed to connect with the existing Spring View Farms Trail corridor.

3.2 B Flooding at Bangerter Highway Underpass

These areas are subject to periodic flooding during period of peak runoff and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.2 C Arrow Trailhead and Paved Trail

Complete Development of Trailhead at Arrow Trailhead for paved and equestrian trail users. Develop a trail connection from the Trailhead to the existing trails on the west side of the river. The trail would proceed east, through Salt Lake County owned land, through a small section of private land, and through Salt Lake Valley Sewer District land, connecting with the existing trails west of Bridge B2.

Recommendations: Equestrian Trail

3.2 D Equestrian Trail Gap: Parry Farms to Bridge B2

Equestrian trails are not part of the current trail development project between Parry Farms and 14600 South. The County plans on developing an equestrian trail from Parry Farms (shown on Segment 1 map) north to 14600 South in a future phase. This equestrian trail would begin at the future Parry Farms bridge, following north through the railroad property, and then staying closer to the river, following the east bank of the river as it bends to the west and then north again to 14600 South.

The trail would cross 14600 South at a new Street Crossing, continue north around an existing wetland, then east to the Future Equestrian Bridge Crossing over the Jordan River which takes equestrians to the County-owned land on the north side of the Jordan River. The trail would follow the northern boundary of this property, and trace through the County owned strip, eventually meeting up again with the paved trail at the footbridge just south of Bangerter Highway. The equestrian

trail would then parallel the paved trail, with as much separation as possible, pass under Bangerter Highway, and parallel the paved trail to Bridge B2.

A connecting spur to join the existing Arrowhead Equestrian trailhead to the Jordan River Trail corridor is described below in 3.2 E.

Negotiations with private property owners will be required, but the connection is important for a high level of trail corridor functionality.

All new equestrian trails need to be signed according to the signage program discussed in Chapter 5 and properly maintained.

3.2 E Arrow Equestrian Trailhead

Complete development of the Trailhead at Arrow Trailhead as noted in 3.2 C above, and develop an equestrian trail connection to the existing equestrian trail on the west side of the river. The trail would proceed east through County-owned land, a small segment of private land, and South Valley Sewer District land, connecting with the existing equestrian trail west of Bridge B2.

Negotiations with utilities and private property owners will be required, but the connection is important for a high level of trail corridor functionality.

All new equestrian trails need to be signed according to the signage program discussed in Chapter 5 and properly maintained.

3.2 F Equestrian Trail Gap Alternative: Parry Farms to Bridge B2

An alternative equestrian route could continue from the future equestrian trail and bridges at Parry Farms north through the railroad property, along the west side of 1300 West to 14600 South. Equestrian trail users would then use the future 14600 South street crossing that is part of the County's current trail development work in this section. Equestrians would travel north, along the west side of Spring View Farms, then follow the river west to a proposed equestrian bridge crossing, which takes equestrians to the County-owned land on the north side of the Jordan River. The trail would then continue as described above in Section 3.2 D.

3.2 G Equestrian Trail Gap Alternative: Parry Farms to Bridge B2

Another alternative route for this equestrian trail section could begin at the future Parry Farms bridge, following north through the railroad property, and then staying closer to the river, following the east bank of the river as it bends to the west and then north again to 14600 South, as in 3.2 D above. At 14600 South, the trail would travel west closer to the river. There would have to be an additional street crossing just east of the Jordan River. The trail would then continue along the east bank of the Jordan River, to an alternative future equestrian bridge location. Trail users would then follow the west bank of the river northward to the County owned parcel, where the trail would then continue as described above in Section 3.2 D.

JORDAN RIVER TRAIL MASTER PLAN

3.2 H Street Crossing

An at-grade street crossing will be installed to provide a safe crossing point for pedestrians.

3.2 I Street Crossing

An at-grade street crossing will be installed to provide a safe crossing point for equestrians.

3.2 J Bridge

An equestrian bridge will be installed across the Jordan River to accommodate equestrian users.

3.2 K Street Crossing

If needed, an at-grade street crossing will be located in this area to allow equestrian users to safely cross 14600 South near the river.

3.2 J Alternative Bridge

If needed, an equestrian bridge will be installed across the Jordan River.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.2 A	Funded
Signage	3.2 B	\$1,000
Trailhead & Paved Trail	3.2 C	\$418,800
Equestrian Trail	3.2 D	\$665,280
Equestrian Trail	3.2 E	\$52,800
Equestrian Trail	3.2 F	\$73,920
Equestrian Trail	3.2 G	\$31,680
Crossing	3.2 H	\$5,000
Crossing	3.2 I	\$5,000
Equestrian Bridge	3.2 J	\$250,000
Crossing	3.2 K	\$5,000
Equestrian Bridge	3.2 L	\$250,000

Ownership

Ownership for recommended trail alignments is shown in the maps below, followed by detailed ownership information in tables.

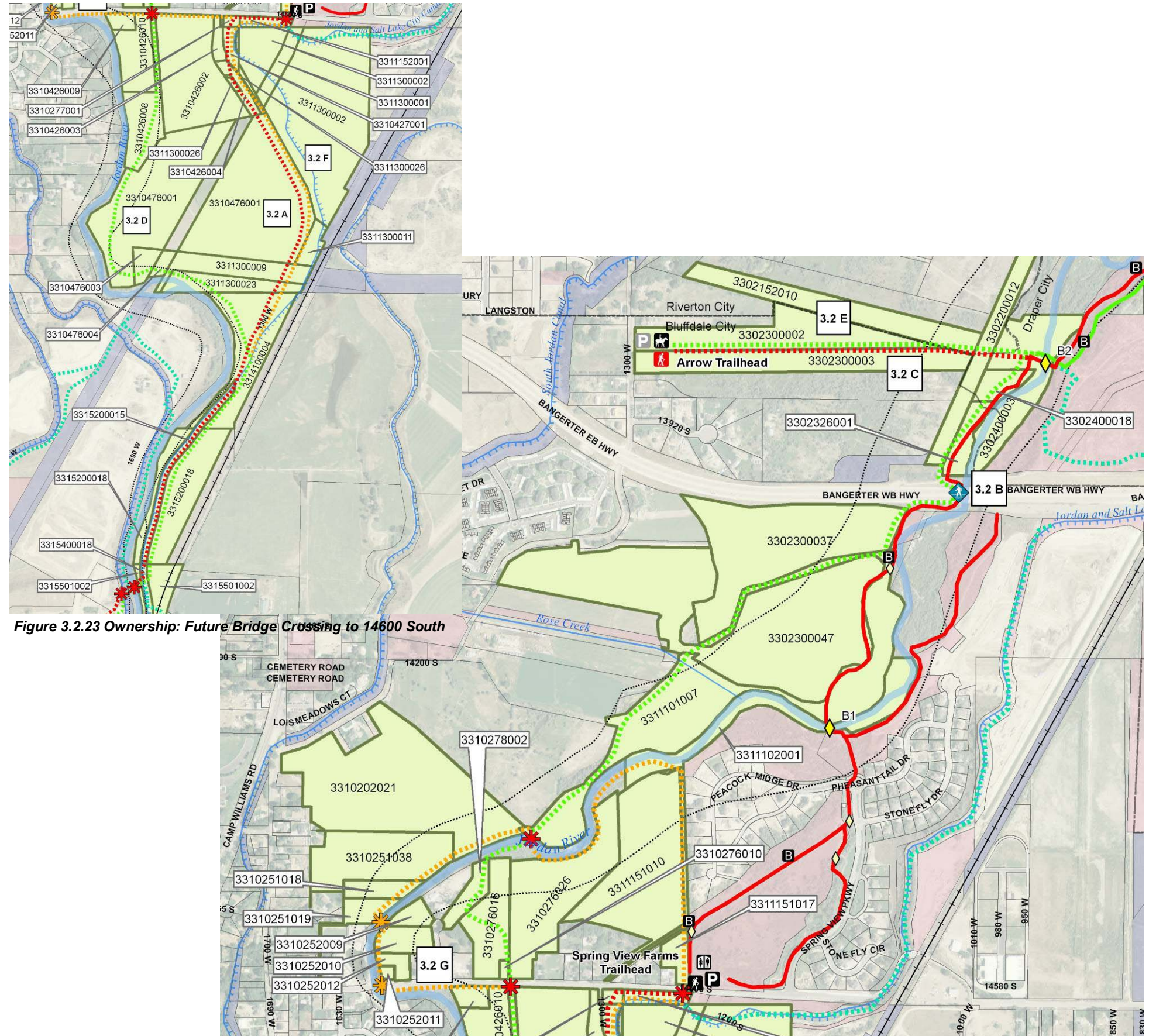


Figure 3.2.23 Ownership: Future Bridge Crossing to 14600 South

Figure 3.2.24 Ownership: 14600 South to Bridge B2

Table 3.2.1—Ownership: Recommendation 3.2 A—Paved

Parcel #	Owner	Acres
3310277001	Rhonda Hansen	0.4
3310426003	Michael Carlson	1.0
3310426004	Michael Carlson	1.4
3310427001	Rhonda Hansen	1.1
3310476001	Michael Carlson	26.7
3311152001	Francis Black	1.2
3311300001	Francis Black	2.4
3311300002	Francis Black	30.9
3311300002	Francis Black	4.7
3311300009	Utah Power and Light	6.0
3311300011	Michael Carlson	1.7
3311300023	Utah Power and Light	4.9
3314100004	City of Bluffdale	12.1
3315200015	Salt Lake City Corporation	3.2
3315400018	Salt Lake City Corporation	1.9
3311300026 - east	Michael Carlson	0.4
3311300026 - west	Michael Carlson	0.2
3315200018 - east	City of Bluffdale	9.7
3315200018 - west	City of Bluffdale	2.6
3315501002 - east	not available	9.0
3315501002 - west	not available	0.6

Table 3.2.1—Ownership: Recommendation 3.2 C— Paved

Parcel #	Owner	Acres
3302152010	Dan Fischer	8.2
3302400003	not available	9.4
3302200012	South Valley Sewer District	9.5
3302300003	Spring View Capital LLC	9.9
3302300002	Salt Lake County	10.0

Table 3.2.1—Ownership: Recommendation 3.2 D—Equestrian

Parcel #	Owner	Acres
3302300037	Ivan Nielsen	19.0
3302300047	Spring View Capital LLC	37.7
3302326001	Ivan Nielsen	2.2
3302400003	not available	9.4
3302400018	Spring View Capital LLC	1.8
3310276010	Keith Reels	1.2
3310276016	Moyal Anderson	5.2
3310278002	Richard Jones	1.8
3310426002	Michael Carlson	16.3
3310426008	Charmaine Smith	6.4
3310426010	Leesa Clark-Winger	5.4
3310476001	Michael Carlson	23.1
3310476003	Salt Lake County Water Conservancy District	1.6
3310476004	Utah Power and Light	0.9
3311101007	Salt Lake County	23.4
3311300009	Utah Power and Light	6.0
3311300023	Utah Power and Light	4.9
3314100004	Bluffdale City	12.1
3315200015	Salt Lake City Corporation	3.2
3315400018	Salt Lake City Corporation	1.9
3315200018 - east	Bluffdale City	9.7
3315200018 - west	Bluffdale City	2.6
3315501002 - east	not available	9.0
3315501002 - west	not available	0.6

Table 3.2.2—Ownership: Recommendation 3.2 E—Equestrian

Parcel #	Owner	Acres
3302152010	Dan Fischer	8.2
3302200012	South Valley Sewer District	9.5
3302300002	Salt Lake County	10.0
3302300003	Spring View Capital LLC	9.9
3302400003	not available	9.4

Table 3.2.3—Ownership: Recommendation 3.2 F—Equestrian

Parcel #	Owner	Acres
3310277001	Rhonda Hansen	0.4
3310426003	Michael Carlson	1.0
3310426004	Michael Carlson	1.4
3310427001	Rhonda Hansen	1.1
3310476001	Michael Carlson	26.7
3311152001	Francis Black	1.2
3311300001	Francis Black	2.4
3311300002	Francis Black	4.7
3311300002	Francis Black	30.9
3311300009	Utah Power and Light	6.0
3311300011	Michael Carlson	1.7
3311300023	Utah Power and Light	4.9
3314100004	Bluffdale City	12.1
3311300026 - east	Michael Carlson	0.4
3311300026 - west	Michael Carlson	0.2

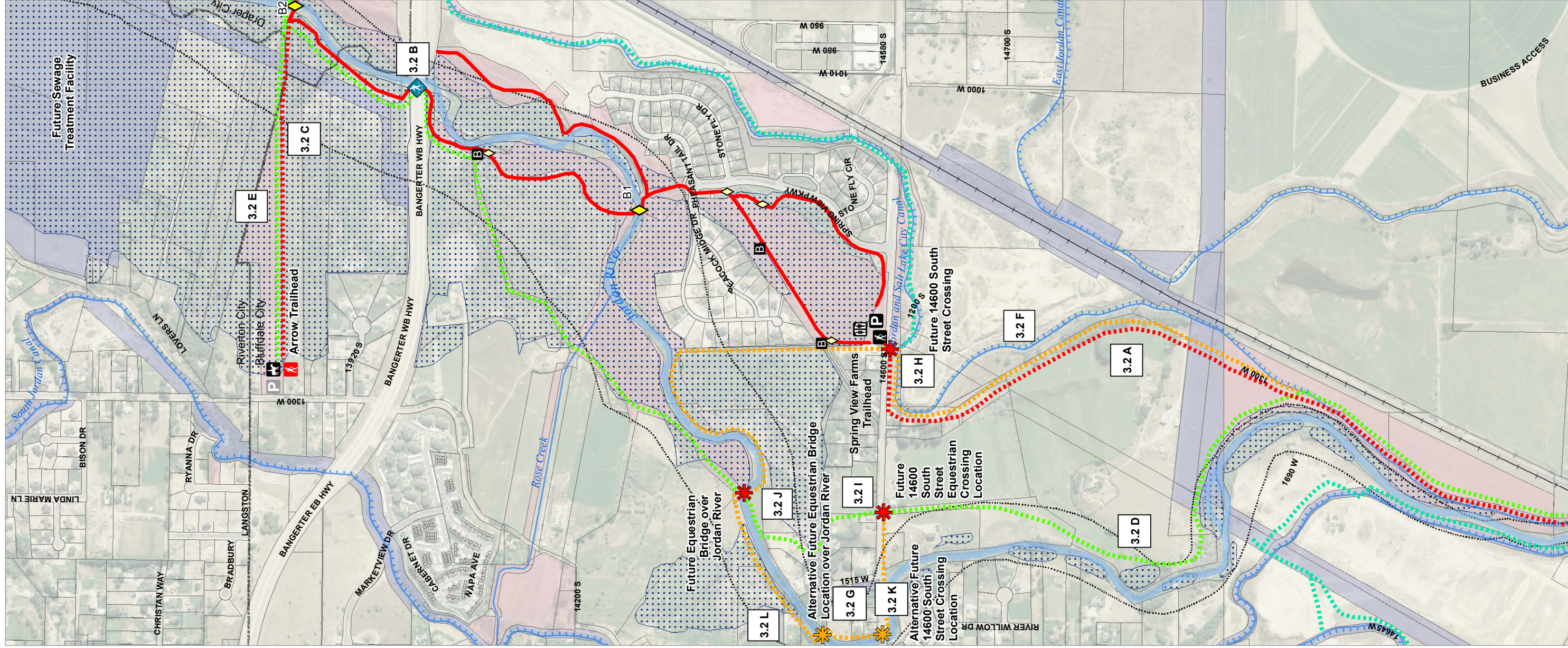
Table 3.2.4—Ownership: Recommendation 3.2 G—Equestrian

Parcel #	Owner	Acres
3310202021	Spring View Capital LLC	24.7
3310251018	Sherm Wayman	1.7
3310251019	Sherm Wayman	1.7
3310251038	Phil Mathews	9.2
3310252009	Russell Utley	1.3
3310252010	Guy Gustaveson	2.0
3310252011	Lorin Crump	0.3
3310252012	Dennis Crump	0.9
3310426009	John Mcomie	1.0
3310426010	Leesa Clark-Winger	5.4

Jordan River Trail Master Plan

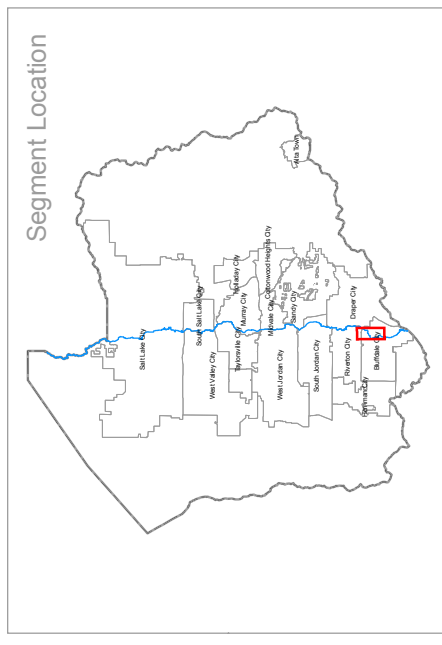
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Unpaved Jordan River Trail
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 2: Bluffdale/Riverton

15400 South to Bangerter Highway



3.3 Segment 3: Bluffdale/Riverton/Draper (Bangerter Highway to Vahe Street)

The paved Jordan River Trail continues north from the Bangerter Highway underpass and crosses to the east side of the river over a large pedestrian bridge (Bridge B2). The trail proceeds north, steeply in some sections, winding past a small hot springs area and an old barn and silo, with several benches along the way. The trail crosses a Corner Canyon Creek with a culvert, and a separate unpaved equestrian trail begins at this point and runs parallel to the paved trail.

The trails diverge in separate directions near Cephus Circle. The equestrian trail proceeds northward, tracing along the western perimeter of the neighborhood and then curves to the northwest following the Galena Canal, where it terminates at 12600 South with no connection to the north.



Figure 3.3.1 Bridge B2
6' wide, 90' span, 20' clearance



Figure 3.3.2 Bridge B3
8' wide, 130' span, 10' clearance

The paved trail, parallel to and separated by an equestrian trail, follows closer to the Jordan River utilizing a sewer easement, and curves to the northwest where it splits. One segment crosses the Jordan River via a pedestrian bridge (Bridge B3). This part of the path changes to a boardwalk for a short segment, passes through a marshy area and then switchbacks up toward Riverbend Park and the Riverbend Recreation Center. Several benches are located along the trail in this section. A formal trailhead and parking lot provide access to the trail near the South County Pool and Riverbend Golf Course. The main Jordan River Trail continues north through an area intended for utility vehicle access. It appears that other people have been removing the access chain and driving across the trail, parking in the open gravel area. The trail passes under 12600 South

Riverbend Park

- Benches
- Open Lawn Area
- Trash Receptacles
- Trailhead Sign



Figure 3.3.3 Riverbend Park

through a pedestrian underpass. A small rest area with a bench is located just north of the 12600 South underpass. The paved trail proceeds to the northeast with a separate unpaved equestrian trail nearby. The Jordan River Rotary Park provides access to the trail with a formal trailhead, where the trail splits. One segment crosses a large vehicular/pedestrian bridge with removable bollards (Bridge B4, also known as Tithing Bridge), passes through Dr. O. Roi Hardy Park, and winds north along the western bank of the Jordan River. This portion of the trail is furnished with many benches along the way and several boardwalks lead users to overlooks, including a gazebo overlooking a wildlife viewing area with a small pond. Access to the neighborhood west of the river is provided by a paved trail and footbridge.



Figure 3.3.4 Bench overlooking river

The other trail segment parallels the eastern bank of the Jordan River, with a separate unpaved equestrian trail following along at varying distances from the paved trail.

The other trail segment parallels the eastern bank of the Jordan River, with a separate unpaved equestrian trail following along at varying distances from the paved trail.



Figure 3.3.5 Old Silo



Figure 3.3.6 Hot Springs



Figure 3.3.7 Hot Springs warning sign



Figure 3.3.8 Equestrian trail beginning at culvert, looking northeast

through a pedestrian underpass.

A small rest area with a bench is located just north of the 12600 South underpass. The paved trail proceeds to the northeast with a separate unpaved equestrian trail nearby. The Jordan River Rotary Park provides access to the trail with a formal trailhead, where the trail splits. One segment crosses a large vehicular/pedestrian

bridge with removable bollards (Bridge B4, also known as Tithing Bridge), passes through Dr. O. Roi Hardy Park, and winds north along the western bank of the Jordan River. This portion of the trail is furnished with many benches along the way and several boardwalks lead users to overlooks, including a gazebo overlooking a wildlife viewing area with a small pond. Access to the neighborhood west of the river is provided by a paved trail and footbridge.

The other trail segment parallels the eastern bank of the Jordan River, with a separate unpaved equestrian trail following along at varying distances from the paved trail.



Figure 3.3.9 Marshy areas west of equestrian trail



Figure 3.3.10 View looking east from trail near Cephus Circle



Figure 3.3.11 View looking south on trail near Cephus Circle



Figure 3.3.12 View looking north toward equestrian trail



Figure 3.3.13 Buried sewer river crossing sign west of trail



Figure 3.3.14 Boardwalk entry into marshes



Figure 3.3.15 View toward Riverbend Golf Course Clubhouse from trail



Figure 3.3.16 View to river, looking southeast, from trail

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.3.17 Boardwalk winding through the wetlands



Figure 3.3.18 View to wetlands from trail, looking east



Figure 3.3.26 Start of equestrian trail north of 12600 South



Figure 3.3.27 View of Jordan River from trail, looking northeast



Figure 3.3.32 Dog Waste Station



Figure 3.3.33 Bridge B4, looking west at Dr. O. Roi Hardy Park



Figure 3.3.19 Benches near Riverbend Trailhead



Figure 3.3.20 Trail access from Riverbend Park Trailhead

Jordan River Rotary Park

- Restrooms—Year Round
- Trash Receptacles
- Dog Waste Station
- Trailhead Sign & Map
- Playground
- Parking (26 standard stalls, 2 handicapped)



Figure 3.3.28 Jordan River Rotary Park



Figure 3.3.34 Custom bench along Riverton segment of trail



Figure 3.3.35 Nesting platform



Figure 3.3.21 Trail access from Riverbend Park Trailhead



Figure 3.3.22 12300 South underpass, looking south



Figure 3.3.29 Bridge B4 24' wide, 73' span, 12' clearance



Figure 3.3.30 Looking south from Bridge B4



Figure 3.3.36 Wildlife viewing in winter



Figure 3.3.37 Paved access to neighborhood



Figure 3.3.24 12600 South underpass detailing



Figure 3.3.25 Bench rest area north of 12600 South

Dr. O. Roi Hardy Park

- Large Pavilion
- Picnic Tables
- Restrooms
- Benches
- Open Lawn Area
- Trash Receptacles
- Parking (50 standard, 2 handicapped)



Figure 3.3.31 Dr. O. Roi Hardy Park



Figure 3.3.38 Gazebo overlook

Issues and Opportunities

- There are steep sections of the trail between Bridge B2 and the Old Silo.
- There is a desire by residents to develop a Nature Center in Draper, near the Old Barn and Silo.
- The land east of the Hot Springs is under a conservation easement.
- There is no equestrian trail south of Bridge B2.
- The equestrian trail between Bangerter Highway and 12600 South needs to be signed and maintained and existing trails need to be preserved as the Galena subdivision develops.
- Flooding occurs at the 12600 South underpass during peak river flow.
- There is an existing utility vehicle access point on the east side of the river on the south side of 12600 South, and that vehicles must drive across part of the paved trail to access this area. It appears that trail users have been removing the access chain, driving across the trail, and parking in an open gravel area east of the river.
- The equestrian trail from Bridge B3 to 12600 South trailhead needs to be identified, marked, and mowed and/or surfaced with bark mulch.
- The equestrian trail from 12600 South trailhead to Alan Point Drive along the east bank of the river needs to be separated from the paved trail (preferably by at least 20'), identified with signage, mowed, and/or surfaced with bark mulch.
- The pump house in Dr. O. Roi Hardy Park encroaches on the equestrian trail west of the Jordan River.

Recommendations: Paved Trail

3.3 A Utah State Department of Natural Resources Land/Steep Trail
If the opportunity arises at some point in the future, the paved trail should be re-routed to relocate the trail to the recommended alignment, but this is not a high priority.

Many trail connections have been proposed through the Utah State Department of Natural Resources lands on the east side of the Jordan River; however, the existing paved trail, including the recommended realignment near the silo, should remain the official Jordan River paved trail. If Draper City wishes to develop additional paved trails, they should be simple and connected, and not over-done. A suggested alignment is shown on the map. These trails will become Draper City trails.

3.3 B 12600 South Informal Trail Access

This maintenance access point needs to restrict unauthorized vehicles more effectively, and should be retrofitted with a gate that can be accessed only by authorized personnel. Gravel areas where people have been parking need to be revegetated and enhanced to blend with the Riverbend Nature Area and Jordan River corridor.

3.3 C Flooding at 12600 South Underpass

These areas are subject to periodic flooding during period of peak run-

off and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

Recommendations: Equestrian Trail

3.3 D Equestrian Trail Opportunity: Utah State Department of Natural Resources Land

Many equestrian trails have been proposed through the Utah State Department of Natural Resources lands on the east side of the Jordan River; however, the existing equestrian trail should remain the official Jordan River Equestrian Trail. Additional equestrian trails are discouraged.

3.3 E Equestrian Trail Gap: North end of Riverbend Nature Area

Develop an equestrian trail that connects the trails surrounding the Riverbend Nature Area, providing a loop trail aligned with 12600 South. The trail should be separated from the roadway and be contained within the Nature Area.

3.3 F Equestrian Trail Alignment: Dr. O. Roi Hardy Park

The new pump house in Dr. O. Roi Hardy park encroaches on the equestrian trail west of the Jordan River. The equestrian trail through the Dr. O. Roi Hardy Park needs to be re-aligned to accommodate equestrian use through this area.

3.3 G Equestrian Trail Maintenance

The equestrian trail between Bangerter Highway and Alan Point Drive (Segment 4) needs to be signed according to the signage program described in Chapter 5, and maintained properly to ensure a high level of functionality for equestrian users.

Construction Costs

Table 3.3.1—Construction Costs: Paved Trail Segment 3

Feature Category	Map Code	CIP Budget Implication
Paved Trail/Policy	3.3 A	\$118,000
Access	3.3 B	\$30,000
Signage	3.3 C	\$1,000
Policy	3.3 D	N/A
Equestrian Trail	3.3 E	\$21,200
Equestrian Trail	3.3 F	\$5,280
Signage	3.3 G	\$3,000

Ownership

Ownership for recommended trail alignments is shown in the map below, followed by detailed ownership information in tables.

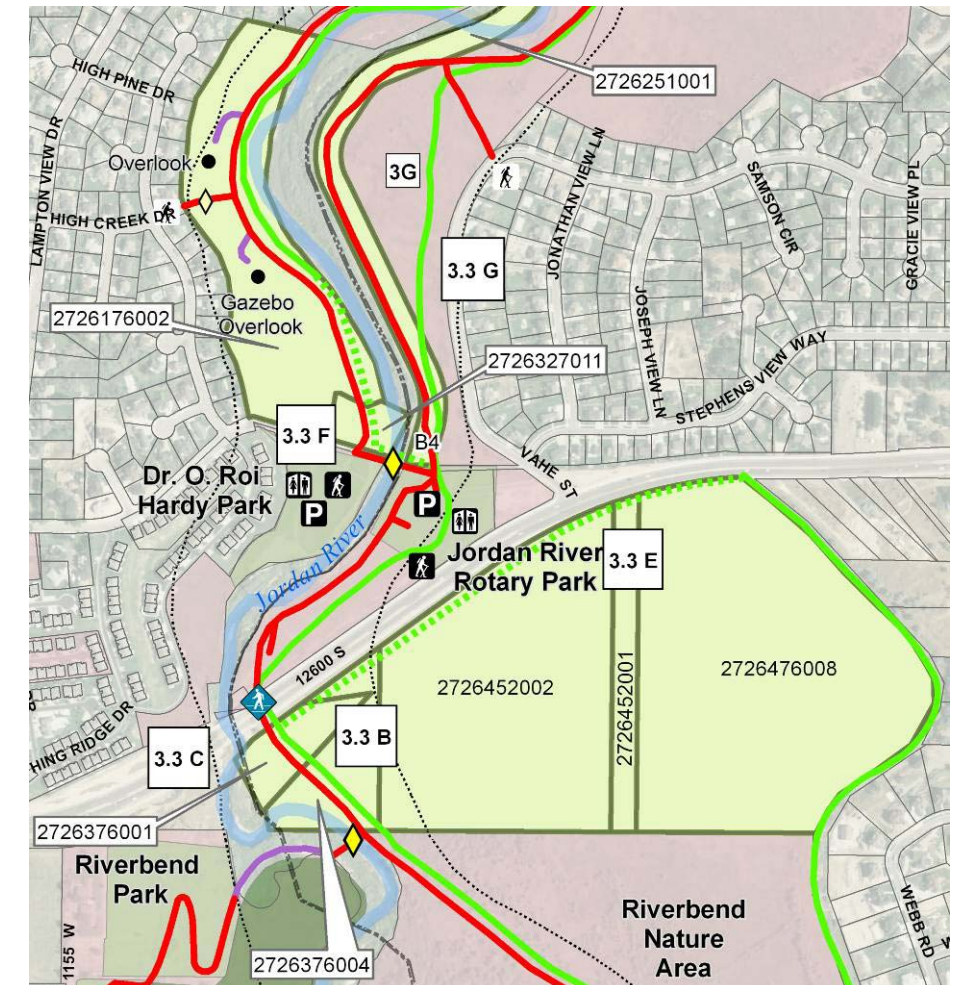


Table 3.3.2—Ownership: Recommendation 3.3 E—Equestrian

Parcel #	Owner	Acres
2726376004	US Government	4.8
2726452001	State of Utah Division Parks/Recreation	3.7
2726452002	US Department of the Interior	26.5
2726476008	US Department of the Interior	34.5
2726976001	Utah Department of Transportation	2.7

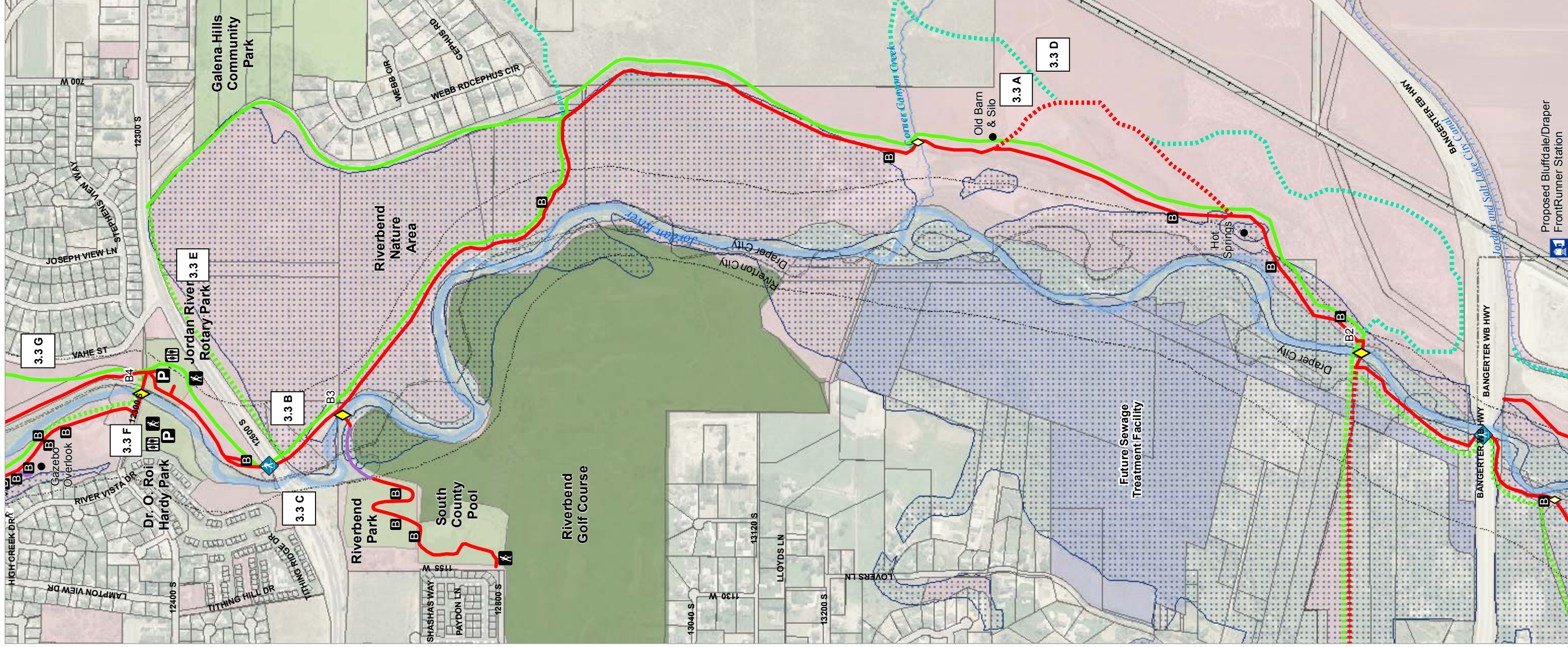
Table 3.3.3—Ownership: Recommendation 3.3 F—Equestrian

Parcel #	Owner	Acres
2726176002	Riverton City	16.8
2726251001	Utah Department of Natural Resources	7.5
2726327011	Riverton City	1.4

Jordan River Trail Master Plan

Paved Trail Facilities

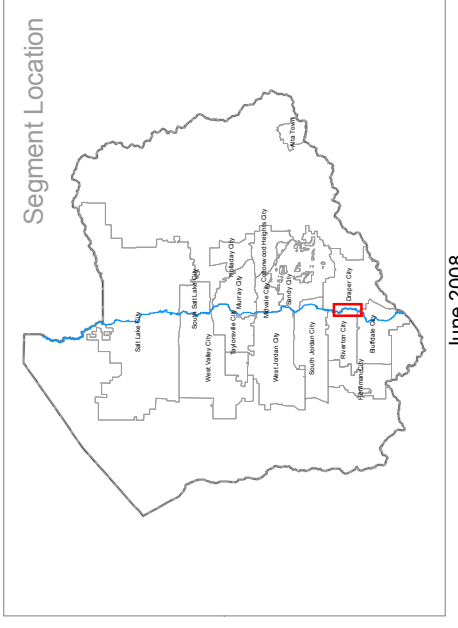
- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 3: Bluffdale/Riverton/ Draper

Bangerter Highway to Yahi Street

0 600 1,200 Feet



Proposed Bluffdale/Draper
FrontRunner Station

June 2008

3.4 Segment 4: Riverton/Draper/South Jordan (Vahe Street to Warm Creek Road)

The eastern trail segment continues north with paved access to the neighborhood via Vahe Street. In a joint effort, the County and Draper City recently completed the paved trail with a 3' shoulder for equestrian use in conjunction with a stream bank stabilization project, which continues the trail north to 11400 South on the east bank of the river. There is a small neighborhood connection to the east, terminating at an emergency turnaround on Alan Point Drive. The equestrian connection continues along Alan Point Drive to 700 West, where it ends.

The western trail segment curves around to the east and heads north again, winding between the Jordan River and several large horse properties. The trail then curves sharply to the west through a narrow trail easement. The trail turns north for a short distance and terminates at Reeves Lane. There is a gap in the Jordan River Trail corridor between Reeves Lane and the Riverfront Park area.

Riverfront Park on the west side at 11100 South offers trail access at several points along Riverfront Trail and provides parking in two separate parking lots. A separate equestrian trail spurs-off of the paved trail near Louise Meadow Drive, tracing along the western bank of the Jordan River. Both trails join together to cross Midas Creek via a small footbridge, and separate again as they wind to the north and east along the river. Several benches are located along the trail, and access to the other side of the South Jordan Trail (10600 South) is provided with a pedestrian/equestrian underpass. As the trail travels through the underpass on the north side of the South Jordan Trail past Mulligan's Golf, the paved trail traces along close to the western bank of the Jordan River with the equestrian trail located to the west.



Figure 3.4.1 Trail along western shore of river, looking north as it begins to wind around horse properties

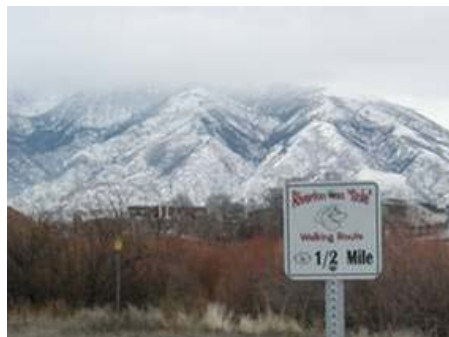


Figure 3.4.2 "Riverton has Sole" walking route sign



Figure 3.4.3 Neighborhood trail access point



Figure 3.4.4 Jordan River Trail access sign



Figure 3.4.6 Eastern shore trail, north of parking lot, looking north



Figure 3.4.8 Equestrian trail paralleling paved trail



Figure 3.4.10 "Beginning" of trail near Rambouillet Drive and Riverfront Trail



Figure 3.4.5 Designated Wetland sign



Figure 3.4.7 Trail looking east, joined by trail access path from Vahe Drive



Figure 3.4.9 Informal Trailhead at Alan Point Drive



Figure 3.4.11 View to north along river

Riverfront Park

- Playground
- Open Play Area
- Pavilions & {Picnic Tables
- Angling Ponds
- Benches & Restrooms
- Trash Receptacles
- Drinking Fountains
- Dog Waste Stations
- Parking (south lot: 19 standard, 1 handicapped; north lot: 31 regular, 2 handicapped)

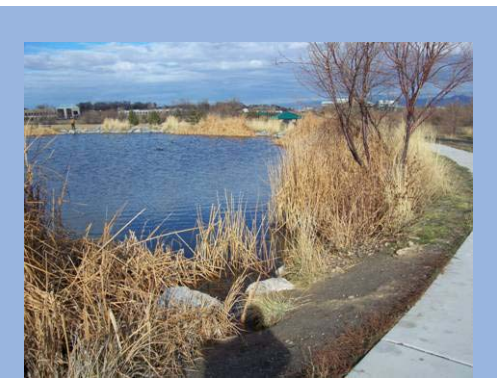


Figure 3.4.13 Riverfront Park



Figure 3.4.12 Picnic pavilion along trail



Figure 3.4.14 Bench along trail, looking south



Figure 3.4.15 Riverfront Park entrance sign



Figure 3.4.16 Riverfront Park signage and restroom facilities



Figure 3.4.17 Ducks sunbathing at Midas Pond



Figure 3.4.18 Midas Pond



Figure 3.4.19 Footbridge in Riverfront Park



Figure 3.4.20 Equestrian trail paralleling trail



Figure 3.4.27 View to west from 10600 South underpass



Figure 3.4.28 View to north from 10600 South underpass



Figure 3.4.21 Footbridge crossing small stream



Figure 3.4.22 Trail looking north



Figure 3.4.29 View to south from 10600 South underpass



Figure 3.4.30 Trail connection to 10600 South near Mulligan's Golf



Figure 3.4.23 Pavilions in northern part of Riverfront Park



Figure 3.4.24 View looking north from trail



Figure 3.4.31 Trail access from 10600 South near Mulligan's Golf



Figure 3.4.32 Trail looking south near Mulligan's Golf



Figure 3.4.25 Trail looking north



Figure 3.4.26 Trail access from 10600 South Business Park parking lot

Issues and Opportunities

- There is a gap in the paved trail and the equestrian trail on the west side of the River between Reeves Lane and Midas Park Pond.

Recommendations: Paved Trail

3.4 A Trail Gap: Reeves Lane to 114th South (west bank)

Develop a paved trail to connect the existing trail ending near Reeves Lane to the existing trail near 11400 South, in Midas Pond Park. The trail passes through some County-owned land, but also through private property. Negotiations with private property owners will be required, but the connection is important.

Recommendations: Equestrian Trail

3.4 B Trail Alignment and Maintenance: 12300 South to Alan Pointe Drive

The equestrian trail in this section needs to be re-aligned to provide the largest separation possible from the paved trail. Chapter 5 establishes desired separation distances between equestrian and paved trail uses. The trail also needs to be signed according to the signage program described in Chapter 5 and maintained properly to ensure a high level of functionality for equestrian users.

3.4 C Trail Gap: Reeves Lane to 11400 South (west bank)

Develop an equestrian trail to connect the existing equestrian trail ending near Reeves Lane to the existing equestrian trail near 11400 South, in Midas Pond Park. The trail passes through some County-owned land, but also through private property. Negotiations with private property owners will be required, but the connection is important.

3.4 D Bridge: 11400 South

Install a bridge over the Jordan River at 11400 South to accommodate pedestrians and equestrians. Ideally, this should take place at the same time the vehicular bridge is installed at this same location.

Construction Costs

Table 3.4.1– Construction Costs: Paved Trail Segment 4

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.4 A	\$118,800
Equestrian Trail/Signage	3.4 B	\$31,680
Equestrian Trail	3.4 C	\$31,680
Bridge over Jordan River	3.4 D	\$250,000

Ownership

Ownership for recommended trail alignments is shown in the map below, followed by detailed ownership information in tables.

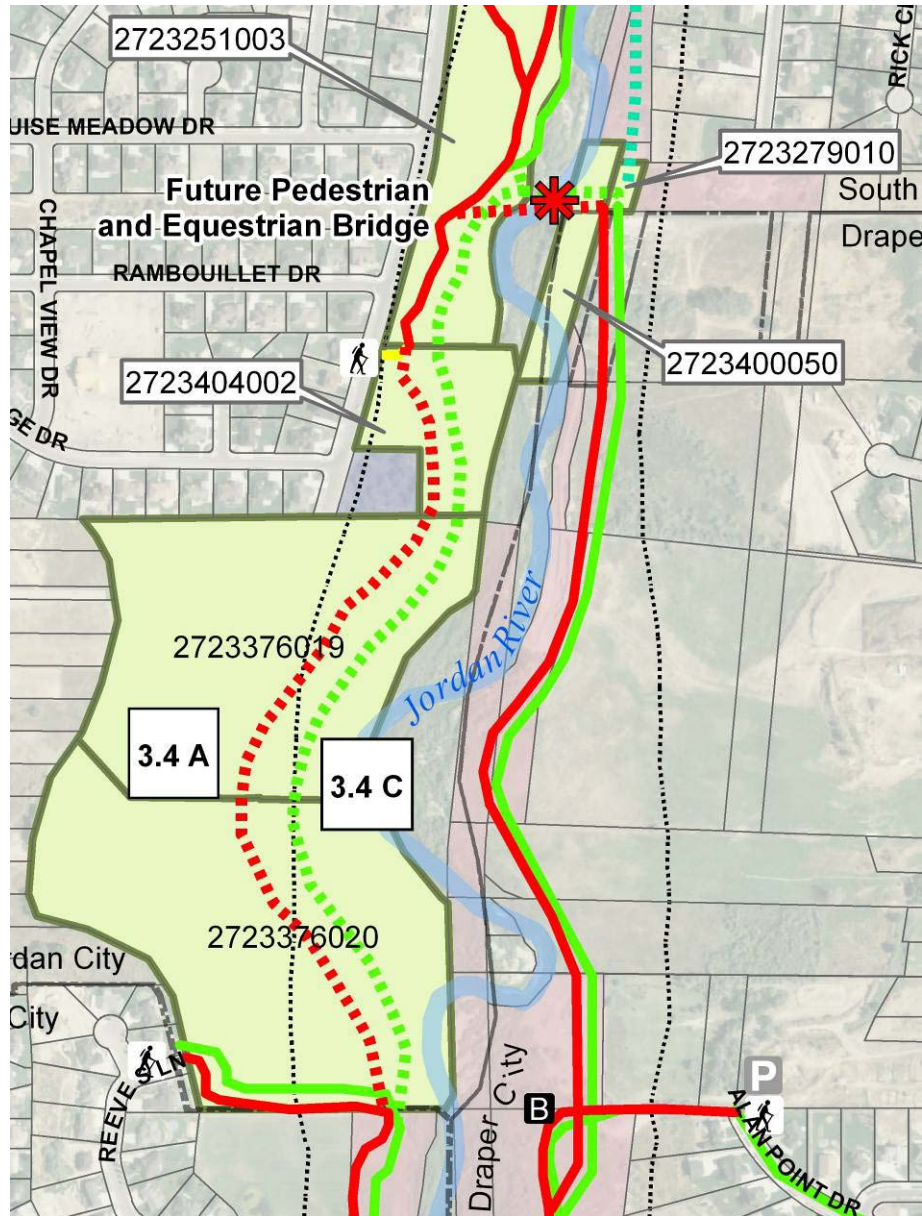


Figure 3.4.33 Ownership: Segment 4

Parcel #	Owner	Acres
2723251003	Provo Jordan River Parkway Authority	15.3
2723279010	South Jordan City	0.3
2723376019	Peter Coats	17.9
2723376020	Peter Coats	20.6
2723400050	not available	1.9
2723404002	South Jordan City	3.8

Jordan River Trail Master Plan

Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing

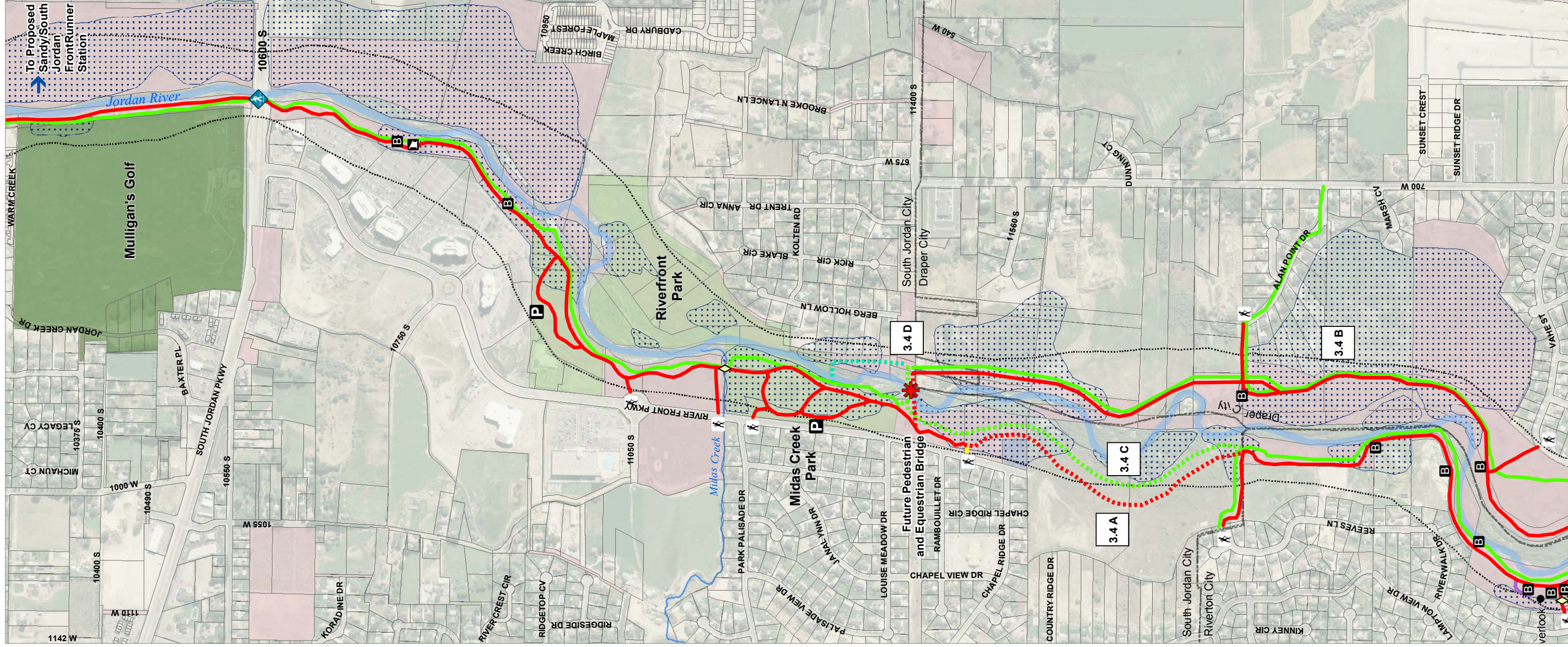
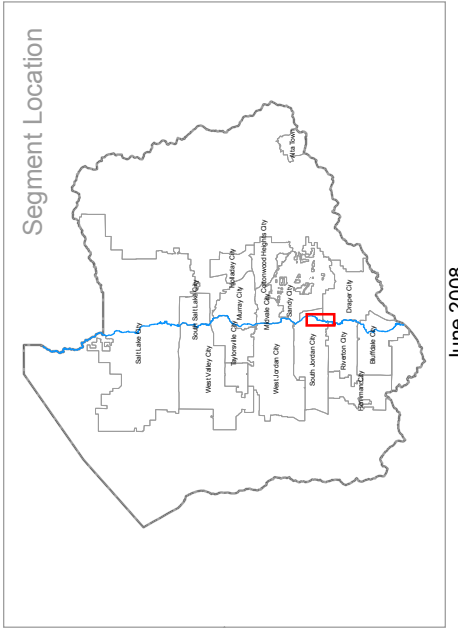
Trail Access & Trail Facilities

- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station

- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land

Segment 4: Riverton/Draper/ South Jordan

Vahe Street to Warm Creek Road



3.5 Segment 5: South Jordan/ Sandy/West Jordan/Midvale (Warm Creek Road to 8450 South)

The Jordan River paved and equestrian trails follow the curve of the river to the northwest, and pass several benches as they head toward the trailhead just south of Shield's Lane. The equestrian trail ends at the Shield's Lane Trailhead.

The Jordan River paved trail continues north through an underpass, taking it under Shield's Lane (10000 South), and travels a short distance, terminating at the western bank of the Jordan River. There is a gap in the trail corridor between 10000 South and 9400 South, where trail access is provided at the River Oaks Golf Course Trailhead.

The trailhead is accessed from Riverside Drive and separate parking for 15 vehicles is provided for trail users. The trail begins at the south end of the parking lot, near a large pedestrian bridge (Bridge B5), which crosses to an area of the course that is off limits to trail users. Currently, this bridge is the south end of this portion of the trail. The trail heads north from this bridge, following the east bank of the Jordan River, past the River Oaks Golf Course Club House. The trailhead parking lot can be accessed from another path near Bridge B6 and ends at 9000 South, near Bridge B7. The trail appears to take users into areas of the golf course that are off-limits, which is reported as confusing by users. There is no underpass or signed crossing to allow trail users to cross 9000 South, and there is a gap in the trail corridor between 9000 South and approximately 8600 South.

The trail resumes at a trail access point located on 700 West and 8600 South in Midvale. The only parking is informal roadside parallel parking near the trail access point. The paved trail follows the Jordan River to the west and several short trails wind in and out, closer to the river. They are designated with crushed asphalt surfacing, and providing several overlook points with benches. The river and trail curve north again, following the western edge of the Bingham Junction area.



Figure 3.5.2 Equestrian trail paralleling paved trail, looking south



Figure 3.5.3 Equestrian trail running adjacent to paved trail, looking south



Figure 3.5.10 Trail looking east near 10000 South



Figure 3.5.11 Shield's Lane Trailhead



Figure 3.5.4 View of Mulligan's Golf from equestrian trail



Figure 3.5.5 Paved trail looking south



Figure 3.5.12 Shield's Lane pedestrian underpass



Figure 3.5.13 Paved trail ends at Jordan River, north of Shield's Lane



Figure 3.5.6 Equestrian trail looking south



Figure 3.5.7 Equestrian trail running aside paved trail



Figure 3.5.14 View south from trail dead end, towards Shield's lane



Figure 3.5.15 View to north where trail dead ends

Shield's Lane Trailhead (10000 South)

- Trailhead Sign & Map
- Restrooms
- Trash Receptacles
- Parking (12 standard, 2 handicapped, undefined equestrian stalls)



Figure 3.5.1 Shield's Lane Trailhead



Figure 3.5.8 Bench overlooking river



Figure 3.5.9 Trail looking south near Shield's Lane Trailhead



Figure 3.5.16 View to Jordan River, north of Shield's Lane, looking east



Figure 3.5.17 View toward trail, north of Shield's Lane, looking west

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.5.18 Ducks in flight

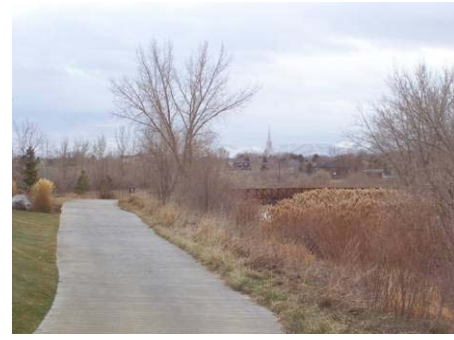


Figure 3.5.19 View looking south near River Oaks Golf Course, towards Bridge B5



Figure 3.5.25 Bridge B6
6' wide, 130' span, 8' clearance



Figure 3.5.26 Bridge B7
10' wide, 130' span, 8' clearance



Figure 3.5.33 View to south east from trail



Figure 3.5.34 Midvale "Gold Medal Mile" marker sign

River Oaks Golf Course Trailhead

- Local Trail Map
- Parking (15 standard)



Figure 3.5.20 River Oaks Trailhead



Figure 3.5.27 View from trail, looking north



Figure 3.5.28 Trail access point at 700 West and 8450 South in Midvale



Figure 3.5.35 Stone bench along crushed asphalt pathway



Figure 3.5.36 Vegetation along Jordan River, looking east



Figure 3.5.21 View from trail near River Oaks Golf Course Club House, looking north



Figure 3.5.22 View from trail towards Bridge B6



Figure 3.5.29 Beginning of trail at 700 West



Figure 3.5.30 Trailhead sign near 700 West

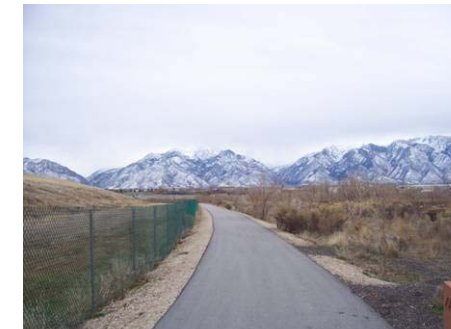


Figure 3.5.37 View to east from trail



Figure 3.5.23 Great Blue Heron wading in Jordan River near River Oaks Golf Course



Figure 3.5.24 Bridge B5
10' wide, 130' span, 9' clearance



Figure 3.5.31 View from trail towards 700 West



Figure 3.5.32 Riparian vegetation along Jordan River near 700 West

Issues and Opportunities

- The equestrian trail ends at the Shields Lane trailhead, with no more equestrian use until Segment 7 in Murray.
- There is a gap in the paved trail between the end just north of Shields Lane and Bridge B5, in River Oaks Golf Course. "No Trespassing" signs in this area are unclear about where the public is allowed.
- There is flooding at the Shields Lane underpass.
- There is a gap in the paved trail between 9000 South and approximately 8500 South 900 West.
- No safe crossing exists for 9000 South.

Recommendations: Paved Trail

3.5 A Flooding at Shields Lane (10000 South) Underpass

These areas are subject to periodic flooding during period of peak run-off and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.5 B Trail Gap: Shields Lane to 8600 South

The trail gap between Shields Lane (10000 South) and Bridge B5 in the middle of River Oaks Golf Course will be paved by the end of Summer 2007. The Utah and Salt Lake Canal Companies are cooperating with Sandy City and South Jordan to make the connection in the 30' wide corridor. This plan recommends continuing the main trail north, from Bridge B5, along the North Jordan Canal Road through the western portion of the River Oaks Golf Course where a safe underpass can be installed under 9000 South.

The existing trailhead and trail along the eastern edge of the golf course should be maintained to provide access to the trail for local residents. Trail users can park at the trailhead and access the main trail by crossing at Bridge B5.

The trail would then continue north, under 9000 South along the canal road and would cross the Jordan River near 8450 South to connect to the existing trail in Midvale.

3.5 C Alternative Route to 9000 South Underpass

A possible alternative would be to add a paved trail segment along the north end of River Oaks Golf Course, just south of 9000 South, where an informal unpaved trail already exists. This would allow trail users quicker access to the new 9000 South underpass if they were headed north on the trail corridor, and would provide a small loop trail around the perimeter of the golf course.

3.5 D Alternative Route from 9000 South to 8600 South

Another alternative route for the paved trail on the north side of 9000 South would continue to the east from the new 9000 South underpass on the North Jordan Canal Road, following the southern boundary of the Rocky Mountain Power substation property. As the trail approaches the Jordan River, it would have to cross the river either at a new, separate pedestrian bridge or the 9000 South vehicular bridge would have to be modified to safely accommodate people crossing the river. The trail would then travel north, paralleling the east bank of the Jordan River through a privately-owned parcel of land falling within unincorporated Salt Lake County. The paved trail then crosses into property owned by West Jordan City, and would join with the existing paved trail just west of 700 West.

3.5 E Unsafe Road Crossing: 9000 South

There is no existing safe crossing near the trail on 9000 South. The County should install a pedestrian underpass along the North Jordan Canal Road, passing under 9000 South along the western edge of River Oaks Golf Course, allowing the trail to continue to the north.

3.5 F Trailhead at River Oaks Golf Course

A trail head is developed at the River Oaks Golf Course which is available to trail users. A path from the parking lot, with a curb cut, to the existing Jordan River Trail is necessary. Currently, there is no pathway to the trail, which is a short distance away, making it unclear and confusing for trail users about whether they are allowed to be in the area and access the trail from this location.

Recommendations: Equestrian Trail

3.5 G Equestrian Trails North of Shields Lane

There are no existing equestrian trails north of Shields Lane in this segment, and no new equestrian trails are proposed in this segment due to the close proximity of the trail corridor to the River Oaks Golf Course and the situation created by the trail corridor running through the golf course from approximately 9400 South to 9000 South. It would be undesirable for all users to mix equestrian, golf, and paved trail uses within such a narrow corridor.

There are no new equestrian trails recommended north of 9000 South due to corridor width limitations further north in segments 6 and 7. It is undesirable to create new segments of equestrian trail that do not have the potential to be linked to other equestrian segments in the future.

Construction Costs

Table 3.5.1- Construction Costs: Paved Trail Segment 5		
Feature Category	Map Code	CIP Budget Implication
Signage	3.5 A	\$1,000
Paved Trail	3.5 B	\$190,080
Paved Trail	3.5C	\$71,280
Paved Trail	3.5 D	\$237,600
Underpass	3.5 E	\$1,000,000
Trailhead	3.5 F	\$5,000
Bridge	3.5 G	\$250,000

Ownership

Ownership for recommended trail alignments is shown in the map below, followed by detailed ownership information in tables.

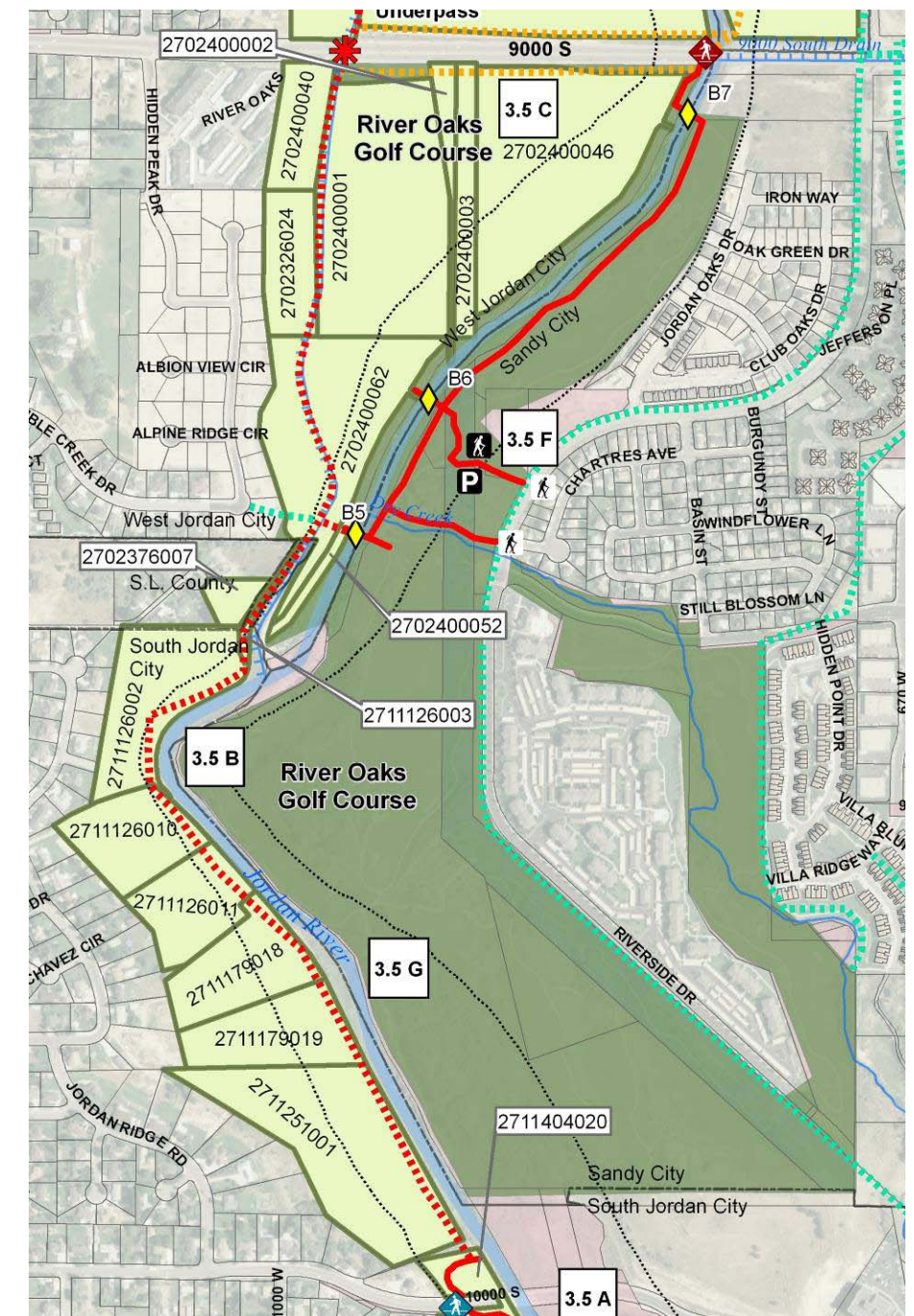


Figure 3.5.38 Ownership: Segment 5, Shield's Lane to 9000 South

Jordan River Trail Master Plan

Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing

Trail Access & Trail Facilities

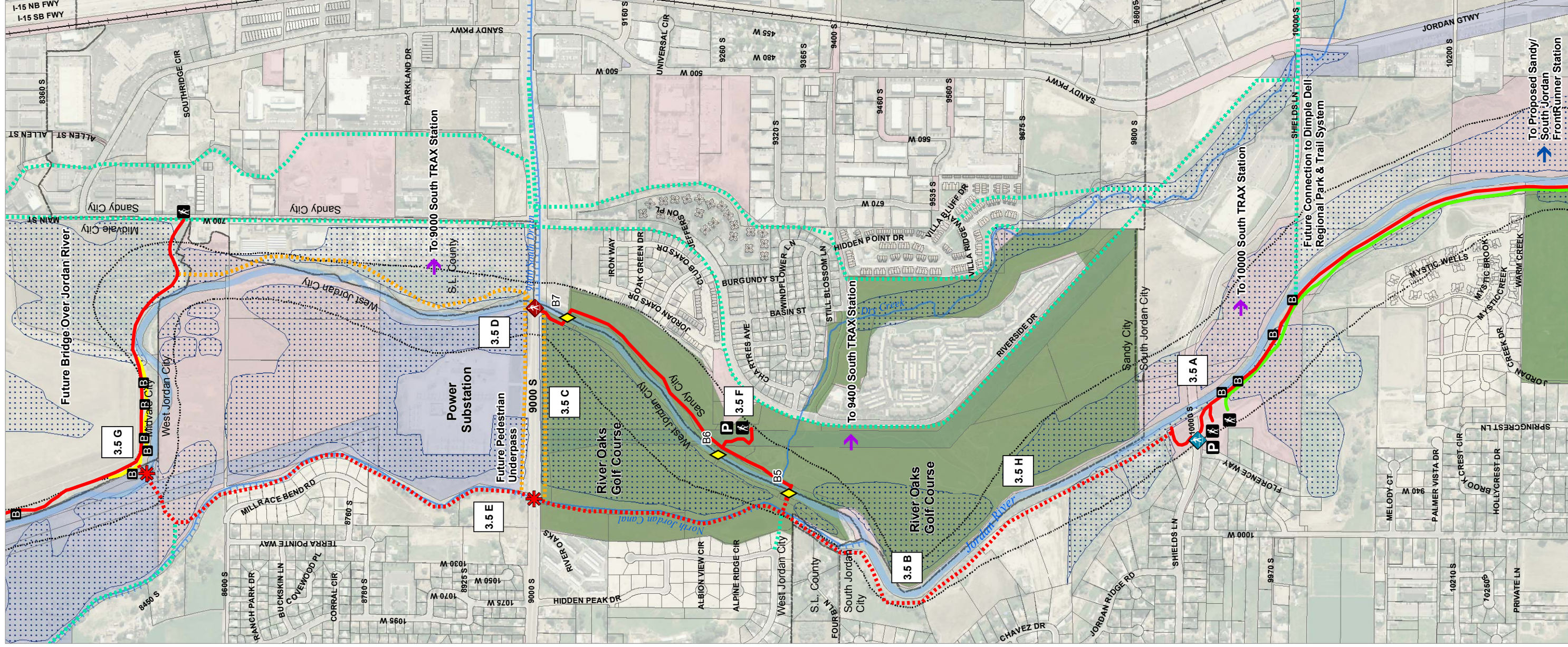
- Future Trailhead
- Equestrian Trailhead
- Formal Trailhead
- Informal Trailhead
- Trail Access Point
- Parking - Formal Paved
- Parking - Informal Unpaved
- Restrooms

Trail Facilities

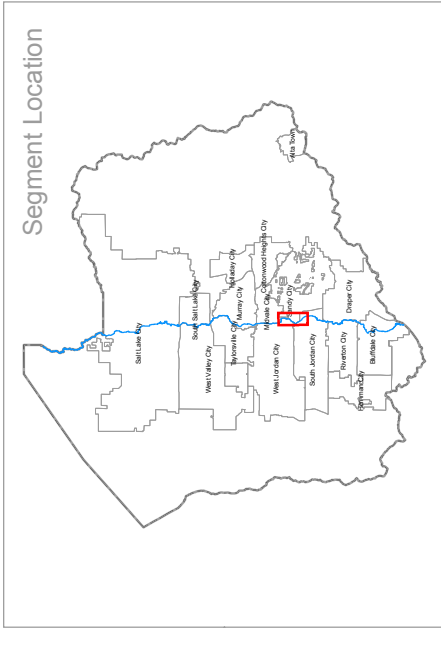
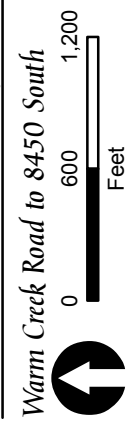
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station

Miscellaneous

- Point-of-Interest
- Bridge Over Jordan River
- Footbridge Over Stream or Canal
- Bench
- Picnic Table
- Trash Receptacle
- City Boundary
- Jordan River
- Streams
- Canals
- Light Rail
- Railroad
- County Boundary
- Meander Corridor (no data north of Surplus Canal)
- Wetlands (WAIDS 2007)
- Golf Course
- Park
- Publicly Owned Land
- Utility Owned Land



Segment 5: South Jordan/Sandy/ West Jordan/Midvale



3.6 Segment 6: West Jordan/Midvale/Murray/Taylorville (8450 South to Winchester Street)

The Jordan River paved trail curves to the north, following alongside the Jordan River, with the power line corridor running along the west bank of the river. Several rest areas are located along the trail with benches located near stone-lined drainage swales. A crushed asphalt pathway meanders down closer to the river, with several benches and plaques for interpretive signage along the path. Near Bridge B8, the trail forks. The north fork takes users to the Midvale Center Street (7800 South) Trailhead. The other fork takes users across the Jordan River over the pedestrian bridge (Bridge B8) and to a trail on the west side of the river. A short unpaved segment runs south of the bridge,



Figure 3.6.1 Bridge B8
16' wide, 90' span, 15' clearance

providing views out over a wetland area with several ponds. From the bridge, the paved trail continues north. The trail can be accessed for maintenance via a gate on the south side of 7800 South, but trail users cross 7800 South using a pedestrian underpass, and emerge at the Gardner Village Trailhead.

The paved trail continues north along the west bank of the Jordan

River with several picnic tables, benches, trash receptacles, and an overlook along the way. The trail crosses a small footbridge which is followed by two loop pathways, and rejoins again to pass under a railroad bridge. The trail winds its way north, crossing another footbridge, before it approaches a group of benches near the 7000 South Trailhead.

The trail then travels under 7000 South using a pedestrian underpass, making its way past recently developed homes towards Winchester Street. Another pedestrian underpass takes users to the north side of Winchester Street to Winchester Park.

Gardner Village Trailhead (North Side of 7800 South)

- Trailhead Sign & Map
- Restrooms
- Picnic Tables
- Plaza
- Open Lawn Area
- Trash Receptacles
- Dog Waste Station
- Parking (in Gardner Village Parking Lot)



Figure 3.6.3 Gardner Village Trailhead

7000 South Trailhead

- Benches
- Trash Receptacles
- Trailhead Sign & Map
- Interpretive Signage & Overlook
- Parking (72 standard, 2 handicapped)



Figure 3.6.4 7000 South Trailhead



Figure 3.6.5 Crushed asphalt pathway



Figure 3.6.6 View from trail looking north



Figure 3.6.9 Bench pull-out



Figure 3.6.10 Water running into Jordan River



Figure 3.6.11 Midvale Trail Marker



Figure 3.6.12 Midvale Center Street Trailhead parking lot



Figure 3.6.13 Statue at Midvale Center Street Trailhead



Figure 3.6.14 Beaver damage along trail

Midvale Center Street (South Side of 7800 South)

- Trailhead Sign & Map
- Picnic Tables
- Trash Receptacles
- Dog Waste Station
- Parking (22 standard, 2 handicapped)



Figure 3.6.2 Midvale Center Street Trailhead



Figure 3.6.7 Stone-lined drainage swale along trail



Figure 3.6.8 Trail looking north



Figure 3.6.15 View of Jordan River, looking north near 7800 South, from Bridge B8



Figure 3.6.16 View to south, west of Bridge B8, looking toward unpaved portion of trail



Figure 3.6.17 Maintenance access from 7800 South



Figure 3.6.18 View to ponds



Figure 3.6.25 Trail overlook



Figure 3.6.26 Footbridge



Figure 3.6.33 Railroad Bridge underpass



Figure 3.6.34 Reclaimed Sewer Water warning sign



Figure 3.6.19 7800 South underpass



Figure 3.6.20 7800 South underpass, north side



Figure 3.6.27 Picnic area



Figure 3.6.28 Ducks along river



Figure 3.6.35 View from trail, looking south



Figure 3.6.36 Footbridge



Figure 3.6.21 Gardner Village trailhead



Figure 3.6.22 Picnic table and lawn area at Gardner Village Trailhead



Figure 3.6.29 Bird's nest along trail



Figure 3.6.30 Beaver damage



Figure 3.6.37 View to Jordan River from trail



Figure 3.6.38 Benches at 7000 South Trailhead



Figure 3.6.23 Trail north of Gardner Village trailhead, looking north



Figure 3.6.24 Trail winding through vegetation, looking south



Figure 3.6.31 Trail loop



Figure 3.6.32 High water sign



Figure 3.6.39 7000 South Trailhead sign



Figure 3.6.40 7000 South underpass



Figure 3.6.41 Footbridge north of 7000 South



Figure 3.6.42 View to north from trail



Figure 3.6.50 Winchester Street underpass



Figure 3.6.51 Winchester Street river crossing, looking north



Figure 3.6.43 View to east from trail



Figure 3.6.44 7000 South overlook & interpretive signage



Figure 3.6.46 Small neighborhood park adjacent to trail



Figure 3.6.47 Trail looking north



Figure 3.6.48 View to south from trail near 6400 South



Figure 3.6.49 Cyclist using trail

Issues and Opportunities

- There are no equestrian trails in this section of the Jordan River Corridor.
- Some equestrian users have expressed a desire to have a continuous equestrian trail corridor between Bluffdale and Murray at a minimum. The existing unpaved trail south of Bridge B8 dead-ends.
- Flooding occurs at the 7800 South underpass during peak river flow. There is flooding at the railroad bridge underpass north of Gardner Village during peak river flow.
- The 7800 South underpass is steep and has sharp turns - this is very dangerous for in-line skaters and pedestrians with strollers.
- There is no safe bicycle access to the trail along 7200 South traveling from the east. A gated community on the north side of the road does not allow public access to the trail corridor through their development.

Recommendations: Paved Trail

3.6 A Midvale Slag and Sharon Steel Site Development

The Midvale Slag and Sharon Steel sites are redeveloping. If Midvale City and the developers wish to develop river-side trails as part of their development, it will be their responsibility. The existing paved trail through this segment should remain the official Jordan River Trail.

For the safety and enjoyment of watercraft users on the river, there should be no additional bridges developed in this section. Trail users can access the trail corridor on the west side of the river at Gardner Village or the Midvale Trailhead using Center Street (7800 South) and the 7200 South Trailhead.

3.6 B Flooding at Center Street (7800 South) Underpass

These areas are subject to periodic flooding during period of peak run-off and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.6 C Dangerous Underpass at Center Street (7800 South)

This underpass is steep with sharp turns, which is very dangerous for in-line skaters and high traffic conditions. This will likely require an engineering solutions, perhaps when the 7800 South bridge is replaced or improved.

3.6 D Flooding at Railroad Bridge Underpass

These areas are subject to periodic flooding during period of peak run-off and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.6 E Streambank Erosion

Portions of the trail between Gardner Village and the railroad bridge have been washing away with high water levels this year. The County plans to eliminate the portions of these two loops that run closest to the river to avoid having to repair these sections continually in the future.

Recommendations: Equestrian Trail

3.6 F Equestrian Trails

There are no existing equestrian trails in this segment of the Jordan River Corridor, and no new equestrian trails are proposed due to width limitations within the corridor.

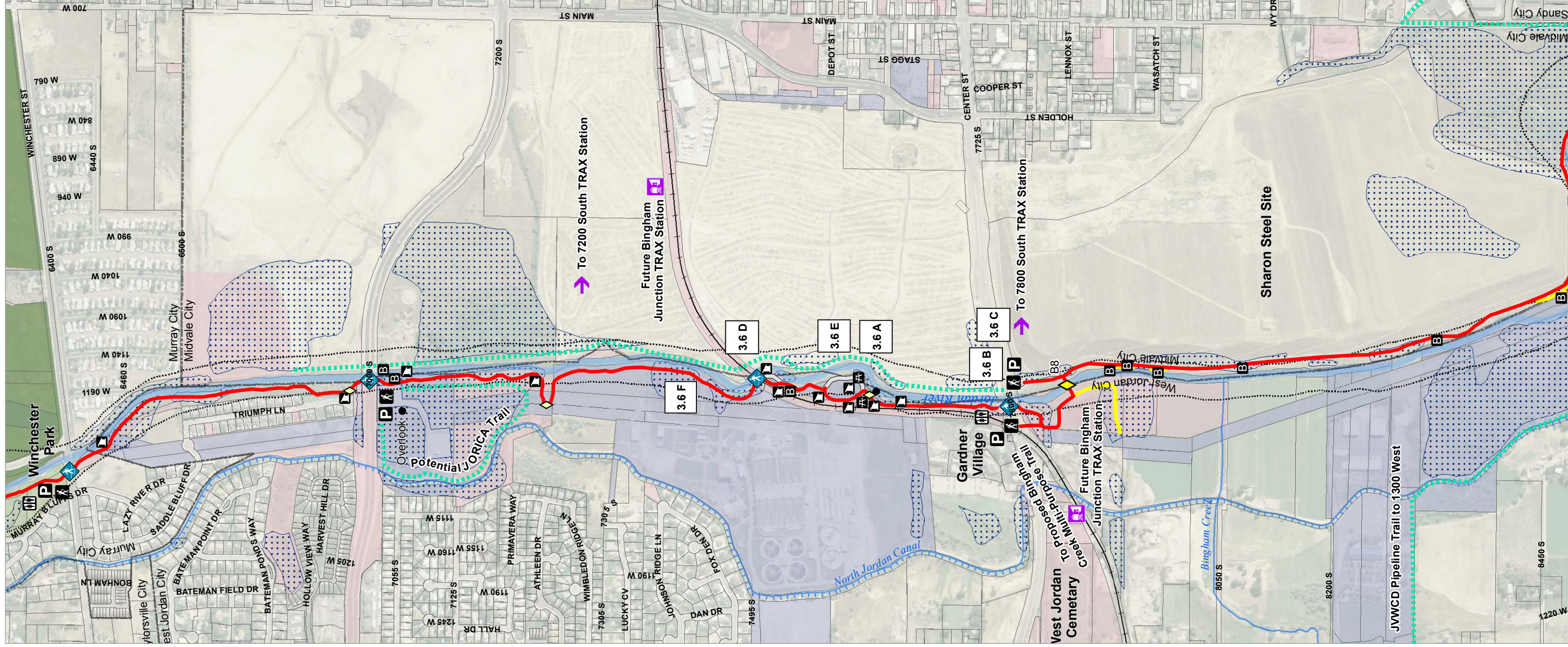
Construction Costs

Table 3.6.1– Construction Costs: Paved Trail Segment 6		
Feature Category	Map Code	CIP Budget Implication
Policy	3.6 A	N/A
Signage	3.6 B	\$1,000
Underpass	3.6C	\$100,000
Railroad Underpass	3.6 D	N/A
Trail Abandonment	3.6 E	\$5,000
Policy	3.6 F	N/A

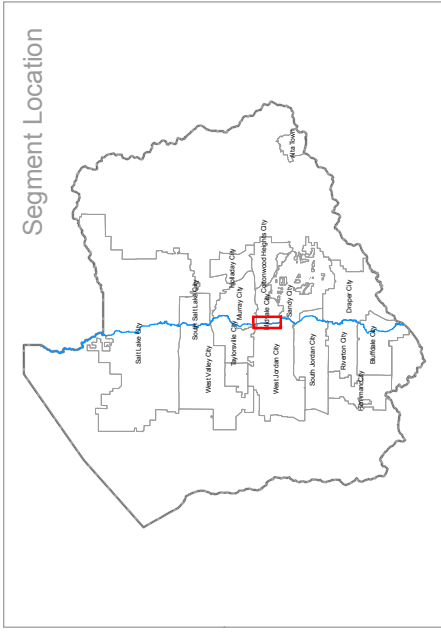
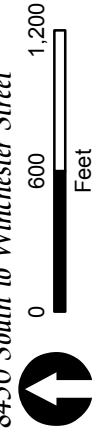
Jordan River Trail Master Plan

Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



8450 South to Winchester Street



3.7 Segment 7: Murray/Taylorsville (Winchester Street to Murray Taylorsville Road)

The Jordan River Trail winds north through Winchester Park, looping through wetlands along the western bank of the Jordan River, with a view of the Murray Parkway Golf Course across the river to the east. Following the edge of Crystal River Drive, the trail continues north past Bridge B9 to a neighborhood access near Interstate 215. The main trail crosses the Jordan River at Bridge B9, where the trail can also be accessed from Murray Parkway Avenue. Trail users pass under Interstate 215 and follow along large looped pathways through a wooded area, and north to Cottonwood Grove Park. Equestrian access begins at Cottonwood Grove Park, with both trails crossing a small neighborhood street using an at-grade crossing.

Winchester Park

- Pavilion
- Restrooms
- Playground
- Picnic Tables
- Benches
- Trash Receptacles
- Parking (55 standard, 4 handicapped, 10 equestrian)



Figure 3.7.1 Winchester Park

The equestrian trail follows the edge of the Jordan River, with the paved trail running to the east, closer to the adjacent neighborhoods. The trail corridor can be accessed from several trail access points in these neighborhoods. The trails curve to the northeast as the river bends, and lead to Walden Park. Access to the neighborhood on the west side of the river is provided just south of 5400 South with Bridge B10.

The equestrian and paved trails pass under 5400 South with separate pathways along the river. The equestrian trail continues to follow the eastern edge of the Jordan River, passing Millrace Park across the river to the west, and features several bench and picnic rest areas along the way. The trail is washed out in two locations and has been rerouted. Horse riders continue over a couple of footbridges, past



Figure 3.7.2 View north from trail near Winchester Park



Figure 3.7.3 Feeding ducks in Winchester Park

Bridge B12 and Germania Park, and end their ride near the Kennecott Environmental Center.

Users of the paved trail continue north from the 5400 South underpass, and may access the trail from the 5400 South parking lot or from Millrace Park via the pedestrian bridge (Bridge B11). The paved pathway follows the curve of Murray Parkway Avenue leading users through Germania Park and past interpretive signage. Users can access the trail using Bridge B12, which provides paved access from a small parking lot on the south side of 4800 South, and from a private neighborhood access to the west. The trail continues to the northeast, running by the Kennecott Environmental Center, overlooks, and interpretive signage, then transitions to a recycled lumber boardwalk running through the wetlands. An informal, unpaved pathway continues from the end of the equestrian trail, following the Jordan River more closely, taking users through a wooded area along the south bank.



Figure 3.7.4 Winchester Park



Figure 3.7.5 "Do Not Feed the Ducks" warning sign



Figure 3.7.6 Winchester Park pond



Figure 3.7.7 Nesting Box

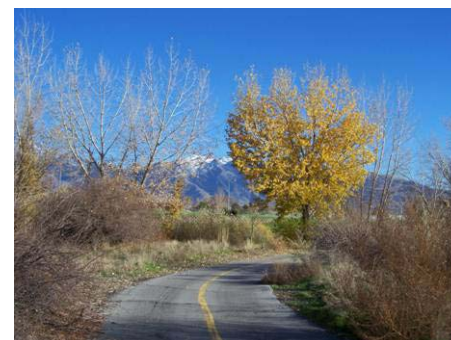


Figure 3.7.8 View looking east from trail



Figure 3.7.9 Wetlands north of Winchester Park



Figure 3.7.10 View from trail toward Murray Parkway Golf Course



Figure 3.7.11 Bridge B9
6' wide, 120' span, 16' clearance



Figure 3.7.12 View to south from Bridge B9

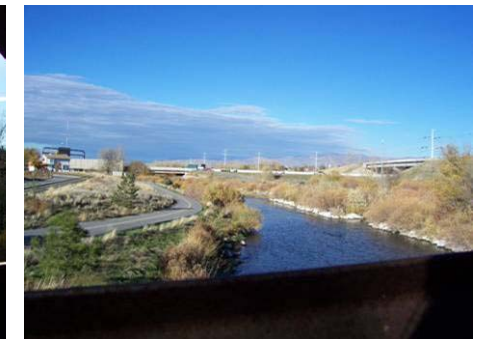


Figure 2.7.13 View to north from Bridge B9



Figure 3.7.14 View toward neighborhood access east of Bridge B9



Figure 3.17.15 View to south, near Interstate 215



Figure 3.7.16 Interstate 215 pedestrian underpass, looking north



Figure 3.7.17 In-line skater



Figure 3.7.18 Woods north of Interstate 215



Figure 3.7.19 Cottonwood Grove Park



Figure 3.7.25 Paved neighborhood access path



Figure 3.7.26 Trail looking north



Figure 3.7.32 View of playground from equestrian trail, west of Walden Park



Figure 3.7.33 5400 South vehicular crossing

Cottonwood Grove Park

- Open Lawn Area
- Trash Receptacles
- Parking (12 standard, 2 handicapped)



Figure 3.7.20 Cottonwood Grove Park



Figure 3.7.27 Paved neighborhood access path



Figure 3.7.28 Sand Volleyball at Walden Park



Figure 3.7.34 5400 South pedestrian and equestrian underpass



Figure 3.7.35 Germania Park Trailhead, north of 5400 South



Figure 3.7.21 View to north from Cottonwood Grove Park



Figure 3.7.22 At-grade crossing, north of Cottonwood Grove Park



Figure 3.7.29 Open lawn area with picnic tables at Walden Park



Figure 3.7.30 Bridge B10, 6' wide, 121' span, 15' clearance

Millrace Park

- Dog Park with small pavilions, benches, trash receptacles, and dog waste stations
- 2 Pavilions
- Playground
- Restrooms
- Baseball Diamonds
- Open Play Area
- Benches
- Picnic Tables
- Bike Rack
- Trash Receptacles
- Parking (98 standard, 7 handicapped)



Figure 3.7.36 Millrace Park



Figure 3.7.23 "Equestrian Trail Ends" sign near Cottonwood Grove Park



Figure 3.7.24 Equestrian trail running parallel to paved trail

Walden Park

- Restrooms
- Pavilion
- Picnic Tables
- Trash Receptacles
- Playground
- Sand Volleyball
- Benches
- Bike Rack
- Drinking Fountain
- Parking (35 standard, 2 handicapped)



Figure 3.7.31 Walden Park

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.7.37 Bench north of 5400 South



Figure 3.7.38 Bridge B11 8' wide, 132' span, 10' clearance



Figure 3.7.44 Picnic table along equestrian trail, west of Germania Park



Figure 3.7.45 Footbridge west of Germania Park



Figure 3.7.52 Informal unpaved trail that continues from the end of the equestrian trail, north of the Kennecott Environmental Center



Figure 3.7.53 Paved trail continuing north from 5400 South



Figure 3.7.39 Playground in Millrace Park, north of 5400 South

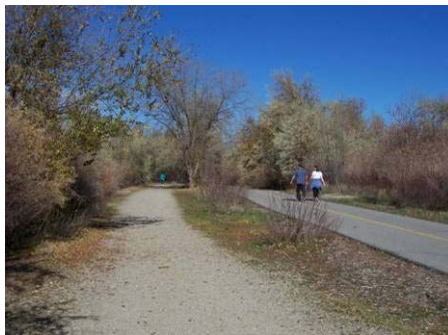


Figure 3.7.40 Equestrian trail running parallel to paved trail, north of 5400 South



Figure 3.7.46 Re-routed equestrian trail, washed out



Figure 3.7.47 Paved trail crosses equestrian trail near Bridge B12

Kennecott Environmental Center Trailhead

- Picnic Tables
- Drinking Fountain
- Bike Rack
- Trash Receptacles
- Parking (15 standard, 2 handicapped)




Figure 3.7.54 Kennecott Environmental Center Trailhead



Figure 3.7.41 Equestrian trail west of Germania Park



Figure 3.7.42 Washed out trail edge, west of Germania Park



Figure 3.7.48 Equestrian trail paralleling paved trail near Kennecott Environmental Center



Figure 3.7.49 Equestrian trail winding along river, near Kennecott Environmental Center



Figure 3.7.55 Trail continuing north



Figure 3.7.56 Interpretive signage near Germania Park

Germania Park Trailhead

- Open Play Area
- Picnic Tables
- Trash Receptacles
- Parking (64 standard, 4 handicapped, 10 equestrian)



Figure 3.7.43 Germania Park Trailhead



Figure 3.7.50 Bridge B12 8' wide, 122' span, 8' clearance



Figure 3.7.51 End of equestrian trail near Kennecott Environmental Center



Figure 3.7.57 Open lawn area near Germania Park



Figure 3.7.58 Footbridge near Germania Park



Figure 3.7.59 Play fields at Germania Park



Figure 3.7.60 Neighborhood trail access, north of Germania Park

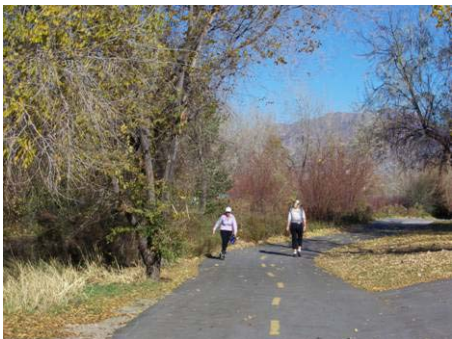


Figure 3.7.61 Trail near Kennecott Environmental Center



Figure 3.7.62 Kennecott Environmental Center Trailhead



Figure 3.7.63 Overlook east of Kennecott Environmental Center



Figure 3.7.64 Boardwalk through wetlands

Recommendations: Equestrian Trail

3.7 B Equestrian Trails

There are existing equestrian trails in this segment of the Jordan River Trail corridor from Bullion Street to approximately 4900 South. No new equestrian trails are proposed in this segment due to width limitations within the corridor.

Construction Costs

Table 3.7.1– Construction Costs: Paved Trail Segment 7		
Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.7 A	\$23,760
Policy	3.7 B	N/A

Issues and Opportunities

- There are no equestrian facilities in this section south of Bullion Street.
- The neighborhood access road west of Bridge B12 is not paved.

Recommendations: Paved Trail

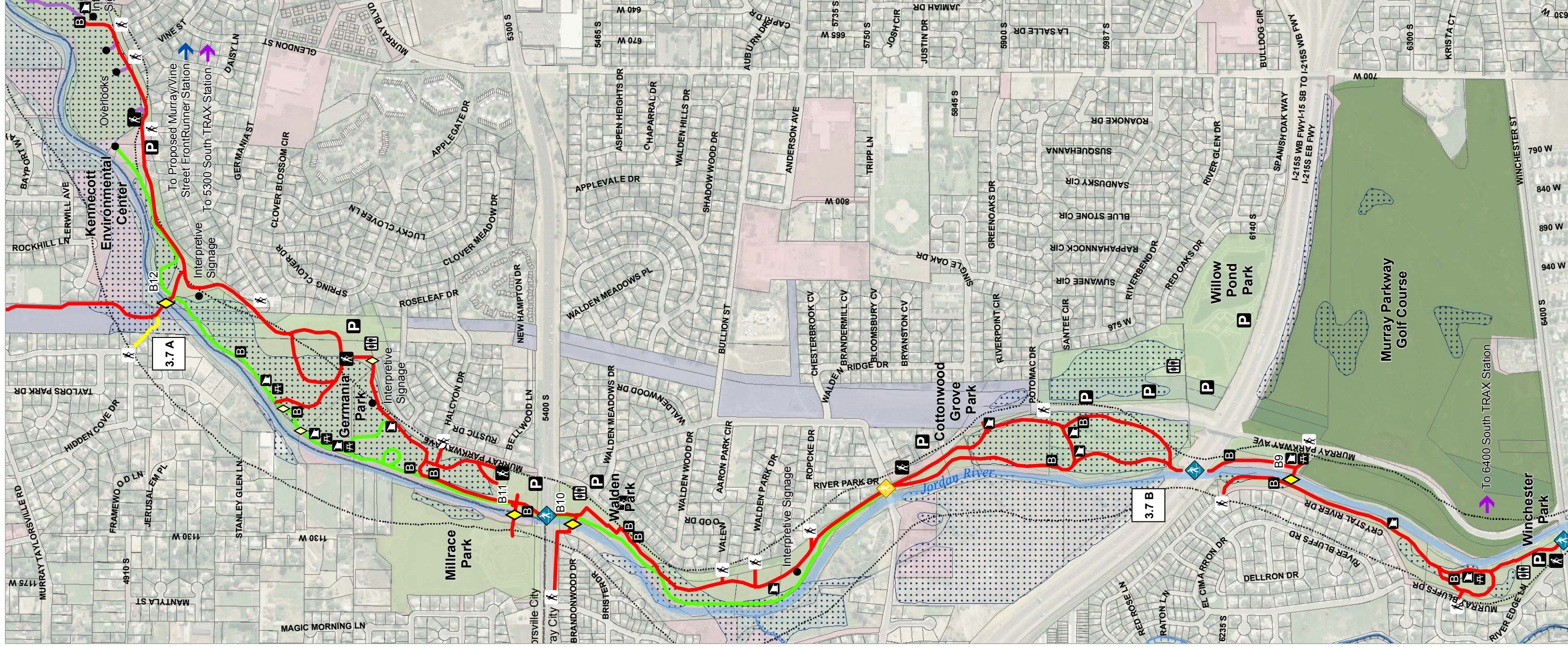
3.7 A Neighborhood Access Unpaved: Hidden Cove Drive

The unpaved trail from the trailhead at Hidden Cove Dr. in Taylorsville should be paved to connect to the Jordan River paved trail. This is a Murray City responsibility.

Jordan River Trail Master Plan

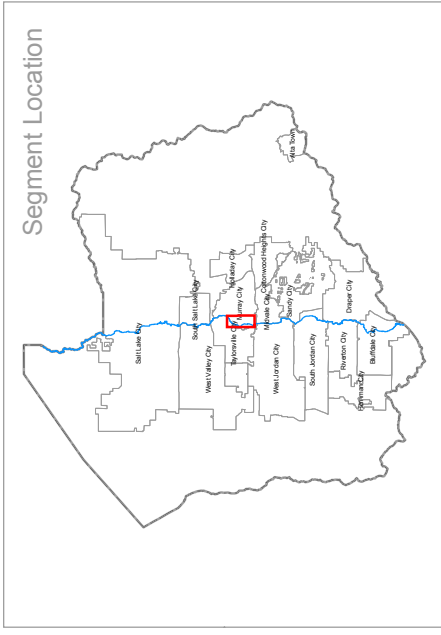
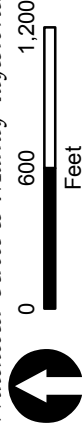
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 7: Murray/Taylorville

Winchester Street to Murray Taylorville Road



3.8 Segment 8: Murray/Taylorsville/West Valley/South Salt Lake (Murray Taylorsville Road (4800 South) to 3300 South)

The Jordan River paved trail continues north from the boardwalk through Arrowhead Park. The park is located on both the north and south sides of 4800 South, and a pedestrian underpass provides access to trail users. The trail crosses to the west bank of the Jordan River using Bridge B13. From here, the trail proceeds north, winding past a large apartment complex to the Taylorsville Freedom Shrine. The trail crosses the Taylorsville Expressway (4700 South) via a pedestrian underpass.

Following the course of the Jordan River, the trail curves to the northwest, bordering a large apartment complex near the Meadowbrook Golf Course. A couple of neighborhood trail access points connect to local neighborhoods. Just north of the Meadowbrook Golf Course, the trail crosses back to the east bank of the Jordan River with Bridge B14. The trail then crosses Meadow Brook Expressway (4100 South) via a new elevated pedestrian skywalk which connects trail users to General Holm Park on the north side of 4100 South. The path snakes through a large wooded natural space and passes a couple of benches on the way north to James Madison Park.



Figure 3.8.3 Arrow Park (South)



Figure 3.8.4 Arrow Park (South)

Taylorsville Freedom Shrine

- Veterans/War Memorial
- Benches
- Drinking Fountain
- Trash Receptacles



Figure 3.8.11 Taylorsville Freedom Shrine



Figure 3.8.5 4800 South underpass



Figure 3.8.6 Bridge B13
8' wide, 92' span, 14' clearance



Figure 3.8.12 Taylorsville Freedom Shrine



Figure 3.8.13 4700 South pedestrian underpass

Arrowhead Park (South of 4800 South)

- Picnic Tables
- Benches
- Interpretive Signage
- Restrooms
- Bike Racks
- Trash Receptacles
- Parking (18 standard, 2 handicapped)



Figure 3.8.1 Arrow Park (South)



Figure 3.8.7 View to south from Bridge B13

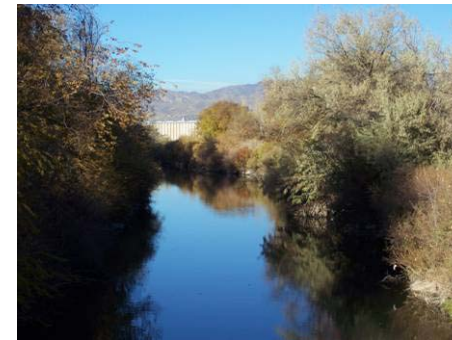


Figure 3.8.8 View to north from Bridge B13



Figure 3.8.14 Trail looking east toward pedestrian underpass



Figure 3.8.15 Trail north of 4700 South

Arrowhead Park (North of 4800 South)

- Picnic Tables
- Benches
- Trash Receptacles
- Parking (27 standard, 2 handicapped)

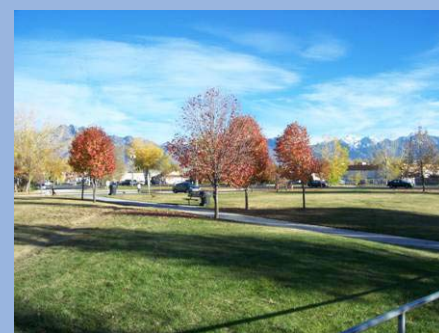


Figure 3.8.2 Arrow Park (North)



Figure 3.8.9 Trail north of Bridge B13, looking south



Figure 3.8.10 Trail looking north, near Taylorsville Freedom Shrine



Figure 3.8.16 Trail adjacent to apartment complex north of 4800 South

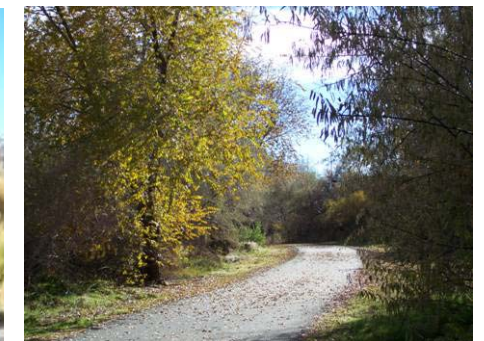


Figure 3.8.17 Trail east of Meadowbrook Golf Course



Figure 3.8.18 View of Jordan River from trail



Figure 3.8.19 Trail north of Meadowbrook Golf Course



Figure 3.8.25 Winding through woods north of General Holm Park



Figure 3.8.26 Bench along path north of General Holm Park



Figure 3.8.33 Trail looking north



Figure 3.8.34 Improvised seating



Figure 3.8.20 Bridge B14
8' wide, 105' span, 11' clearance



Figure 3.8.21 3900 South pedestrian sky bridge



Figure 3.8.27
Conservation Area sign



Figure 3.8.28 View from trail looking north



Figure 3.8.35 Bollard/chain gate south of James Madison Park

General Holm Park – M.G. Holm WWII Veterans Commemorative Trailhead

- Pavilion
- Restrooms
- Playground
- Interpretive Signage
- Picnic Tables
- Benches
- Trash Receptacles
- Parking (23 standard, 2 handicapped)



Figure 3.8.22 General Holm Park



Figure 3.8.29 View to west bank of Jordan River from trail



Figure 3.8.30 View to south from trail

Recommendations: Paved Trail

3.8 A Proposed Trails west of Jordan River in West Valley
Trails have been proposed in West Valley City north of Meadow Brook Expressway to 3300 South. The existing Jordan River Trail on the east side of the river should remain the official trail. If West Valley City wishes to develop a trail on the west side, it should be a West Valley City trail and not a Jordan River Trail.

For the safety and enjoyment of watercraft users on the river, there should be no bridges developed in this section. Users may use Meadow Brook Trail and 3300 South Street to access the Jordan River Trail corridor on the east side of the river in South Salt Lake City.

Recommendations: Equestrian Trail

3.8 C Equestrian Trails

There are no existing equestrian trails in this segment of the Jordan River Trail corridor, and no new equestrian trails are proposed due to width limitations within the corridor.



Figure 3.8.23 General Holm Park



Figure 3.8.24 General Holm Park Trailhead



Figure 3.8.31 Access path to UTA facility



Figure 3.8.32 Bench along trail

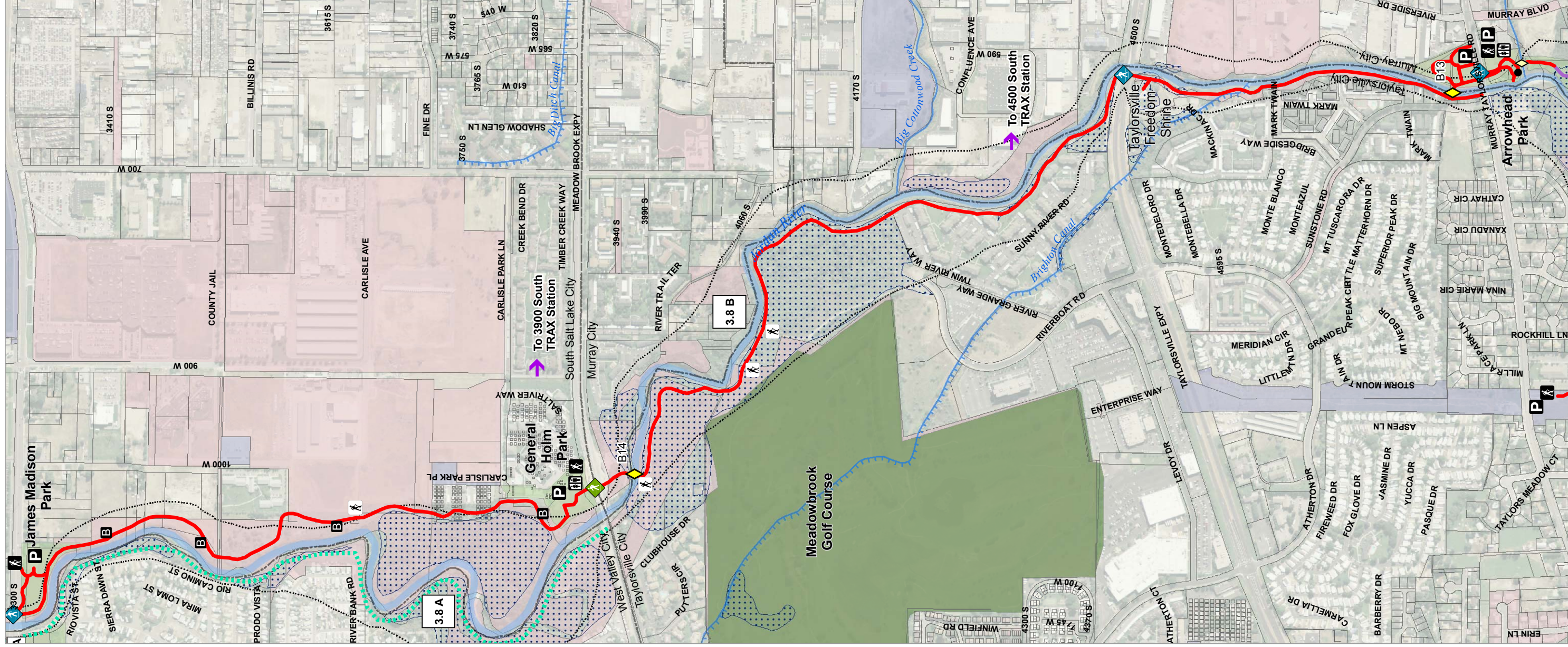
Construction Costs

Table 3.8.1– Construction Costs: Paved Trail Segment 8		
Feature Category	Map Code	CIP Budget Implication
Policy	3.8 A	N/A
Policy	3.8 B	N/A

Jordan River Trail Master Plan

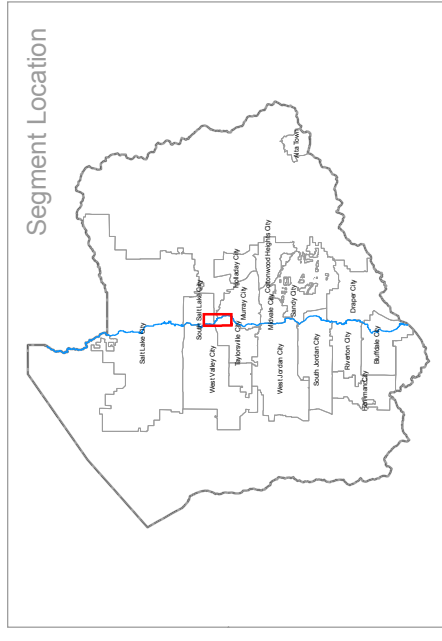
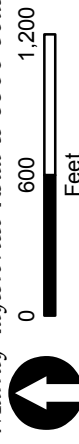
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 8: Murray/Taylorville/ West Valley/S. Salt Lake

Murray Taylorville Road to 3300 South



3.9 Segment 9: West Valley/South Salt Lake/Salt Lake City (3300 South to 1700 South)

The paved trail travels north through James Madison Park, on the south side of 3300 South, and continues under the street via a pedestrian underpass. The trail follows the large bend in the river, passing the Oxbow Jail and Utah Cultural Celebration Center, through large open spaces of vegetation. The Oxbow Trailhead, north of the jail, provides trail access and parking.

The Jordan River Trail curves back to the north, and forks just north of the Oxbow Trailhead near Bridge B15. The east fork continues north along the east bank of the Jordan River, crossing a footbridge and passing a light industrial area. The paved portion of this trail ends at Paul Workman Ball Park, near Bridge B16. A short unpaved trail continues north from here, stopping at SR-201.

The west fork crosses the Jordan River at Bridge B15 and travels north to the Redwood Nature Area. There is an informal gravel trail to an overlook north of the substation. The main trail loops through the Redwood Nature area, a large wooded area, with several benches scattered throughout. The south end of this loop takes users to the Redwood Multipurpose Center just off of Redwood Road and 3100 South. The north side of this loop takes users to a neighborhood trail access point just off of Castell Street. The trail continues north along the western bank of the Jordan River, past several small hobby farms, and through the Redwood Trailhead Park, where users can access Paul Workman Ball Park on the east side of the river using Bridge B16.

James Madison Park

- Restrooms
- Picnic Tables
- Trash Receptacles
- Open Play Area
- Parking (26 standard, 1 handicapped)



Figure 3.9.1 James Madison Park

The trail travels north from Redwood Trailhead Park through a small open space and passes under SR-201 and 2100 South using pedestrian underpasses. Glendale Golf Course is located just north of 2100 South on the west side of the Jordan River. The paved trail travels through the golf course for a short section, going around a dam and crossing a large canal using a footbridge. The trail takes users past a small peninsula of open meadow surrounded by a bend in the river, and then the trail crosses to the eastern bank of the river using Bridge B17. The trail is lighted north of this bridge throughout most of the Salt Lake City portion.

Continuing north past industrial areas, the trail crosses the railroad tracks with a special gated crossing which forces users to slow down and be aware as they cautiously move across the tracks and travel towards Raging Waters.



Figure 3.9.4 View towards James Madison Park

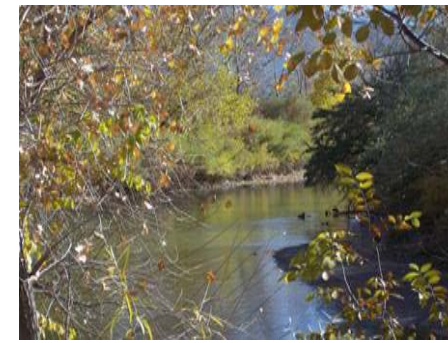


Figure 3.9.5 View to Jordan River from James Madison Park



Figure 3.9.6 Trail north of 3500 South underpass, looking south



Figure 3.9.7 Trail looking north, near Oxbow Jail

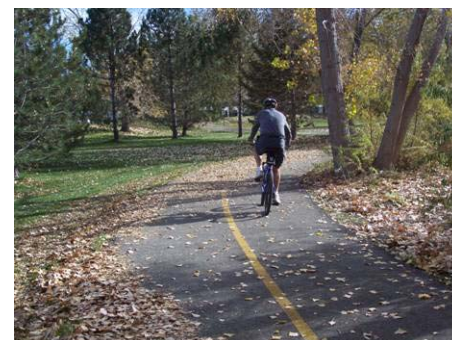


Figure 3.9.8 Cyclist near Oxbow Jail



Figure 3.9.9 View of Jordan River from trail



Figure 3.9.10 Trail through wooded area north of Oxbow Jail



Figure 3.9.11 Parking at Oxbow Trailhead

Oxbow Trailhead

- Trailhead Sign & Map
- Parking for approximately 4 vehicles (not striped)



Figure 3.9.12 Oxbow Trailhead



Figure 3.9.13 View to south from east side of Bridge B15



Figure 3.9.14 Bridge B15 10' wide, 120' span, 15' clearance



Figure 3.9.2 3500 South pedestrian underpass, looking north



Figure 3.9.3 James Madison Trailhead sign



Figure 3.9.15 East fork of trail north of Bridge B15



Figure 3.9.16 East fork of trail, looking north

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.9.17 Footbridge near 2900 South industrial area



Figure 3.9.18 View from footbridge towards dredging piles along the banks of the Jordan River



Figure 3.9.25 Ducks near Redwood Nature Area



Figure 3.9.26 Open space near Redwood Nature Area



Figure 3.9.33 "Restoration Area" sign near Lester Street Trailhead



Figure 3.9.34 Trail to Redwood Paved Center



Figure 3.9.19 Dredging piles along Jordan River near Redwood Nature Area



Figure 3.9.20 Hastings Cutoff historical marker along east fork of trail



Figure 3.9.27 Bench in Redwood Nature Area



Figure 3.9.28 Picnic table in Redwood Nature Area

Redwood Multipurpose Center/Lester Street Trailhead

- Community Center
- Baseball Diamonds
- Trailhead Sign & Map
- Roadside Parking



Figure 3.9.35 Redwood Multipurpose Center/Lester Street Trailhead



Figure 3.9.21 View to northwest, near Redwood Nature Area



Figure 3.9.22 View to north from Bridge B15



Figure 3.9.29 Open meadow in Redwood Natural Area



Figure 3.9.30 Nesting box near Power Substation



Figure 3.9.36 Trail through west end of Redwood Nature Area



Figure 3.9.37 Canal running through Redwood Nature Area



Figure 3.9.23 Bench near Bridge B15



Figure 3.9.24 Trail looking north, near Bridge B15



Figure 3.9.31 Oxbow Trailhead sign



Figure 3.9.32 Trail Crossing west of Power Substation



Figure 3.9.38 Drop-down bridge crossing canal in Redwood Nature Area



Figure 3.9.39 View towards Castell Street trail access point



Figure 3.9.40 Dredging piles and garbage east of Redwood Nature Area



Figure 3.9.41 Llama at hobby farm north of Redwood Nature Area, along trail



Figure 3.9.47 View of boat ramps at Redwood Trailhead Park



Figure 3.9.48 Bridge B16, 10' wide, 165' span, 11' clearance



Figure 3.9.54 Pedestrian underpass, 2100 South, looking north



Figure 3.9.55 Maintenance access footbridge north of 2100 south, no public access



Figure 3.9.42 Horse property along trail north of Redwood Nature Center



Figure 3.9.43 Neighborhood trail access north of Redwood Nature Area

Paul Workman Ball Park

- Ball Diamond
- Soccer Field
- Restrooms
- Concession Building
- Drinking Fountain
- Bike Rack
- Trash Receptacles
- Parking (20 standard, 1 handicapped)



Figure 3.9.49 Paul Workman Ball Park



Figure 3.9.56 Diversion Dam near Glendale Golf Course



Figure 3.9.57 Footbridge through Glendale Golf Course

Redwood Trailhead Park

- Pavilion
- Playground
- Picnic Tables
- Restrooms
- Drinking Fountain
- Trash Receptacles
- Parking (35 standard, 1 handicapped)



Figure 3.9.44 Redwood Trailhead Park



Figure 3.9.50 Redwood Trailhead Park trailhead sign



Figure 3.9.51 Trail north of Redwood Trailhead Park, looking north



Figure 3.9.58 View from footbridge toward diversion dam and 2100 South overpass



Figure 3.9.59 Bridge B17, 10' wide, 100' span, 8' clearance



Figure 3.9.45 Trail near Redwood Trailhead Park, looking south



Figure 3.9.46 Redwood Trailhead Park



Figure 3.9.52 Trail near SR 201, looking east



Figure 3.9.53 Pedestrian underpass SR 201



Figure 3.9.60 Trail north of diversion dam, looking northeast



Figure 3.9.61 Open meadow in horseshoe bend north of 2100 South



Figure 3.9.62 Open meadow in horseshoe bend



Figure 3.9.63 Trail north of horseshoe bend



Figure 3.9.64 Gated railroad crossing for bikes and pedestrians



Figure 3.9.65 Trail south of Raging Waters, looking south

Issues and Opportunities

- Large dredge piles east of the Redwood Nature Area have been deposited on both banks of the Jordan River. These areas are collection grounds for garbage and weeds, and create an unattractive environment for trail users.
- Thickly vegetated areas near the Redwood Nature Center and Oxbow Jail are common locations for crime and other unacceptable behaviors.
- The trail pavement has been broken up for several years in this segment from root growth and has not been repaired.
- Trailheads at Paul Workman Ballpark and Redwood Trailhead Park will serve both Jordan River Trail users and Parley's Trail users in the future, and need to accommodate those needs in the future as they arise.

Recommendations: Paved Trail

3.9 A Flooding at 3300 South Underpass

These areas are subject to periodic flooding during period of peak runoff and high water. When flooding occurs, the flooded trail segments shall be closed, and will be signed according to the signage program discussed in Chapter 5.

In the future, as vehicular crossings of the Jordan River undergo reconstruction or modifications, responsible agencies should investigate engineering solutions to correct trail flooding problems.

3.9 B Proposed Trails in West Valley

Trails have been proposed in West Valley City between 3300 South and Bridge B15 at approximately 3100 South. The existing Jordan River Trail on the east side of the river will remain the official Jordan River Trail, and a portion of trail on the west side of the river from the bridge south to the property line for the Utah Cultural Celebration Center should be paved. West Valley City intends to develop a trail on the west side through the Utah Cultural Celebration Center.

For the safety and enjoyment of watercraft users on the river, there should be no additional bridges developed in this section. Users may use 3300 South Street to access the Jordan River Trail corridor on the east side of the river in South Salt Lake City.

3.9 C Maintenance Issues

The section of trail in South Salt Lake City between 2900 South and Paul Workman Park has dredge materials, weeds, and debris that should be cleaned-up.

Thickly vegetated areas near Redwood Nature Center and Oxbow Jail need to be controlled as this is an area where crime and unacceptable behaviors are common.

The trail pavement has been broken up and should be repaired.

Recommendations: Equestrian Trail

3.9 E Equestrian Trails

There are no existing equestrian trails in this segment of the Jordan River Trail, and no new equestrian trails are proposed due to width limitations within the corridor. It is undesirable to create new segments of equestrian trail that do not have the potential to be linked to other equestrian segments in the future.

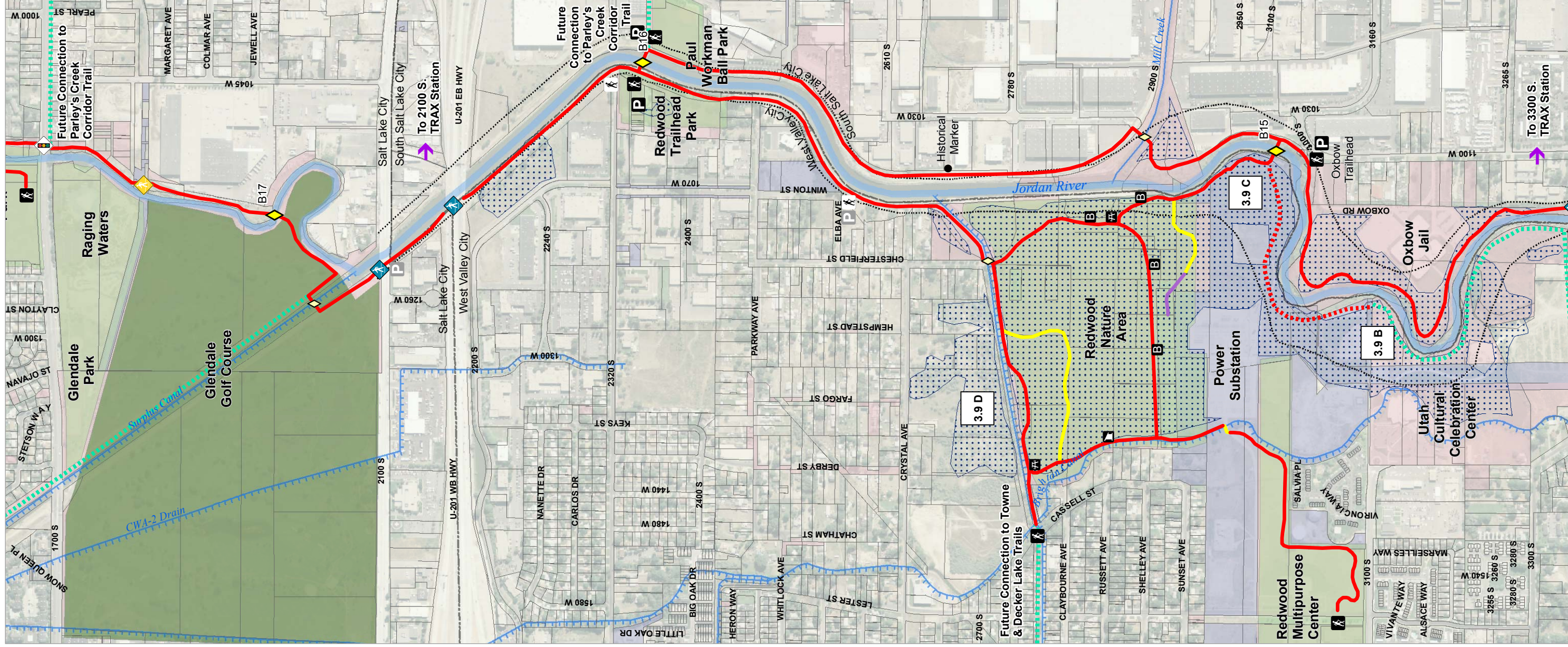
Construction Costs

Feature Category	Map Code	CIP Budget Implication
Signage	3.9 A	\$1,000
Policy	3.9 B	N/A
Maintenance	3.9 C	\$15,000
Policy	3.9 D	N/A

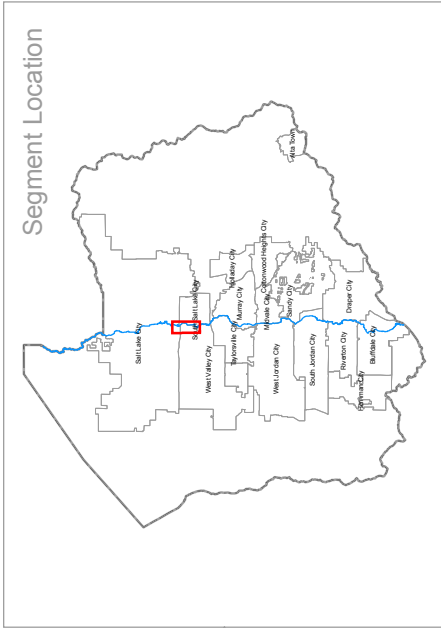
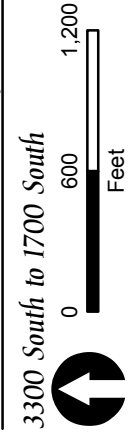
Jordan River Trail Master Plan

Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
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 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 9: West Valley/South Salt Lake/Salt Lake City



3.10 Segment 10: Salt Lake City (1700 South to Interstate 80)

The Jordan River Trail continues north towards Raging Waters. The Canoe Marina donated by the Exchange Club is located along the eastern edge of the Raging Waters parking lot. The trail then crosses 1700 South via a signalized crosswalk to Glendale Park. One trail segment begins in the parking lot and traces the west bank of the Jordan River along the east end of the park, and curves to the west as the river bends. This portion of the trail ends at a trail access point along 1200 West.

The main trail segment follows the eastern bank of the river, passing an informal neighborhood park featuring the “River of Words”

Peace Labyrinth and an informal pathway trail. This pathway trail also accesses the neighborhood on 1200 West via Bridge B18 which is not ADA accessible, along unpaved pathways. The trail continues north through residential neighborhoods, and crosses to the west side of the river just south of California Avenue at Bridge B19. The trail then crosses California Avenue with a signalized crosswalk, passes through a small neighborhood park/trailhead, and winds back to the east.

The trail then curves back around to the north and passes through a small neighborhood park. As the river bends to the west, one segment of the trail continues along the south side of the river, heading west through Modesto Nature Park/Trailhead, then follows the river as it



Figure 3.10.1 Canoe Marina donated by the Exchange Club, Raging Waters parking lot



Figure 3.10.2 Trail east of Canoe Marina, looking south

turns north again. This trail segment ends at the sidewalk on Fremont Avenue. The main trail segment crosses the river in the neighborhood park at Bridge B20, and follows the Jordan River through the “Bend in the River Park”.



Figure 3.10.4 Glendale Park

The Jordan River Trail continues across Fremont Drive on the east side of the river with a non-signalized crosswalk where warning flashers will be installed soon, and travels north along the west edge of Jordan Park and the International Peace Gardens.

Neighbors can access the trail at Brooklyn Avenue and travel north along the trail, which crosses back to the east bank of the river at Bridge B22, or they can cross Bridge B21 to access the trail along the east bank. The trail traces the west edge of the park and curves along its northern end. The trail then ends near Montague Avenue where a utility building and raised railroad tracks present an unsafe crossing for trail users. The trail picks up again north of the railroad tracks on 900 South.

The trail passes through the Ninth South Park along the eastern bank of the river, where neighbors can access the trail using Bridge B22. Continuing north, the trail crosses 800 South using a signalized crosswalk and switches to the west bank of the river. From here, the trail continues north for a short distance where it crosses the river again at Bridge B23, then crosses 700 South using a crosswalk. The trail can be accessed from 600 South via Bridge B24 and from a small neighborhood park along Jake Gam Boulevard. The trail then crosses 500 South using an un-signalized crosswalk, passing through a small “Olympic” themed mini park, and travels north crossing 400 South (signalized) and 300 South (crossing flags) with crosswalks. The trail then winds to the west, following the curves of the Jordan River, through the small



Figure 3.10.6 Neighborhood Trail Access at Bridge B18



Figure 3.10.7 Non-accessible path to Bridge B18



Figure 3.10.5 “Salt Lake City Gets Fit Together” sign



Figure 3.10.8 Bridge B18 4' wide, 69' span, 10' clearance



Figure 3.10.9 River looking north from 1700 South

Alzheimer’s Association Wildlife Grove, and ends at Interstate 80.



Figure 3.10.11 View from trail looking west toward Glendale Park



Figure 3.10.11 Trail west of Glendale Park, near Riverside Drive



Figure 3.10.12 Utility Structure near informal Labyrinth path



Figure 3.10.13 “River of Words” Peace Labyrinth



Figure 3.10.14 Mosaic benches surround Labyrinth



Figure 3.10.15 Trail north of Labyrinth

Glendale Park

- Restrooms
- Soccer Fields
- Small Pavilions
- Picnic Tables
- Trash Receptacles
- Parking (28 standard, 1 handicapped)



Figure 3.10.3 Glendale Park



Figure 3.10.16 River north of Labyrinth



Figure 3.10.17 Bench near Bridge B19



Figure 3.10.23 View of river and trail along 1300 South, looking east



Figure 3.10.24 View of trail along 1300 South near 1000 West



Figure 3.10.29 Art wall and seating in Neighborhood Park



Figure 3.10.30 Overhead structure in Neighborhood Park



Figure 3.10.18 Bridge B19
10' wide, 100' span, 11' clearance



Figure 3.10.19 California Avenue overpass



Figure 3.10.25 Trail along 1300 south where river bends to the north



Figure 3.10.26 View from trail at 1300 South, looking east

Bend in the River Park

- Urban Tree House and Green Space
- Benches
- Drinking Fountain
- Boardwalk
- Sculpture
- Parking (9 standard, 1 handicapped)



Figure 3.10.32 Bend in the River Park

Neighborhood Park/Trailhead (North of California Ave.)

- Benches
- Picnic Table
- Bike Rack
- Drinking Fountain
- Information Kiosk
- Parking (4 standard, 1 handicapped)



Figure 3.10.20 Neighborhood Park

Modesto Nature Park/Trailhead

- Boat Launch
- Playground
- Picnic Tables
- Benches
- Bike Rack
- Canvas Awning
- Marshlands
- Informal Pathways
- Open Play Area
- Drinking Fountain
- Flag Pole



Figure 3.10.27 Neighborhood Park



Figure 3.10.28 Boat ramp in Neighborhood Park



Figure 3.10.32 Bridge B20
10' wide, 100' span, 6' clearance



Figure 3.10.33 Urban Tree House, north of Bridge B20



Figure 3.10.21 Pedestrian traffic signal at California Avenue



Figure 3.10.22 Pedestrians near neighborhood park

Jordan Park/International Peace Gardens

- Baseball Diamonds
- Horseshoe Pits
- Pavilions
- Tennis Courts
- Playgrounds
- Skate Park
- Sand Volleyball
- Picnic Tables
- Restrooms
- Trash Receptacles



Figure 3.10.34 Jordan Park

JORDAN RIVER TRAIL MASTER PLAN



Figure 3.10.35 Jordan River Trail north of Fremont Ave, in Jordan Park



Figure 3.10.36 Trail along western edge of International Peace Gardens



Figure 3.10.43 Non-accessible railroad crossing south of 900 South



Figure 3.10.44 Trail starting point dead-ends at railroad tracks, south of 900 South



Figure 3.10.50 Bridge B23 10' wide, 100' span, 11' clearance



Figure 3.10.51 Trail north of 700 South



Figure 3.10.37 Bridge B21 10' wide, 100' span, 6' clearance



Figure 3.10.38 Pier west of International Peace Gardens

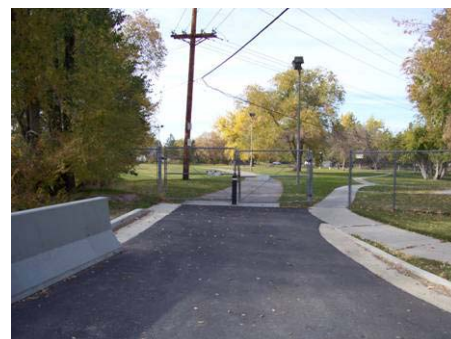


Figure 3.10.45 Trail access point, 900 South looking north



Figure 3.10.46 Trail north of 800 South



Figure 3.10.52 Small linear park near Jake Garn Boulevard



Figure 3.10.53 Bridge B24 7' wide, 77' span, 6' clearance



Figure 3.10.39 Pathway to Jordan River Trail from Peace Gardens

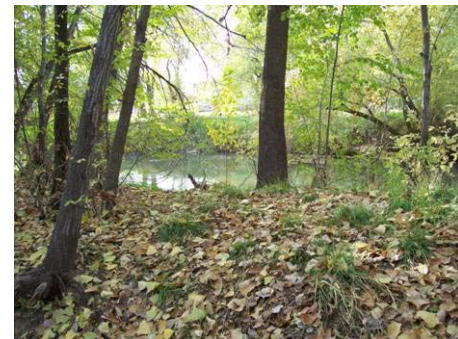


Figure 3.10.40 View to river from trail

- Ninth South Park**
- Picnic Tables
 - Restrooms
 - Trash Receptacles
 - Parking (18 standard, 2 handicapped)



Figure 3.10.47 Ninth South Park



Figure 3.10.54 View of river near Jake Garn Boulevard



Figure 3.10.55 Trail looking north towards 500 South crossings



Figure 3.10.41 View to railroad crossing north of Peace Gardens



Figure 3.10.42 Improvised footpath north of Peace Gardens



Figure 3.10.48 Bridge B22 10' wide, 70' span, 10' clearance

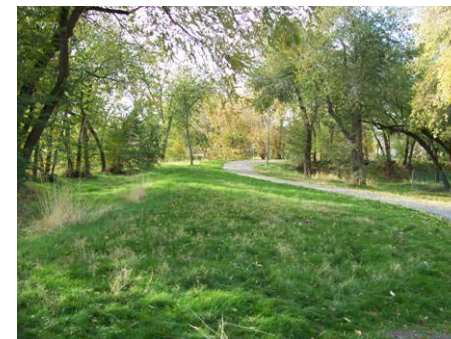


Figure 3.10.49 Trail south of 700 South



Figure 3.10.56 "Trail Crossing" sign along 500 South

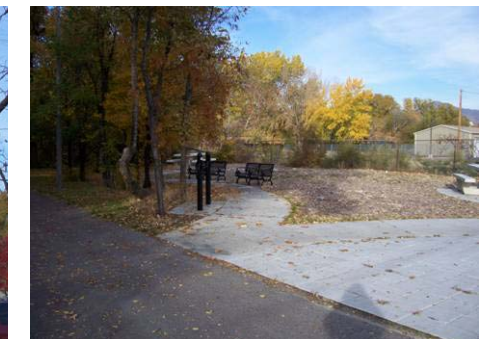


Figure 3.10.57 Olympic Legacy Park, north of 500 South

Olympic Legacy Park (500 South)

- Benches
- Picnic Table
- Bike Rack
- "Prometheus Fire Bearing" Sculpture
- Olympic Themed Site Furniture



Figure 3.10.58 500 South Olympic Park

Alzheimer's Association Wildlife Grove

- Benches
- Picnic Table
- Trash Receptacle



Figure 3.10.65 Alzheimer's Association Wildlife Grove

- The paved trail ends just south of Interstate 80, with a short portion of unpaved trail continuing to the Interstate 80 overpass and does not resume until trail access is provided at North Temple Street in Segment 11.

Recommendations: Paved Trail

3.10 A Unpaved Trail: "River of Words" Peace Labyrinth

The northern unpaved trail at the "River of Words" Peace Labyrinth should be considered part of the Jordan River Trail corridor. The trail should be paved in this area as part of the request by the surrounding community for the park to be upgraded to an Urban Fishery. The southern portion of the trail should remain unpaved.

The neighborhood access from Bridge B28 should be paved from the park all the way to the sidewalk on 1200 West, making it ADA accessible and a connection should be made to the paved trail that traces the northern boundary of Glendale Park.

3.10 B Paved trail Gap East Bank: 900 South Railroad Tracks

There is a short gap in the east bank trail as it crosses 900 South Street and the railroad. The railroad track along 900 South is being abandoned and will be removed in 2008. The abandoned right-of-way will become the property of Salt Lake City. The trail should be connected across 900 South to connect Jordan River trails in Jordan Park to Ninth South Park.

A trail access point should be developed at this point with signage, which would also serve as a trail corridor connection to a future 900 South trail heading east to Liberty Park developed by Salt Lake City.

For the safety and enjoyment of watercraft users on the river, there should be no additional bridges developed in this section.

The trail needs signage directing users to continue west across Bridge B21.

3.10 C Unpaved trail: Just South of I-80

The unpaved trail just south of I-80 should be paved.

3.10 D Interstate 80 Underpass

Salt Lake City has secured funding to develop a pedestrian underpass at Interstate 80 that will take users under the freeway, although the paved trail currently ends just south of Interstate 80 and does not resume until North Temple. The funding is only for the underpass design and construction, not to continue the trail to North Temple.

Recommendations: Equestrian Trail

3.10 E Equestrian Trails

There are no existing equestrian trails in this segment of the Jordan River Trail, and no new equestrian trails are proposed due to width limitations within the corridor.



Figure 3.10.59 Trail north of Olympic Legacy Park



Figure 3.10.60 Trail looking north toward 400 South crossing



Figure 3.10.66 View of trail, north of 300 South



Figure 3.10.67 Monument sign in Alzheimer's Association Wildlife Grove



Figure 3.10.61 400 South overpass



Figure 3.10.62 "Jordan River Parkway" sign north of 400 South



Figure 3.10.68 Trail south of Interstate 80



Figure 3.10.69 View under Interstate 80 overpass



Figure 3.10.63 Trail north of 400 South, looking north



Figure 3.10.64 300 South crossing

Issues and Opportunities

- The area near the "River of Words" Peace Labyrinth is largely undeveloped, and the community is pushing for it to be developed as an Urban Fishery.
- There is no paved trail on either end of Bridge B18, and narrow paths with sizeable grade changes make this bridge inaccessible.
- There is a lack of proper signage directing trail users to the main trail across bridge B21 on the west bank of the river. The east bank trail stops at the north end of Jordan Park/International Peace Gardens near a utility building and the railroad tracks make this portion of the trail inaccessible. There is no pedestrian rail barrier at this location.

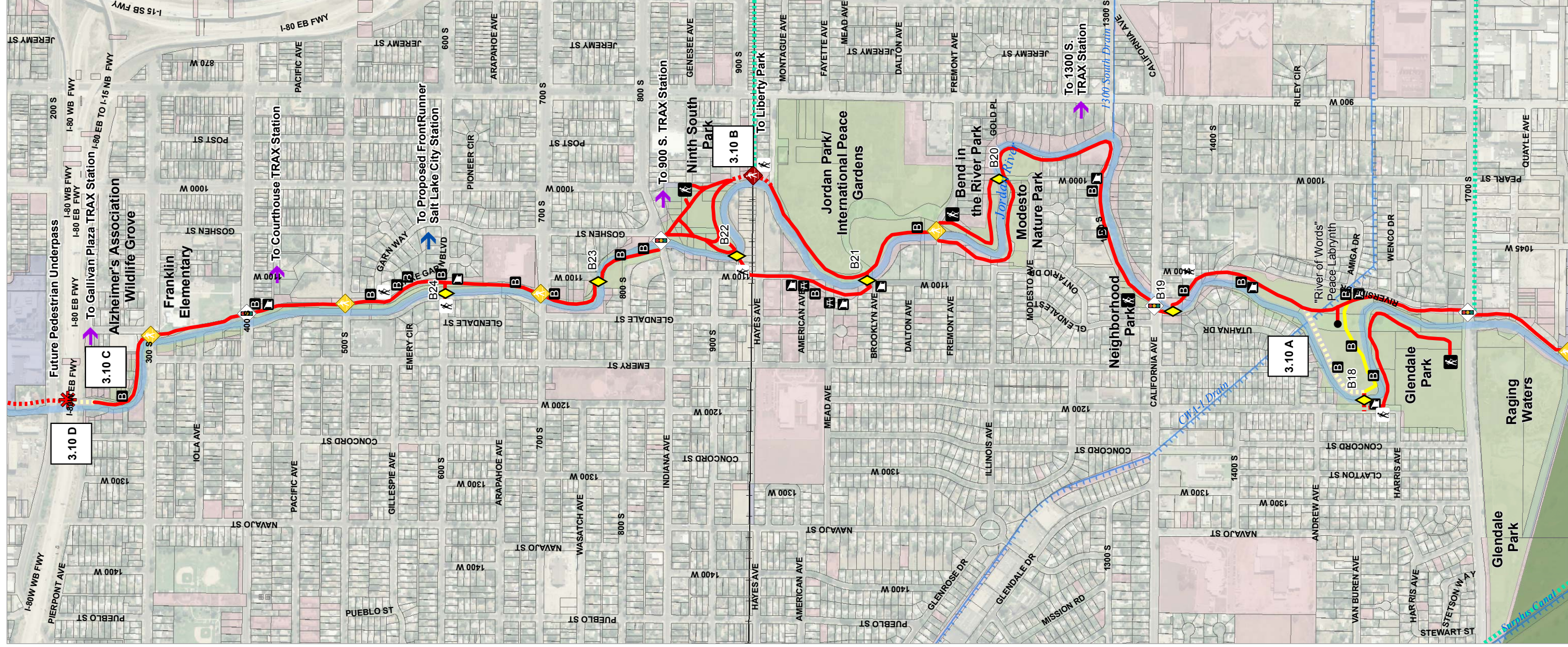
Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.10 A	\$47,520
Paved Trail/Trail Access	3.10 B	\$25,760
Paved Trail	3.10 C	\$23,760
Underpass	3.10 D	Funded
Policy	3.10 E	N/A

Jordan River Trail Master Plan

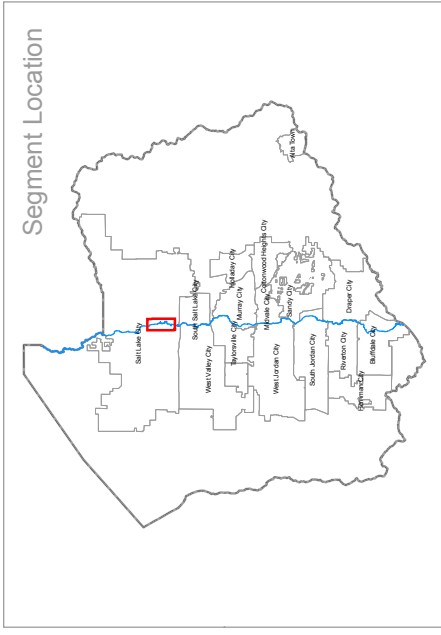
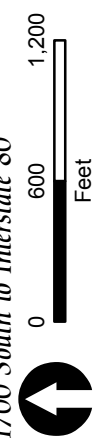
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 10: Salt Lake City

1700 South to Interstate 80



3.11 Segment 11: Salt Lake City (Interstate 80 to DuPont Avenue)

There is a gap in the Jordan River Trail corridor between Interstate 80 and North Temple Street. The trail resumes just west of the Utah State Fair Park at a trail access point on the north side of North Temple Street. The trail follows along the eastern bank of the Jordan River for a short distance, before crossing at Bridge B25 to the western bank of the river. The trail continues north for a small segment where it splits. An equestrian trail continues north along the western edge of the river, passing Bridge B27, ending in Cottonwood Park.

The paved trail once again crosses the river at Bridge B26, back to the eastern bank of the Jordan River, bypassing the private Utah State Fair Park pathways. This segment curves along the southwestern edge of Constitution Park, which provides trail access, through linear open space past Bridge B27, and winds through a small portion of Cottonwood Park located on the north side of the Jordan River. The trail connects to the main area of Cottonwood Park using Bridge B28. The Jordan River Trail continues north past the Cottonwood Park area, across 500 North with an un-signalized crosswalk. A pedestrian warning flasher will soon be installed at this location.

The trail continues north past a bench and trash receptacle on the north side of 500 North to Bridge B29, where the trail splits. The paved trail crosses Bridge B29 to the eastern bank of the Jordan

River, traveling past Backman Elementary School where trail access is provided, and crosses 700 North with a signalized crosswalk. The trail continues north, tracing the western edge of Riverside Park, Past Bridge B30 which crosses the river and provides access to the equestrian trail on the opposite bank. The trail then follows the river as it bends eastward through a large open space near Cornell Street, across the river from the Day Riverside Library.

The equestrian trail resumes at Bridge B29 and follows the western bank of the Jordan River, crossing 700 North at a signalized intersection, and continues north, winding behind the Day Riverside Library. The equestrian trail crosses 1000 North at a signalized crosswalk, where the trail then switches to the eastern bank of the Jordan River, paralleling the paved trail. There is a sign posted on the

western bank on a chain link gate stating that this portion of the Jordan River Trail is temporarily closed.

The trails then travel north, across the river from the Jordan River Golf Course. There is a short portion of unpaved trail that runs along the western bank of the river, from the north edge of the Jordan River Golf Course to the Rose Park Golf Course, but does not connect to 1000 North.

Cottonwood Park

- Amphitheatre
- Large Pavilion
- Small Pavilions
- Playground
- Basketball Court
- Sand Volleyball
- Baseball Diamond
- Trash Receptacles
- Parking (Street-side)



Figure 3.11.9 Cottonwood Park



Figure 3.11.1 Trail access point on North Temple near Utah State Fair Park



Figure 3.11.2 Trail and trash receptacle north of North Temple



Figure 3.11.3 Bridge B25
6' wide, 75' span, 9' clearance



Figure 3.11.4 View from Bridge B25 towards Utah State Fair Park



Figure 3.11.10 Bridge B27
4' wide, 70' span, 8' clearance

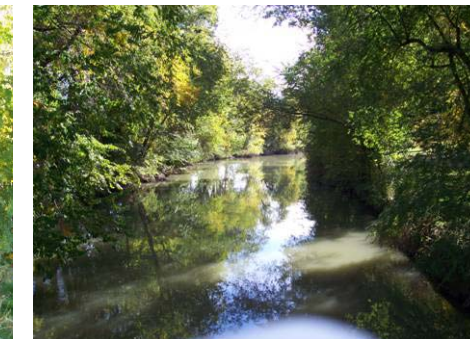


Figure 3.11.11 View from Bridge B27, looking south



Figure 3.11.5 Trail along west bank of Jordan River, west of Utah State Fair Park



Figure 3.11.6 "Utah State Fairpark/Jordan River Parkway" wall

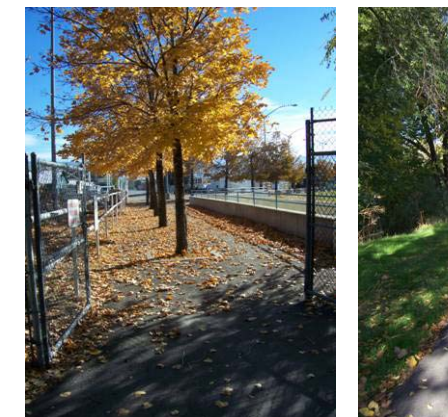


Figure 3.11.12 View to south, looking toward Utah State Fair Park trail



Figure 3.11.13 View to north, east of Bridge B26



Figure 3.11.7 View towards unpaved trail west of Bridge B26



Figure 3.11.8 Bridge B26
8' wide, 77' span, 9' clearance

Constitution Park

- Northwest Salt Lake Community Center/ Senior Citizens Center
- Small Pavilions
- Tennis Courts
- Playgrounds
- Baseball Diamonds
- Picnic Tables
- Soccer Fields
- Trash Receptacles
- Parking (113 standard, 8 handicapped, 2 reserved, 1 bus only)



Figure 3.11.13 Constitution Park



Figure 3.11.20 View from unpaved trail west of Constitution Park



Figure 3.11.21 Unpaved trail near Cottonwood Park



Figure 3.11.28 Trail north of 500 North, looking south



Figure 3.11.29 Unpaved trail west of Backman School



Figure 3.11.14 Constitution Park, looking north



Figure 3.11.15 Trail access from Constitution Park



Figure 3.11.22 Street-side parking for Cottonwood Park



Figure 3.11.23 Trail through Cottonwood Park



Figure 3.11.30 Bridge B29 4' wide, 69' span, 6' clearance



Figure 3.11.31 View across Jordan River to Backman School, looking east



Figure 3.11.16 Paved trail north of Constitution Park



Figure 3.11.17 View of Cottonwood Park, north of Jordan River



Figure 3.11.24 Rest area west of Cottonwood Park



Figure 3.11.25 Bridge B28 8' wide, 70' span, 8' clearance



Figure 3.11.32 700 North overpass



Figure 3.11.33 Unpaved paved trail west of Riverside Park



Figure 3.11.18 View of river from Bridge B28



Figure 3.11.19 unpaved trail north of Bridge B26



Figure 3.11.26 Trail north of Cottonwood Park, looking north



Figure 3.11.27 Crosswalk at 500 North

Riverside Park

- Soccer Fields
- Baseball Diamonds
- Picnic Tables
- Horse Shoe Pits
- Tennis Courts
- Basketball Courts
- Restrooms
- Playgrounds
- Pavilions
- Trash Receptacles
- Parking (87 standard, 4 handicapped)



Figure 3.11.34 Riverside Park



Figure 3.11.35 View across Jordan River to Riverside Park



Figure 3.11.36 View from equestrian trail toward Day Riverside Library



Figure 3.11.37 Bridge B30
4' wide, 70' span, 6' clearance



Figure 3.11.38 "Trail Open on East Side of River" sign on east side of 1000 North

Mini Park

- Picnic Tables
- Open Play Areas



Figure 3.11.39 Mini Park



Figure 3.11.40 Unpaved trail north of 1000 North



Figure 3.11.41 Trail east of Jordan River Golf Course

Issues and Opportunities

- There is a gap in the paved trail between Interstate 80 and North Temple Street.
- There is no barrier provided where the trail intersects with the rail lines in this gap.
- No formal trailhead facilities exist on North Temple Street.
- Signs indicate that equestrian trail access at 1000 North along the west side of the river is temporarily closed off. The greens at the Jordan River Golf Course go up to the edge of the river bank, indicating that this closure may be permanent. A small portion of this trail can be accessed from Bridge B31 in the Rose Park Golf Course, but continues south only to northern boundary of the Jordan River Golf Course.

Recommendations: Paved Trail

3.11 A Paved trail Gap: Just south of I-80

The small gap just south of I-80 should be paved. The negotiations for rights-of-way through the land north of I-80 are nearing completion.

3.11 B Underpass: I-80

Salt Lake City is planning to extend the trail under I-80 with a pedestrian underpass.

3.11 C Paved Trail Gap: I-80 to North Temple

Salt Lake City is completing negotiations to secure rights-of-way through the properties north of I-80 to North Temple along the Jordan River. The trail will need to cross rail lines crossing the river and trail corridor. The rail lines are currently being consolidated into a single track. An at-grade trail crossing for the double track is in negotiations with the railroad company, which will include a pedestrian-operated barrier. The area is also designed to include flood control and major drainage structures that will include a connecting trail into the Gateway area of Salt Lake City via the City Creek Trail.

3.11 D Trailhead: North Temple

Salt Lake City is teaming up with UDOT to develop a trailhead on the south side of North Temple, on the west bank of the Jordan River.

3.11 E Trailhead: South Side of 1000 North, East of River

A trailhead should be developed at 1000 North. It should serve both urban water trail users and surface trail users. (See Section 4.4 for more information on urban water trail facilities).

3.11 F Signage

Signage indicating that the west side of the Jordan River Trail is temporarily closed between 1000 North and Bridge B31 in Rose Park Golf Course should be replaced with signage indicating this area is for maintenance access only. The section of trail from Bridge B31 south to the northern boundaries of the Jordan River Golf Course should be signed as maintenance access only.

Recommendations: Equestrian Trail

3.11 G Equestrian Trails

No new equestrian trails are proposed due to width limitations within the corridor.

Construction Costs

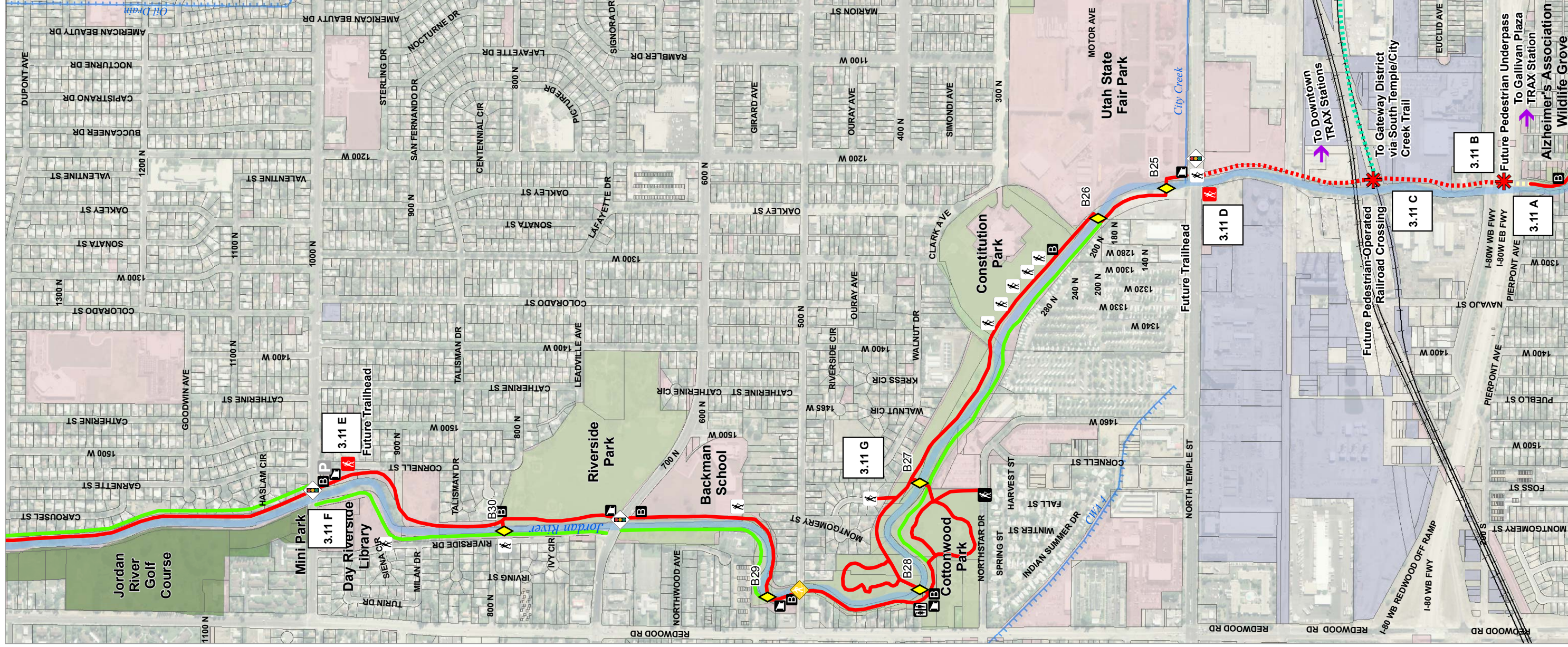
Table 3.11.1– Construction Costs: Paved Trail Segment 11

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.11 A	\$23,760
Underpass	3.11 B	\$300,000
Trail & Railroad Crossing	3.11 C	\$10,000,000
Trailhead	3.11 D	Funded
Trailhead	3.11 E	\$185,000
Signage	3.11 F	\$2,000
Policy	3.11 G	N/A

Jordan River Trail Master Plan

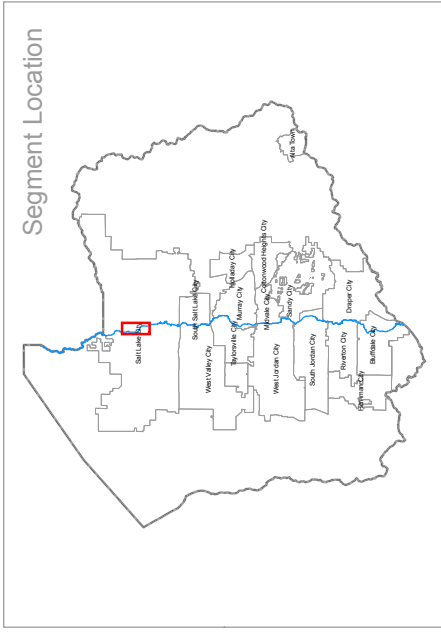
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
- Trail Facilities**
- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Unpaved Jordan River Trail
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
 - City Existing Trail
 - City Proposed Trail
 - Trails to be Abandoned
- Public Transit**
- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment II: Salt Lake City

Interstate 80 to DuPont Avenue



June 2008



3.12 Segment 12: Salt Lake City/Salt Lake County (DuPont Avenue to Interstate 215)

The paved and equestrian trails continue north along the eastern bank of the Jordan River. A rest area is provided at a trail access point on Sunset Drive. The trails then enter the Rose Park Golf Course, where they cross Bridge B31 to the west bank of the river. A small portion of unpaved trail heads south from this point, but stops when it reaches the boundary of the Jordan River Golf Course. From Bridge B31, the an unpaved trail and the equestrian trail continue north, winding through the center of the Rose Park Golf Course, following the Jordan River. The river and trails curve to the west as they approach the north end of the golf course. There is a small rest area and a boat launch just east of Redwood Road. There is no crosswalk, bridge, or underpass provided to get trail users safely across Redwood Road to the remaining segments of the trail.

An informal unpaved parking area with room for maybe two vehicles is located on the west side of Redwood Road, but access is dangerous. The trails head to the northwest where Bridge B32 provides access across the river. A small segment of the trails follow the west bank of the river, dead-ending in thick vegetation. There is another trail that forks off of this just north of Bridge B32, and continues to the west. The main trails cross the bridge, and traces along the east bank of the Jordan River, west of industrial uses.

As the trails approach the county line, they dip down into a large marshy meadow, and curve back to the east, where the trails can be accessed from an informal parking area located next to Redwood Road at approximately 2700 North. The trails then follow the river back towards the west, becoming very rough and undefined, until they basically becomes unusable as they approaches Interstate 215.



Figure 3.12.4 Trail entering Rose Park Golf Course



Figure 3.12.5 Jordan River Trail information sign



Figure 3.12.12 Informal parking west of Redwood Road



Figure 3.12.13 Old car frame in meadow adjacent to trail



Figure 3.12.1 Rest spot and trail access on Sunset Drive



Figure 3.12.6 Trail heading north from Bridge B31



Figure 3.12.7 Trail through Rose Park Golf Course



Figure 3.12.14 Trail segment continuing along west bank of Jordan River



Figure 3.12.15 Open fields to the west of the trail



Figure 3.12.2 Neighborhood access to Jordan River Trail

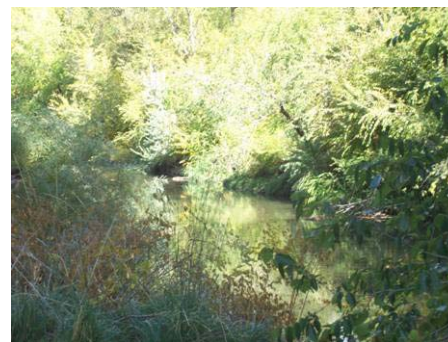


Figure 3.12.8 View of Jordan River from trail north of Rose Park Golf Course

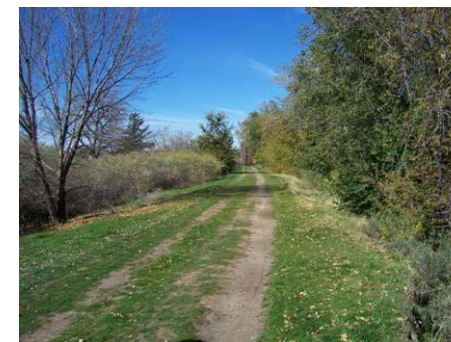


Figure 3.12.9 Trail north of Rose Park Golf Course, looking north



Figure 3.12.16 Bridge B32 8.5' wide, 88' span, 6' clearance



Figure 3.12.17 End of trail segment along western bank of Jordan River



Figure 3.12.3 Bridge B31 12' wide, 54' span, 6' clearance



Figure 3.12.10 Rest area east of Redwood Road



Figure 3.12.11 Redwood Road crossing, looking southeast



Figure 3.12.18 View to Redwood Road overpass from Bridge B32



Figure 3.12.19 View to Jordan River from Bridge B32, looking north



Figure 3.12.20 Trail west of Bridge B32, looking northwest



Figure 3.12.21 "Provo/Jordan River Parkway" sign



Figure 3.12.28 Structure spanning river



Figure 3.12.31 View across river to Utah State OHV Park



Figure 3.12.38 Garbage along trail



Figure 3.12.29 View of Jordan River from trail south of Redwood Road /2700 North access point



Figure 3.12.22 Trail access north of Bridge B32



Figure 3.12.23 Trail north of Bridge B32



Figure 3.12.32 Trail north of Redwood access point



Figure 3.12.33 View to river from trail



Figure 3.12.30 Redwood Road/2700 North trail access point



Figure 3.12.39 "Storm Drain" sign along trail



Figure 3.12.24 View of river from trail



Figure 3.12.25 Garbage along trail



Figure 3.12.34 Garbage along trail



Figure 3.12.35 Concrete remnants spanning river



Figure 3.12.40 Oil dumped along trail near river



Figure 3.12.41 Trail as it narrows approaching I-215



Figure 3.12.26 Approach to marsh



Figure 3.12.27 Trail through marsh



Figure 3.12.36 View to river from trail



Figure 3.12.37 Trail north of Redwood access point



Figure 3.12.42 Trail becomes blocked by vegetation



Figure 3.12.43 Interstate 215 overpass

Issues and Opportunities

- Paved trail only exists to Bridge B31 in the Rose Park Golf Course.
- There is no pedestrian crossing provided at Redwood Road.
- The existing informal parking area is dangerous for drivers trying to re-enter traffic.
- Portions of the trails around 2300 north are muddy due to high water table.
- There is a lot of garbage, including buckets of used motor oil, deposited along the trail corridor between Redwood Road and the current end of the trail.
- The trails dead-end about a quarter of a mile from Interstate 215.

Recommendations: Paved Trail

3.12 A Paved trail Gap: Bridge B31 to I-215

The trail on the west side of the river from Bridge B31 in the Rose Park Golf Course to approximately Riverview Avenue and Midland Street will be paved Summer 2008.

The short section of trail on the west side of the river in the future sports complex will be abandoned. Salt Lake City has no plans to extend the trail on the west side of the river, or to abandon the other fork of the City trail that heads west from this area. A paved trail should be developed on the east side of the river from the Redwood Road crossing to the Davis County line. The paved trail will be adjacent to the existing equestrian trail.

Davis County officials believe there is an easement along the east side of the river for trail development which currently has an unpaved trail. This trail would ultimately tie into the trail development related to the Legacy Parkway development. Portions of this section may require boardwalks to elevate the trail through high water table areas.

3.12 B Redwood Road Street Crossing

Funds have been acquired for a new pedestrian crossing at Redwood Road at approximately 1800 North, but Salt Lake City plans to keep the informal parking area west of this crossing for emergency access purposes, and may develop a trailhead at this location sometime in the future.

3.12 C Maintenance

All of the garbage and used motor oil needs to be cleaned up in this section, and the recommendations in Chapter 5 should be implemented.

Recommendations: Equestrian Trail

3.12 D Equestrian Trails

Access to the Utah State Fairpark needs to be maintained for equestrian users. No new equestrian trails are proposed due to width limitations within the corridor.

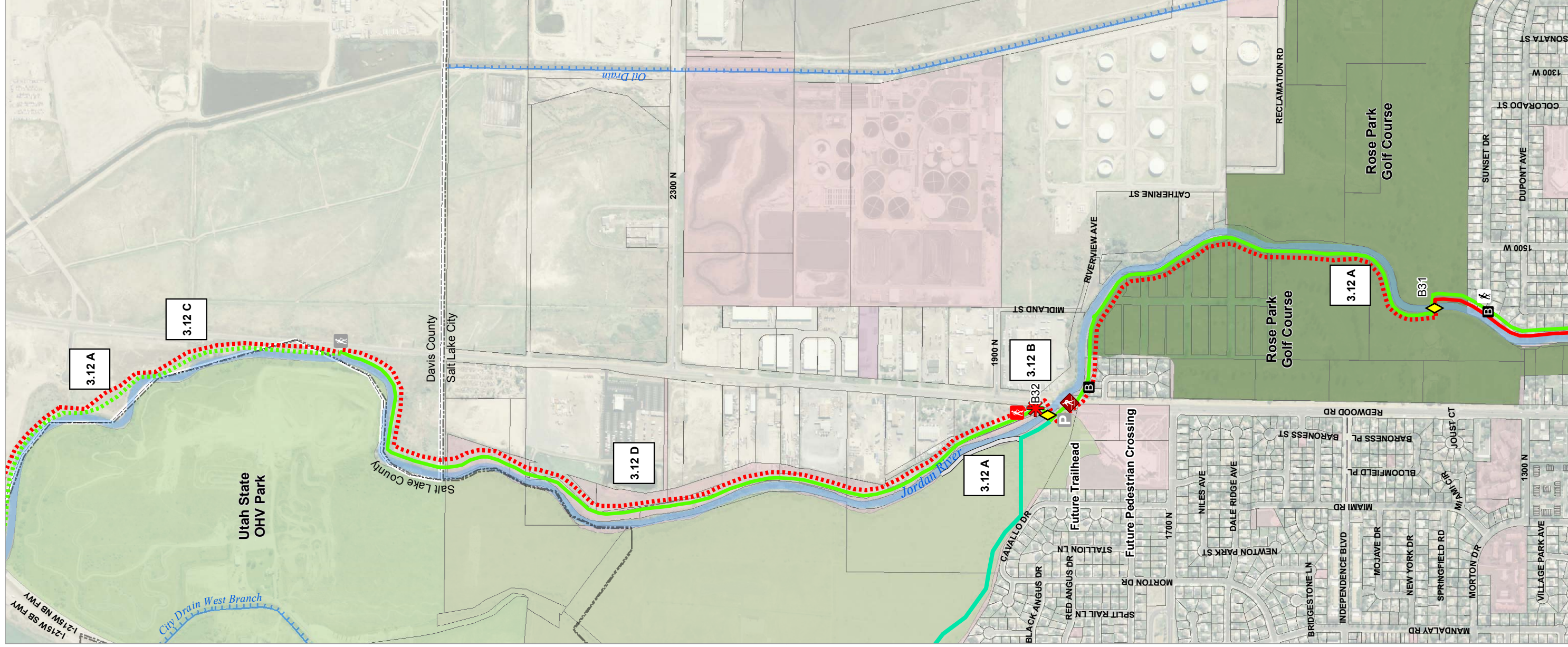
Construction Costs

Feature Category	Map Code	CIP Budget Implication
Paved Trail	3.12 A	Funded
Street Crossing	3.12 B	Funded
Maintenance	3.12 C	\$10,000
Bridge	3.12 D	\$140,000-\$220,000

Jordan River Trail Master Plan

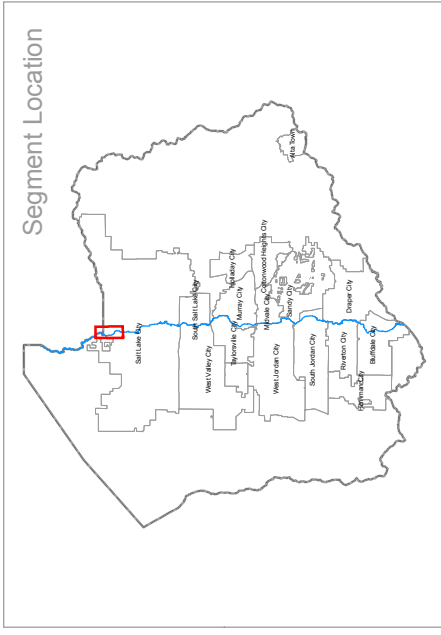
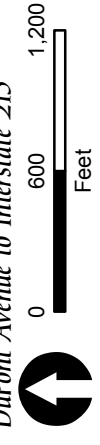
Paved Trail Facilities

- Future Element**
- Future Element
 - Alternative Future Element
- Crossings**
- Crosswalk
 - No Safe Crossing
 - Signalized Crossing
 - Underpass
 - Elevated Pedestrian Crossing
- Trail Access & Trail Facilities**
- Future Trailhead
 - Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
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- Existing Paved Jordan River Trail
 - Recommended Jordan River Trail
 - Existing Unpaved Jordan River Trail
 - Existing Unpaved Jordan River Trail - To Be Paved
 - Existing Jordan River Equestrian Trail
 - Recommended Jordan River Equestrian Trail
 - Existing Jordan River Boardwalk
 - Alternative Jordan River Trail Alignment
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 - City Proposed Trail
 - Trails to be Abandoned
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- TRAX Station
 - Direction to Nearest TRAX Station
 - Proposed FrontRunner Station
 - Direction to Nearest Proposed FrontRunner Station
- Miscellaneous**
- Point-of-Interest
 - Bridge Over Jordan River
 - Footbridge Over Stream or Canal
 - Bench
 - Picnic Table
 - Trash Receptacle
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor (no data north of Surplus Canal)
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land



Segment 12: Salt Lake City/ Salt Lake County

DuPont Avenue to Interstate 215



June 2008

4.0 Urban Water Trail

One of the key elements of the Jordan River Trail Master Plan is the water trail. Numerous groups and individuals use non-motorized watercraft on the river for recreational purposes; yet to date there has been no documentation or information available to users. It has been a long-standing desire by users, interest groups, and the County to provide valuable information about recreating on the Jordan River, and while a plan was never completed, some data was collected and made available to this planning effort.

Water Trail Data

In 2000, the Great Salt Lakekeeper (GSL), a non-profit organization whose purpose is to protect Great Salt Lake and its tributaries and environment, received a grant from the National Park Service (NPS) RTCA Program, and partnered with the Salt Lake County Division of Parks and Recreation, and SWCA, Inc. and Monson Engineering (both local consulting firms) to gather data with the intent to complete *The Jordan River Navigational Hazards Removal and Recreational Boating Plan* (unpublished). Project supporters included the Utah Division of Forestry, Fire and State Lands; Utah Division of State Parks and Recreation; Salt Lake County Engineering Department; Foundation for the Provo-Jordan River Parkway; Utah Whitewater Association; and SPLORE.

Data was collected during the summer of 2001 using Geographic Information Systems (GIS), and was processed over the winter of 2001. Maps and a database were created, and GSL then began making presentations to municipal representatives and government agencies during 2002. Though results of the data collection were never published, critical and valuable information regarding hazards, river conditions, and recreational opportunities was gathered. The data was generously provided in electronic format and was critical in developing the Urban Water Trail portion of the Master Plan, primarily because it provided the viewpoint of experienced boaters and recreation planners.

According to those who participated in the data collection, the conditions necessitating the study included the following: the Jordan River is largely perceived as unsafe for recreational boating, hazards create liability for responsible parties, the general public is not aware of hazard locations, demand for recreational boating is high, public access is severely limited, existing boating facilities are underdeveloped, the river is not perceived as a public waterway, and no coordinated water trail plan exists.

Desired results of the uncompleted project were to develop maps of hazards and recreation points, and to create navigational hazards removal and recreational boating plans, as well as produce information and marketing tools such as CD ROMS, multi-media shows, and website applications. The goal was an Urban Water Trail Guide Book that would act as a complete trail guide for the Jordan River with information on water trail facilities, wildlife in the Jordan River corridor,

and cultural information.

Two Utah laws corroborate these goals, and designate the unique status of the Jordan River as a public, navigable waterway, and require planning and implementation providing better access for boating and public use.

Administrative Rule R652-70-100: Sovereign Lands

“This rule provides for the management and classification of the surface of sovereign lands in Utah, which include but are not limited to, the beds of Bear Lake, the Great Salt Lake, Utah Lake, the Jordan River... It also provides for the issuance of special use leases, general permits and easements on sovereign lands and the procedures and fees necessary to obtain these rights of use. This rule implements Article XX of the Utah Constitution, and Section 65A-10-1.”

Administrative Rule R652-2-200

“The State of Utah recognizes and declares that the beds of navigable waters within the state are owned by the state and are among the basic resources of the state, and that there exists, and has existed since statehood, a public trust over and upon the beds of these waters. It is also recognized that the public health, interest, safety, and welfare require that all uses on, beneath or above the beds of navigable lakes and streams of the state be regulated, so that the protection of navigation, fish and wildlife habitat, aquatic beauty, public recreation, and water quality will be given due consideration and balanced against the navigational quality or economic necessity or justification for, or benefit to be derived from, any proposed use.”

About Water Hazard Names & Classifications

The existing condition of the Urban Water Trail is described beginning in Section 4.1, and represents the data collected during the GSL/NPS study. They divided the river corridor into 15 different sections, and the hazards and facilities within each section were given an individual waypoint number and mapped using GIS. Descriptions and photographs were also collected as necessary and are included here. Using the expertise of experienced boaters and recreation planners, the project team assigned a level of risk to each hazard and a functionality and location value to each facility using the most dangerous hazard as a point of comparison for other hazards, and the best facilities as points of comparison for water trail facilities.

The project section numbers do not correspond with the segment numbers of this plan, therefore, the waypoints described in the text and shown on the maps indicate the GSL/NPS section number followed by the data point number within that section. This number is followed by the hazard ranking in parentheses. Hazard levels can vary depending upon the level and flow of the river. The rankings shown were assigned at the time of data collection in the summer of 2000, a low water year.

Some waypoints are described as launches, take-outs, or portages. A launch is a put-in where boaters can begin a trip, a take-out is where

boaters end a trip, and a portage is composed of two sites, where boaters must take-out around a hazard and put-in again on the downstream side of the hazard. Some locations may serve as both launches and portages or take-outs and portages. Sites that serve as launches or take-outs need convenient access to parking and restrooms and require larger ramps for staging large groups. Sites that serve only as portages require smaller ramps, and do not need access to restrooms or parking.

Launches, put-ins, and portages were ranked according to current level of functionality in 2002, and the value of the location as a system-wide element. For example, a waypoint that is rated with low functionality and high value location, does not function adequately in its current condition, but is in a good location considering its position within the water trail system as a whole and individual site characteristics.

Overall Issues, Opportunities, and Recommendations

The issues pertaining to the overall water trail corridor are discussed below, followed by an explanation of water hazard documentation in general. Detailed descriptions of existing conditions, issues and opportunities, and recommendations for each of the twelve corridor segments are then provided in section 4.1 through 4.12.

Trail Gaps

Discontinuous trail segments prevent water trail users from enjoying a continuous trip on the Jordan River through Salt Lake County, and the primary factor causing this fractured condition of the water trail is the prevalence of hazards and impassable elements within the river corridor. These hazards range from dams, weirs, and debris to bridges with low clearance. In addition, portage facilities and rights-of-way have not been negotiated to give water trail users a clear, safe route through the entire Jordan River corridor.

One of the most important keys to improving the functional level of the water trail corridor is to “fill in” these gaps by negotiating hazard removal, negotiating and developing appropriate portage facilities around the hazards, and improving safety and awareness through signage and education efforts.

Signage

Signage along the Urban Water Trail currently represents a hodgepodge of designs, information content, and purposes, and presents trail users with incomplete and sometimes confusing, information about the trail facilities. Water trail facility signs are sporadic and inconsistent, and the trail lacks consistent location signs for information and emergency purposes as well as water hazard warning signs.

The Foundation for the Provo-Jordan River Parkway, along with Salt Lake County developed a standard trailhead sign which has been installed at several trailheads along the river, though they typically do not have any information regarding water trail facilities, guidelines,

safety information, or water trail maps. This plan recommends the implementation of a comprehensive signage program to improve the functional level, safety, and awareness of the water trail. Detailed information can be found in section 5.4 of this plan.

Trail Maps

The lack of a comprehensive, easy-to-read, readily available map prevents many potential water trail users from exploring the Jordan River. As discussed in Chapter 5, a comprehensive water trail map should be incorporated as part of the Salt Lake County standard trailhead sign design. In addition, the County should make mapping available online, and in printed format that water trail users can utilize to plan trips, and to take with them on their visits to the Jordan River. These map could take the form of a guide booklet or large foldout map, and could include information on the history of the river, flora and fauna found along the river, and cultural information. These printed guides should be made available online and at County facilities throughout the Salt Lake Valley. An excellent example is the "Willamette River Water Trail Guide," which can be viewed and downloaded at www.willamettewatertrail.org.

Bridges

Too many bridges across the river detract from the quality of the water trail user experience and can present hazards if not designed and installed correctly. Therefore, the number of new bridges should be strictly limited.

Water Trail Hazards

Concrete, flood debris, trees, garbage, and overhanging limbs cause dangerous boating conditions on the Jordan River, particularly those noted as medium, high, or very high hazard in the following text. Water trail hazards can vary with the level of the river. The general public needs to be informed of the risks to make educated decisions about recreation on the Jordan River. Salt Lake County should also be negotiating with responsible parties for hazard removal or safe portage around these facilities, including appropriate signage.

Trailheads

More water trailheads are needed that provide adequate parking in close proximity to launch and portage locations. Interpretive signage at water trailheads should inform water trail users about the history and ecology of the river, including plant and animal species they may encounter on their river trip.

Water Trail Facility Designs

Launches and portages need to be developed with safe, flexible, functional designs that meet water trail user needs at different flow levels of the river, and that accommodate boating parties of varying sizes and skill levels.

Kayak Parks

Kayakers have expressed an interest in developing kayak play areas at 2100 South and 6400 South. This plan recommends additional

studies to explore the feasibility of such parks.

Rowing

Rowers are currently practicing on the Great Salt Lake and Surplus Canal, and have expressed an interest in rowing facilities closer to the cities. The site requirements for rowing cannot be met along the Jordan River and facilities are not included as part of this plan.

Water Level

The ability to travel the entire river length depends on the water level. Sometimes the flow is too low to allow boats to pass, and other times the flow is too high for all recreational users except those who are very skilled and who have first-hand knowledge of the corridor and its hazards.

Comprehensive Planning

A comprehensive regional plan for the Jordan River from its origin at Utah Lake to its destination at the Great Salt Lake should be developed in coordination with this plan to facilitate access to these sovereign waters.

4.1 Segment 1: Bluffdale (Southern County Line to 15400 South) (GSL segments 2,3, and 4)

The Jordan River flows north from Utah Lake, and enters Salt Lake County at the Bluffdale City boundary, near the Jordan Narrows Diversion Dam and Canals.



Figure 4.1.1 Floating barricades at the Narrows, Utah County (WP 2-08)



Figure 4.1.2 Turner Dam at the Narrows (WP 3-01)



Figure 4.1.3 Railroad Bridge crossing at the Narrows (WP 3-02)



Figure 4.1.4 Portage put-in for the Turner Dam at the Narrows (WP 3-03)



Figure 4.1.5 Foot bridge for Gauging Station Bridge at the Narrows (WP 3-04)



Figure 4.1.6 Rapids and split channel at the Narrows (WP 3-06)



Figure 4.1.7 Rapids, right channel at the Narrows (WP 3-07)

the area. Water trail users are currently faced with unsafe conditions as they try to pass through the area, or try to begin a river trip in this portion.

As water trail users travel downstream from Utah County, they encounter the Jordan Narrows Pump Station barrels at waypoint 2-08 (high hazard), just south of the Salt Lake County line. This forces boaters to take-out at waypoint 2-09 (high hazard) to get around the Jordan Narrows Pump Station and Turner Dam. This portage has low functionality, and is a high value location - it is a critical portage, but access is unsafe and not formally allowed by the water district or the canal companies. Boaters have been known to take-out in this area and carry their boats along the railroad tracks until they reach the river again, which is illegal and very unsafe because trains travel at high speeds through the area. The river then flows through the Turner Dam, waypoint 3-01 (very high hazard), and underneath the Union Pacific rail bridge, waypoint 3-02 (zero hazard). There is concrete in the channel under the bridge, waypoint 3-02a, (high hazard). Waypoint 3-03 is a portage downstream from the UP rail bridge with low functionality and a low value location due to the long portage around the Pump Station.

Water trail users then travel north a short distance before encountering the Gauge Station Bridge, waypoint 3-04 (very high hazard), which is a small footbridge over the Jordan River with a 4 foot clearance. This bridge is extremely dangerous to boaters in high water conditions

because of the low clearance. Users proceed through small rapids, waypoint 3-05 (zero hazard), and past a mid-stream island which clogs the channel, resulting in a narrow passage, waypoint 3-06 (low hazard). The river splits around the island with the main channel passing to the left, waypoint 3-07 (low hazard), and an area with overhanging limbs along the west shore. There are only 3 islands remaining in the Jordan River Corridor within Salt Lake County - a remnant of the past historical character of the river before settlers modified the river course to meet their agricultural needs.

Next boaters encounter the Rock Garden rapids, waypoint 3-08 (low hazard), passing through the 50-yard-long right-split of the river with a riffle, waypoint 3-09 (zero hazard), and the tail end of the Rock Garden rapids, waypoint 3-10 (medium hazard). The water trail then proceeds through three sets of rapids, waypoints 3-11 (zero hazard), 3-12 (zero hazard) and 3-13 (medium hazard), where the left and right forks of the channel rejoin at waypoint 3-14 (medium hazard).

Boaters proceed north where the river splits again with the main channel going to the west, passing through rapids at waypoint 3-15 (zero hazard), 3-16 (zero hazard), 3-17 (medium hazard), 3-18 (zero hazard), 3-19 (zero hazard) rapids. The forks of the river join near waypoint 3-20 (zero hazard), where the spillway canal also meets the river. The water trail continues through rapids at waypoint 3-21 (zero hazard) where there are large chunks of debris and concrete in the channel. The river passes under waypoint 3-21a (zero hazard), a historic bridge crossing and through waypoint 3-22 (high hazard). Old pilasters of a bridge that have been left in the river channel and along the sides.

The trail flows north about a quarter of a mile to waypoint 3-23 (medium hazard), where flood debris with rapids block 90 percent of the channel. Boaters then pass an old irrigation diversion with one wall remaining on the bank, and an old structure on the west bank, waypoint 3-24 (zero hazard). Water trail users must then take-out before they reach waypoint 3-25 (medium hazard), the lower dam at Jordan Narrows. Waypoint 3-26 is the portage for this dam and is located on west side of the dam between the irrigation and diversion dam. It has low functionality and a high value location. Boaters then launch at waypoint 3-27, a put-in on the west bank composed of a concrete ramp heading down to the river. This launch has low functionality and a high value location.

The trail continues to waypoint 3-28 (high hazard), where flood debris totally chokes off the river and causes a one-foot drop in the water. There is no way to get a boat through this area, and boaters must take-out and put-in between the dam and railroad crossing with no safe portage and launch. The river then flows to waypoint 3-29 (high hazard), where a fallen tree on the west side and two closely-spaced trees impede travel. There is more flood debris at waypoint 3-30 (high hazard), just south of the railroad bridge, and at waypoint 4-01 (medium hazard), where flood debris has collected under the railroad bridge and covers 75 percent of the channel.



Figure 4.1.8 Rapids, rock garden, right channel at the Narrows (WP 3-08)



Figure 4.1.9 Rapids, riffle right split (WP 3-09)



Figure 4.1.16 Another split channel at the Narrows (WP 3-18)



Figure 4.1.17 Split in channel, main channel left (WP 3-19)



Figure 4.1.10 Tail end of rock garden rapids (WP 3-10)



Figure 4.1.11 Rapids after rock garden, right channel (WP 3-12)



Figure 4.1.18 Rapids after Cultural Site at the Narrows (WP 3-21)



Figure 4.1.19 Cultural Site, Historic Bridge Crossing at the Narrows (WP 3-21A)



Figure 4.1.12 Rapids, end of right channel at the Narrows (WP 3-13)



Figure 4.1.13 Right and left fork channels meet (WP 3-14)



Figure 4.1.20 Rapids at base of Cultural Site at the Narrows (WP 3-22)



Figure 4.1.21 Upper narrows below Cultural Site at the Narrows (WP 3-23)



Figure 4.1.14 River splits again, main channel goes left (WP 3-16)



Figure 4.1.15 End of second split channel at the Narrows (WP 3-17)



Figure 4.1.22 Cultural Site, Historic Irrigation Dam at the Narrows (WP 3-24)



Figure 4.1.23 Take out at Lower Narrows Irrigation Dam (WP 3-25)



Figure 4.1.24 Lower irrigation dam at the Narrows (WP 3-27)



Figure 4.1.25 Railroad bridge at Lower Narrows (WP 4-01)



Figure 4.1.32 Overhanging tree at Lower Narrows (WP 4-08)



Figure 4.1.26 Rock garden at Lower Narrows (WP 4-02)



Figure 4.1.27 Rapids at Lower Narrows (WP 4-03)



Figure 4.1.28 Rock garden at Lower Narrows (WP 4-04)



Figure 4.1.29 Rapids at Lower Narrows (WP 4-05A)



Figure 4.1.30 Rapids at Lower Narrows (WP 4-06)



Figure 4.1.31 Rapids at Lower Narrows (WP 4-07)

Issues and Opportunities

- The approach to the barrels and portage are not safe.
- The portage at waypoint 2-09 functions well, but is not in the best location. Water trail users continuing down the Jordan River from Utah Lake have been hauling their canoes along the rail line trying to get to a place where they can safely re-launch. There is not a safe route from the portage to a safe, convenient launch.
- The existing launch at waypoint 3-03 is too far from the existing portage.
- The gauge station bridge at waypoint 3-04 is too low.
- The concrete at waypoint 3-02a should be removed if users will be portaging around this area.
- Old pilasters of a bridge at waypoint 3-22 have been left in the river and are dangerous for boaters.
- The existing portage at waypoint 3-26 for the lower dam is not suitable for safe use.
- The launch at waypoint 3-27 is not suitable for safe use.

Recommendations

4.1 A Boaters Approaching From Utah County and Completing a Trip at the Narrows or Continuing Down-River

Provider signing up-river to warn boaters of the Jordan River Pump Station barrels stretching across the river at the diversion dam.

Provide a safe portage up-river of the pump station and diversion dam for boaters completing a trip from Utah County, or those who wish to continue on the river. The launch portion of the portage should take place on the downriver side of the dam north of the small parking lot and driveway at the caretakers home. This is not an official launch point; it is a fairly long portage allowing boaters to continue on their river trip.

Current land ownership and security issues may make this difficult; however, in the future the portage and limited parking spaces would greatly enhance the boating experience and enhance safety. As improvements in the area are addressed, consideration should be made to accommodate safely exiting the river at this point. A reservation system may also work, where boaters must make prior

arrangements with the property owner to park a car and/or be picked up at the portage. Boaters would also have access to the restroom and parking available at the new Jordan Narrows Trailhead.

4.1 B Gauge Station Bridge (Waypoint 3-04)

Remove, and replace if necessary, as this bridge is too low and causes hazardous conditions.

4.1 C Hazards (Waypoint 3-22)

Remove and appropriately dispose of the concrete pilasters which remain in the river.

4.1 D Portage and Launch (Waypoints 3-26 and 3-27)

Redesign the existing portage and launch, and combine if possible. This is a desirable location for a portage and launch.

4.1 E Interpretive Opportunity (Waypoint 4-05)

A cultural interpretive opportunity exists with the old power station. Interpretation should be addressed corridor-wide and include consistent signing.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Portage/Signage	4.1 A	\$12,000
Bridge	4.2 B	\$140,000
Hazard Removal	4.1 C	n/a
Launch/Portage Upgrade	4.1 D	\$20,000
Signage	4.1 E	\$2,500

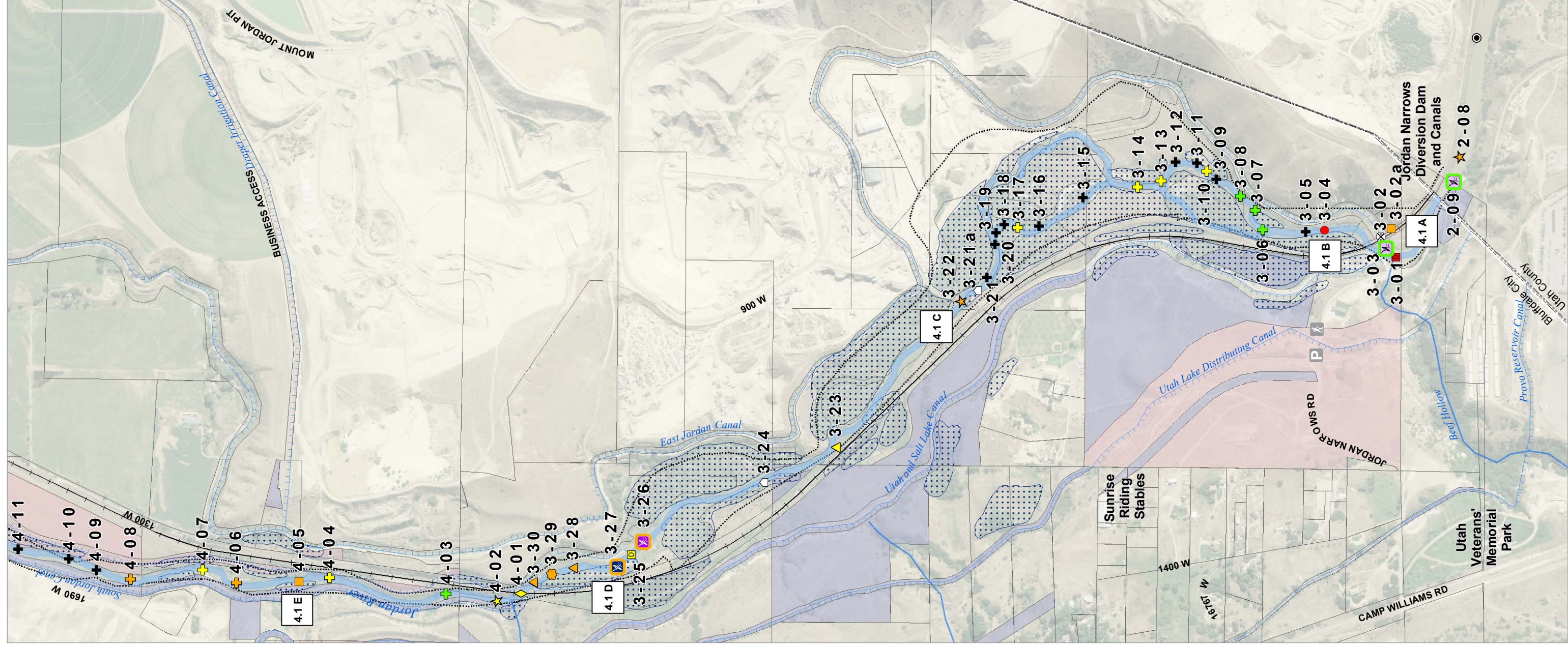
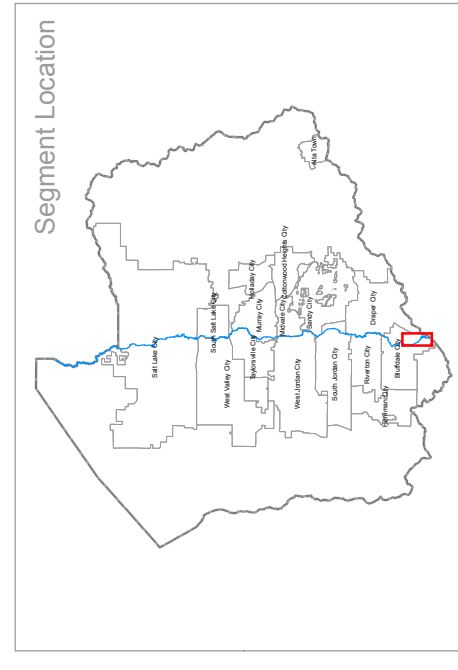
Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
 - Launch - HV Location, MV Function
 - Launch - HV Location, LV Function
 - Launch - MV Location, VLV Function
 - Launch - MV Location, MV Function
 - Launch - LV Location, LV Function
 - Launch - VLV Location, LV Function
 - Portage - HV Location, LV Function
 - Portage - HV Location, HV Function
 - Portage - LV Location, VLV Function
 - Portage - LV Location, HV Function
 - Portage - High Hazard
 - Potential Launch - HV Location, MV Function
 - Potential Launch - HV Location, LV Function
 - Potential Launch - LV Location, VLV Function
 - Potential Launch - LV Location, LV Function
 - Bridge - High Hazard
 - Bridge - Medium Hazard
 - Bridge - Low Hazard
 - Bridge - No Hazard
 - Footbridge - Very High Hazard
 - Footbridge - Medium Hazard
 - Footbridge - Low Hazard
 - Footbridge - No Hazard
 - Railroad Bridge - High Hazard
 - Railroad Bridge - Medium Hazard
 - Railroad Bridge - No Hazard
 - Dam - Very High Hazard
 - Dam - High Hazard
 - Dam - Medium Hazard
 - Dam - Low Hazard
 - Weir - Very High Hazard
 - Weir - High Hazard
 - Flood Debris - Very High Hazard
 - Flood Debris - High Hazard
 - Flood Debris - Medium Hazard
 - Flood Debris - Low Hazard
 - Pipe - Very High Hazard
 - Pipe - High Hazard
 - Pipe - Medium Hazard
 - Pipe - Low Hazard
 - Pipe - No Hazard
 - Cultural Element - Low Hazard
 - Cultural Element - No Hazard
 - Miscellaneous Debris - High Hazard
 - Miscellaneous Debris - Medium Hazard
 - Miscellaneous Debris - Low Hazard
 - Miscellaneous Structure - Very High Hazard
 - Miscellaneous Structure - High Hazard
 - Miscellaneous Structure - Medium Hazard
 - Miscellaneous Structure - Low Hazard
 - Miscellaneous Structure - No Hazard
 - Rapid - High Hazard
 - Rapid - Medium Hazard
 - Rapid - Low Hazard
 - Rapid - No Hazard
 - Tributary - No Hazard
 - Tree - Very High Hazard
 - Tree - High Hazard
 - Tree - Medium Hazard
 - Tree - Low Hazard
 - Sign
- Equestrian Trailhead
 Formal Trailhead
 Informal Trailhead
 Trail Access Point
 Parking - Formal Paved
 Parking - Informal Unpaved
 Restrooms
 Highways
 City Boundary
 Jordan River
 Streams
 Canals
 Light Rail
 Railroad
 County Boundary
 Meander Corridor
 Wetlands (WAIDS 2007)
 Golf Course
 Park
 Publicly Owned Land
 Utility Owned Land
- HV = High Value
 MV = Medium Value
 LV = Low Value
 VLV = Very Low Value
- Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.
 Waypoint locations have been modified for readability.

Segment 1: Bluffdale

Southern County Line to Business Access Road



4.2 Segment 2: Bluffdale/Riverton (15400 South to Bangerter Highway) (GSL segments 4 and 5)

The trail continues north to another riffle at waypoint 4-11 (zero hazard), through rapids at waypoint 4-12 (low hazard), and past an abandoned irrigation dam at waypoint 4-13 (low hazard). Boaters then encounter rapids with flood debris at waypoint 4-14 (medium hazard) and rapids at waypoint 4-15 (low hazard) along the right fork of another split in the river at approximately 15500 South. A barbed-wire fence at waypoint 4-16 (high hazard) and a fallen tree at waypoint 4-17 (high hazard) completely block the river channel under the Rocky Mountain Power high voltage line, presenting a serious challenge to recreational water users.

The trail then passes through a small rapid at waypoint 4-18 (low hazard) and a rapid with an overhanging tree on the right side of the channel at waypoint 4-19 (high hazard). Another barbed-wire fence occurs at waypoint 4-20 (high hazard) where the river rejoins. If users take the left fork of the river, they will encounter a rock weir at waypoint 4-21 (high hazard), a riffle at waypoint 4-22 (low hazard), and rapids at waypoint 4-23 (zero hazard). The river then flows through more rapids at waypoint 4-24 (zero hazard) and waypoint 4-26 (zero hazard) before reaching an earthen weir at waypoint 4-27 (high hazard).

Trail users pass two fence poles in the middle of the river with rapids at waypoint 4-28 (zero hazard) and proceed north through more rapids at waypoints 4-29 (zero hazard), 4-30 (zero hazard), and 4-31 (zero hazard). There is a launch at waypoint 4-32, on the south side of 14600 South, along the west bank of the river. The launch has low value functionality because it is steep and rocky, and has no parking. However, it is a high value location. There is a pipeline at waypoint 4-33 (zero hazard), just upstream of the 146000 South vehicular bridge, waypoint 4-34 (zero hazard).

There are a series of rapids at waypoints 5-01 (zero hazard), 5-02 (zero hazard), and 5-03 (zero hazard), where the river bends to the east. Boaters then pass waypoint 5-04 (zero hazard), the location of an old barbed-wire fence that used to run across river. A barbed-wire

fence used to run across the river at waypoint 5-05 (high hazard), but has since been modified to keep cattle out while allowing water users through, and now represents no hazard. The fence was changed to a wire crossing the river with pieces of pvc pipe dangling down to scare cattle away from the crossing. Water trail users pass waypoint 5-06 (medium hazard), an island with rapids, rapids at waypoints 5-07 (zero hazard) and 5-08 (zero hazard), and another island with rapids at waypoint 5-09 (zero hazard). More rapids are found at waypoints 5-10 (zero hazard) and 5-11 (low hazard), then at waypoint 5-12 (medium hazard) where the current slams boats into overhanging bulrushes as the river curves back to the north. Just upstream of Bangerter Highway, waypoints 5-13 (zero hazard) and 5-14 (low hazard), more rapids are encountered.



Figure 4.2.1 Rapids, flood debris Lower Narrows (WP 4-11)



Figure 4.2.2 Rapids, ladder at Lower Narrows (WP 4-12)



Figure 4.2.7 Fallen tree Lower Narrows (WP 4-17)



Figure 4.2.8 Rapids, channel splits (WP 4-18)



Figure 4.2.15 Barbed fence across river (WP 5-05)



Figure 4.2.16 Island, rapids at Loumis property (WP 5-06)



Figure 4.2.3 Cultural Site, Historic Dam (WP 4-13)



Figure 4.2.4 Rapids with flood debris (WP 4-14)



Figure 4.2.11 Rock weir (WP 4-21)



Figure 4.2.12 Rapids (WP 4-23)



Figure 4.2.5 Barbed fence at Lower Narrows (WP 4-15)



Figure 4.2.6 Barbed fence, river splits (WP 4-16)



Figure 4.2.13 Earthen weir (WP 4-27)



Figure 4.2.14 Questar Pipeline upstream of bridge (WP 4-32)



Figure 4.2.9 Rapids, overhanging tree, hazard at right (WP 4-19)



Figure 4.2.10 Barbed fence, river splits (WP 4-20)



Figure 4.2.17 Rapids, canoe (WP 5-07)



Figure 4.2.18 Rapids, island (WP 5-09)



Figure 4.2.19 Rapids (WP 5-10)



Figure 4.2.20 Rapids slam boat into overhanging bulrush (WP 5-12)

Construction Costs

Table 4.2.1– Construction Costs: Water Trail Segment 2		
Feature Category	Map Code	CIP Budget Implication
Hazard Removal	4.2 A	n/a
Hazard Removal	4.2 B	n/a
Launch/Portage Upgrade	4.2 C	\$15,000
Hazard Removal/Signage	4.2 D	n/a

Issues and Opportunities

- Barbed-wire fences spanning the river at waypoints 4-16 and 4-20 are dangerous for boaters and prevent access.
- There is a rock weir at waypoint 4-21 is dangerous for boaters.
- An earthen weir spans the river at waypoint 4-27 is hazardous for water trail users.
- The existing launch at 4-32 along 14600 South is an important location, but the functionality of the launch is very low.
- There is another barbed-wire fence at waypoint 5-05 that is hazardous for recreational water users.
- At waypoint 5-12, the vegetation should be removed.

Recommendations

4.2 A Barbed-wire Fencing (Waypoints 4-16 and 4-20)

Remove barbed-wire fencing at these points.

4.2 B Earthen Weir (Waypoint 4-27)

Remove earthen weir in this location.

4.2 C Launch at 14600 South (Waypoint 4-32)

Redesign and reconstruct the launch so that it is safe and provides for some designated parking.

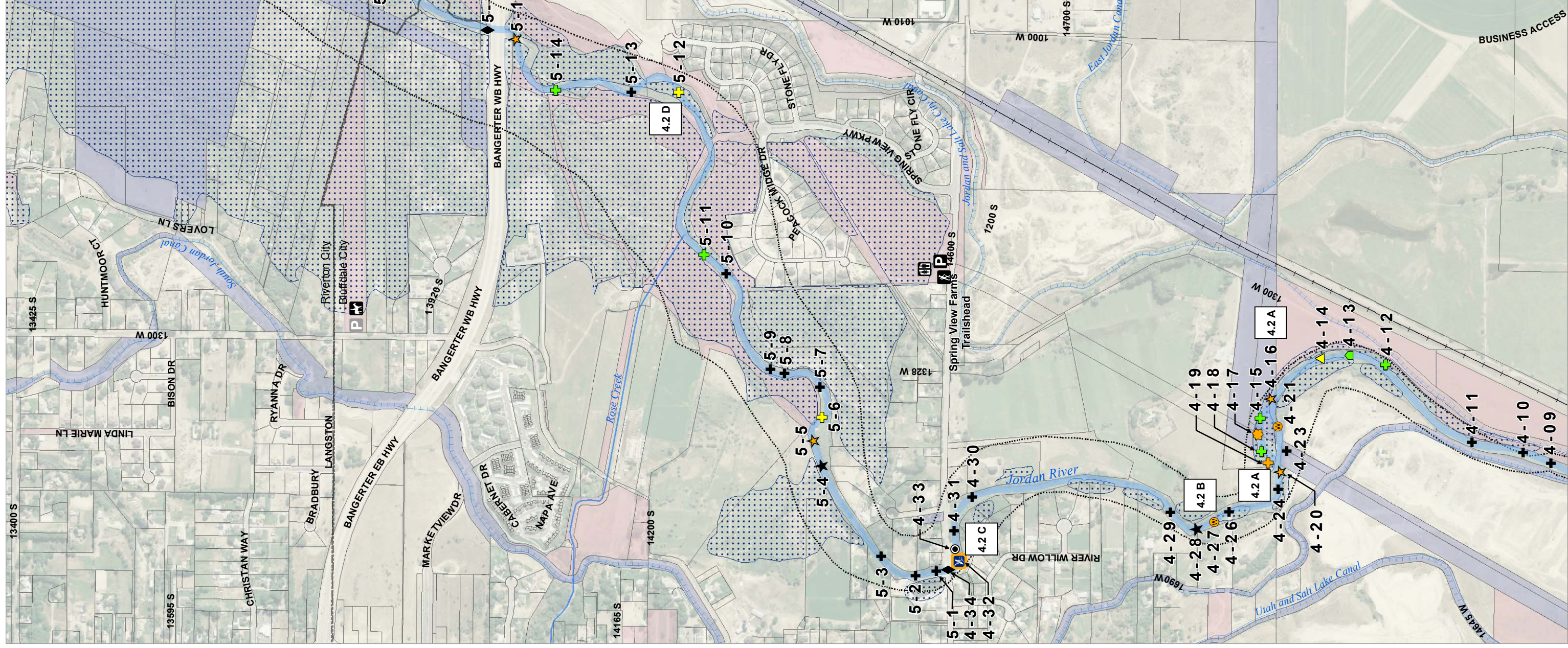
4.2 D Unsafe River Conditions (Waypoint 5-12)

Remove vegetation which may obstruct visibility and provide signing warning boaters that the river changes direction quickly in this location.

Jordan River Trail Master Plan

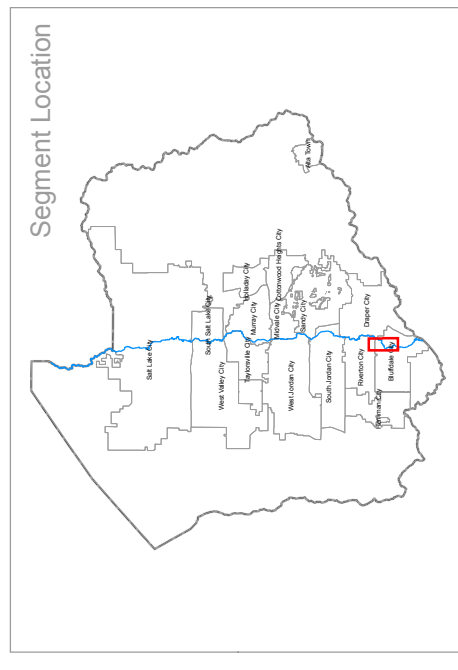
Urban Water Trail Recommendations

- Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
 - Highways
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land
 - Launch - HV Location, HV Function
 - Launch - HV Location, MV Function
 - Launch - HV Location, LV Function
 - Launch - HV Location, VLV Function
 - Launch - MV Location, MV Function
 - Launch - MV Location, LV Function
 - Launch - LV Location, LV Function
 - Launch - VLV Location, LV Function
 - Portage - HV Location, HV Function
 - Portage - HV Location, MV Function
 - Portage - HV Location, VLV Function
 - Portage - High Hazard
 - Potential Launch - HV Location, MV Function
 - Potential Launch - HV Location, LV Function
 - Potential Launch - MV Location, LV Function
 - Potential Launch - LV Location, LV Function
 - Bridge - High Hazard
 - Bridge - Medium Hazard
 - Bridge - Low Hazard
 - Bridge - No Hazard
 - Footbridge - Very High Hazard
 - Footbridge - Medium Hazard
 - Footbridge - Low Hazard
 - Footbridge - No Hazard
 - Railroad Bridge - High Hazard
 - Railroad Bridge - Medium Hazard
 - Railroad Bridge - No Hazard
 - Dam - Very High Hazard
 - Dam - High Hazard
 - Dam - Medium Hazard
 - Dam - Low Hazard
 - Weir - Very High Hazard
 - Weir - High Hazard
 - Flood Debris - Very High Hazard
 - Flood Debris - High Hazard
 - Flood Debris - Medium Hazard
 - Flood Debris - Low Hazard
 - Pipe - Very High Hazard
 - Pipe - High Hazard
 - Pipe - Medium Hazard
 - Pipe - Low Hazard
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 - Cultural Element - Low Hazard
 - Cultural Element - No Hazard
 - Miscellaneous Debris - High Hazard
 - Miscellaneous Debris - Medium Hazard
 - Miscellaneous Debris - Low Hazard
 - Miscellaneous Structure - Very High Hazard
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 - Miscellaneous Structure - Low Hazard
 - Miscellaneous Structure - No Hazard
 - Rapid - High Hazard
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 - Tree - Low Hazard
 - Sign
- HV = High Value
MV = Medium Value
LV = Low Value
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- Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.
- Waypoint locations have been modified for readability.



Segment 2: Bluffdale/Riverton

Business Access Road to Bangerter Highway



4.3 Segment 3: Bluffdale/Riverton/Draper (Bangerter Highway to Vahe Street) (GSL segments 5 and 6)

At the time the original data was collected, another barbed-wire fence ran across the river at waypoint 5-15 (high hazard), immediately south of waypoint 5-16 (zero hazard), the Bangerter Highway bridge. It is believed that this fence has now been removed with the development of Spring View Farms, but this needs to be field verified. North of Bangerter Highway, a sewer line crosses the river at waypoint 5-17 (zero hazard). This line passes overhead, and a distinct foul odor permeates the area for a good distance up and downstream, creating a negative impact for water users. A footbridge then crosses the river at waypoint 5-18 (zero hazard), followed by rapids at waypoint 5-19 (low hazard). As the river begins to bend to the east again, a tree snag and flood debris restrict the channel at waypoint 5-20 (medium hazard), with more flood debris at waypoint 5-21 (medium hazard) as the river begins to flow to the north again. At waypoint 5-22 (zero hazard) north of the hot springs, an old barbed-wire fence used to cross the channel, but no longer poses a hazard.



Figure 4.3.1 Barbed wire fence across river (WP 5-15)

The water trail continues north to a snag at waypoint 5-23 (medium hazard), past waypoint 5-24 (zero hazard) where another old barbed wire fence used to present boaters with a major hazard. The trail then passes through a series of hazards at waypoints 5-25 (low hazard) flood debris, 5-26 (low hazard) a fallen tree, 5-27 (medium hazard) flood debris, 5-28 (low hazard) a fallen tree, 5-29 (low hazard) tree, and 5-30 (medium hazard) a tree in the river current. As the river bends to the west, boaters then pass under waypoint 5-31 (medium hazard), a footbridge for the River Bend Golf Course. This bridge becomes a high hazard during high water flows from spring runoff or releases from Utah Lake. There is an undeveloped dirt bank boat launch at waypoint 6-01 near 12600 South in Draper. The launch has low functionality and a high value location.



Figure 4.3.2 Flood debris snag (WP 5-20)

Just downstream from the launch, there is a snag in the center of the channel at waypoint 6-02 (medium hazard), but is easy to clear. Trail users must then navigate past waypoint 6-03 (low hazard) where bank slumping and assorted debris makes the channel more difficult to clear. As the river curves to the north, waypoint 6-04 (medium hazard) boaters encounter three islands, flood debris, cattail islands, and

cattails growing in mid-channel, a tree and boulders in the current, and boulders fallen from bluff-slumps at waypoint 6-05 (medium hazard). The trail then proceeds north under waypoint 6-06 (zero hazard), the 12600 South street vehicular bridge, and a tree with debris flow north of 12600 south at waypoint 6-07 (medium hazard). Immediately downstream, boaters pass through waypoints 6-08 (low hazard), concrete riprap in the river channel mixed with boulders where garbage is piling up, and 6-09 (zero hazard), the historic footbridge in Riverton at 12300 South.



Figure 4.3.3 Flood debris in path of turn (WP 5-21)



Figure 4.3.4 Snag on river (WP 5-23)



Figure 4.3.5 Flood debris (WP 5-27)



Figure 4.3.6 Fallen tree (WP 5-28)



Figure 4.3.7 Fallen tree (WP 5-29)



Figure 4.3.8 Boat launch, undeveloped ramp (WP 6-01)



Figure 4.3.9 Snag in center of channel (WP 6-02)



Figure 4.3.10 Slumping bank (WP 6-03)



Figure 4.3.11 Three islands, flood debris, cattails (WP 6-04)



Figure 4.3.12 Flood debris, boulders from slumps (WP 6-05)



Figure 4.3.13 Vehicular bridge at 12600 South (WP 6-06)



Figure 4.3.14 Tree flood debris north of 12600 South (WP 6-07)



Figure 4.3.15 Concrete riprap in channel, boulders, debris (WP 6-08)



Figure 4.3.16 Historic footbridge at 12300 South (WP 6-09)

Issues and Opportunities

- A barbed-wire fence spans the river at waypoint 5-15, causing a significant hazard for boaters.
- The footbridge in Riverbend Golf Course at waypoint 5-31 is too low.
- The existing launch at waypoint 6-01 near 12600 South in Draper is an important location but is not functional.

Recommendations

4.3 A Riverbend Golf Course Bridge (Waypoint 5-31)

An existing footbridge at the golf course is low and causes unsafe conditions during high water events. This bridge should be replaced or reconstructed so there is more clearance for boaters.

4.3 B Launch Site at 12600 South (Waypoint 6-01)

Although this is a highly valued launch site, there is no safe access for water trail users from 12600 South, and there is already an existing trailhead with parking facilities on the north side of 12600 South at Jordan River Rotary Park. It is recommended that a new launch be constructed at this trailhead.

4.3 C Vegetation Near Islands (Waypoint 6-04)

The vegetation near islands are choking the waterway and present a hazard. The islands are some of the last remnants of the pre-settlement condition of the Jordan River. The river channel should be cleared of vegetation and debris which presents the boating hazard.

Construction Costs

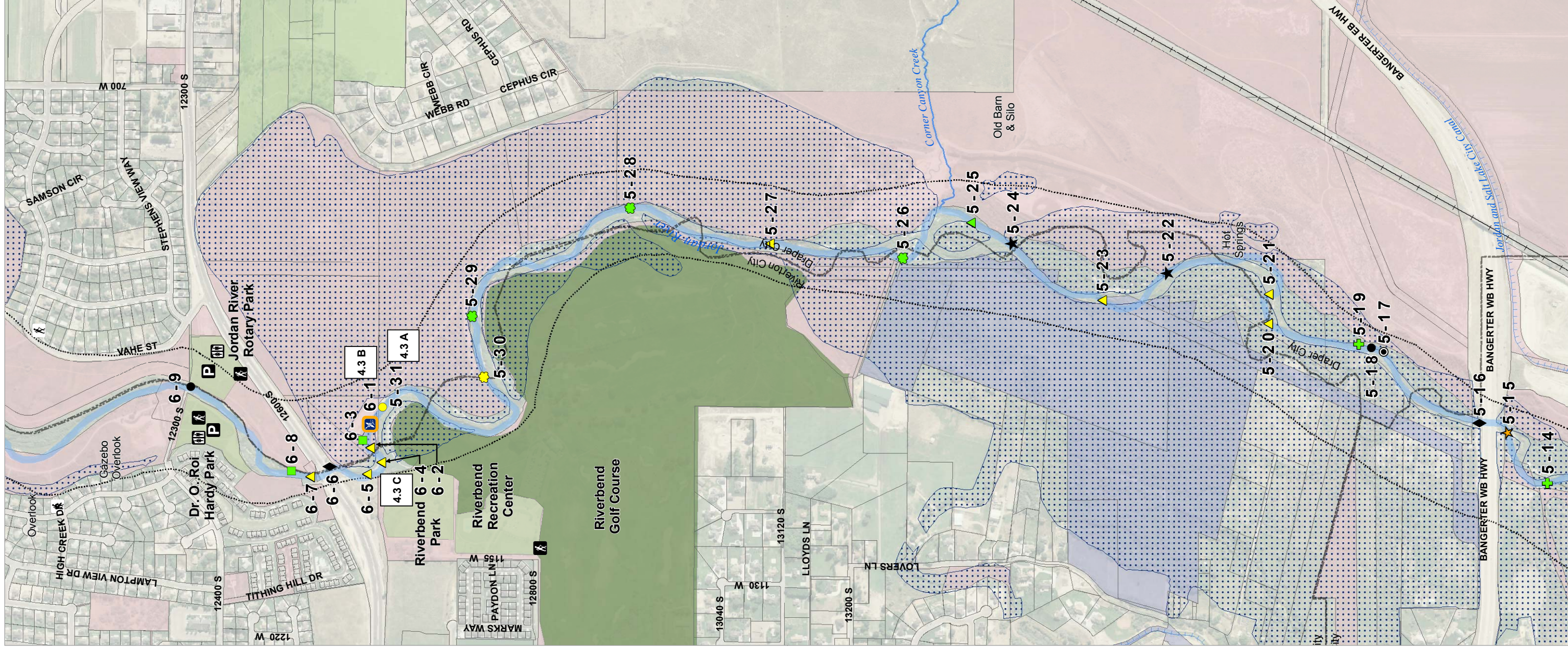
Feature Category	Map Code	CIP Budget Implication
Bridge	4.3 A	\$180,000
Launch Upgrade/Trailhead	4.3 B	\$10,000
Hazard Removal	4.3 C	n/a

Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
- Launch - HV Location, MV Function
- Launch - HV Location, LV Function
- Launch - HV Location, VLV Function
- Launch - MV Location, MV Function
- Launch - MV Location, LV Function
- Launch - LV Location, LV Function
- Launch - VLV Location, LV Function
- Portage - HV Location, LV Function
- Portage - HV Location, VLV Location
- Portage - LV Location, HV Function
- Portage - LV Location, VLV Function
- Portage - High Hazard
- Potential Launch - HV Location, MV Function
- Potential Launch - HV Location, LV Function
- Potential Launch - HV Location, VLV Function
- Potential Launch - LV Location, LV Function
- Bridge - High Hazard
- Bridge - Medium Hazard
- Bridge - Low Hazard
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- Tree - Very High Hazard
- Tree - High Hazard
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- Sign

HV = High Value
MV = Medium Value
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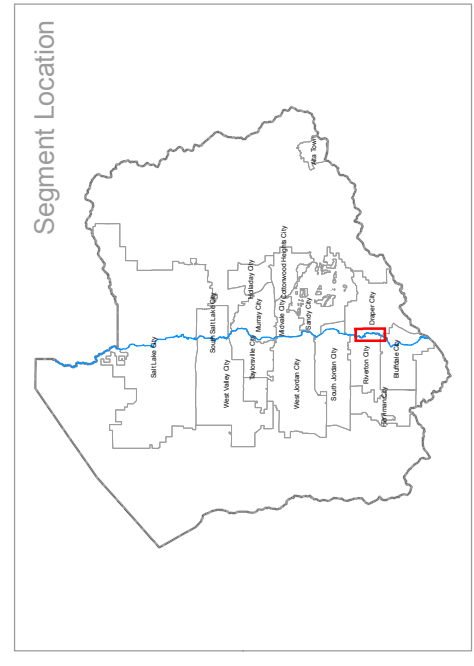
Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

Waypoint locations have been modified for readability.

Segment 3: Bluffdale/Riverton

Draper

Bangerter Highway to Vahe Street



4.4 Segment 4: Riverton/Draper/South Jordan (Vahe Street to Warm Creek Road) (GSL segments 6 and 7)

The Urban Water Trail continues north past waypoint 6-10 (high hazard) where a fallen dead tree covers around half of the channel, and waypoint 6-12 (low hazard) where flood debris is found in the middle of the channel. The river is wide at this point, but the pile is large. Further north, near the Draper and South Jordan City boundaries at waypoint 6-13 (medium hazard), one large tree cross the current and another tree lies parallel to current, blocking access. Just downstream, more trees block the whole river at waypoint 6-14 (very high hazard). As the river swings around the north side of Riverfront Park, a fallen tree lies across the channel at waypoint 6-15 (high hazard), near 11000 South in South Jordan.

Boaters proceed north past waypoint 6-16 (zero hazard), the Willow Creek confluence, to waypoint 6-17, a potential launch at 10600 South. This site was rated with very low functionality, and a high value location, but a launch site at Shield's Lane would be more appropriate considering the potential launch site at 9000. The river then flows under waypoint 6-18 (zero hazard), the 10600 South bridge, and through flood debris located east of Mulligan's Golf at waypoint 7-01 (high hazard).



Figure 4.4.1 Fallen dead tree in channel (WP 6-10)



Figure 4.4.2 Flood debris in middle of channel (WP 6-12)



Figure 4.4.3 Large tree in current blocking access (WP 6-13)



Figure 4.4.4 Tree blocking whole river (WP 6-14)



Figure 4.4.5 Fallen tree across channel at 11000 South (WP 6-15)



Figure 4.4.6 Willow Creek Confluence (WP 6-16)



Figure 4.4.7 Bridge at 10600 South, potential launch site (WP 6-17)



Figure 4.4.8 Vehicular bridge at 10600 South (WP 6-18)



Figure 4.4.9 Flood debris north of 10600 South (WP 7-01)

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Signage	4.4 B	\$500

Issues and Opportunities

- There is a potential launch site at waypoint 6-17.

Recommendations

4.4 A Launch Point (Waypoint 6-17)

A previously considered launch point at this location is not recommended. Rather consolidate launches previously considered at 10600 South and 9000 South to a location identified as waypoint 7-02a at Shields Lane (See Recommendation 4.5 A in the next section).

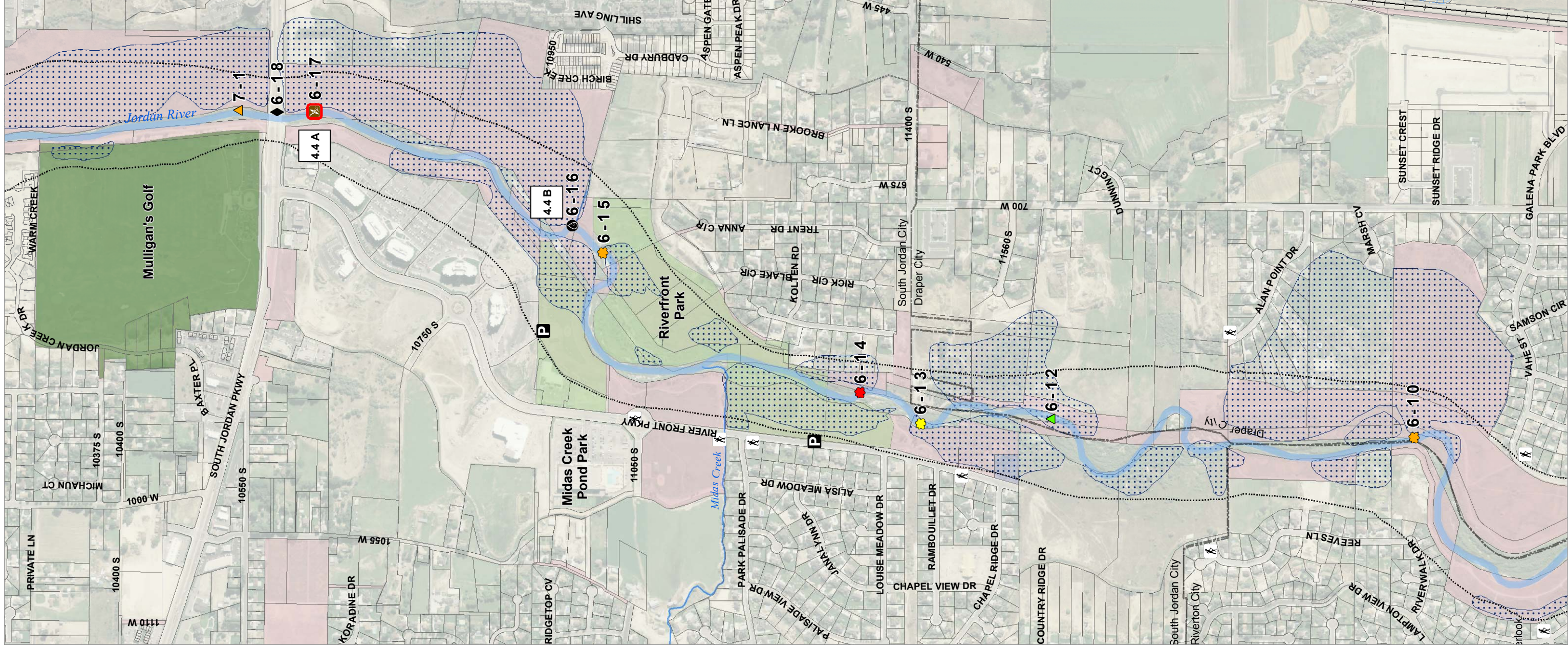
4.4 B Confluence of Willow Creek (Waypoint 6-16)

Provide a sign warning boaters of a potential change in water flow due to the confluence.

Jordan River Trail Master Plan

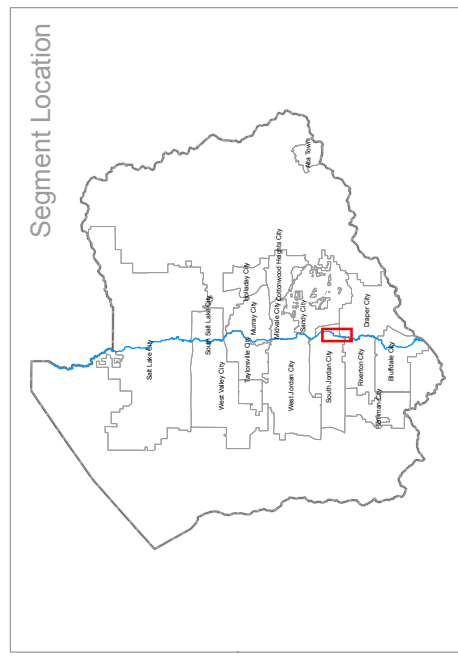
Urban Water Trail Recommendations

- Launch - HV Location, HV Function
 - Launch - HV Location, MV Function
 - Launch - HV Location, LV Function
 - Launch - MV Location, MV Function
 - Launch - MV Location, LV Function
 - Launch - LV Location, LV Function
 - Launch - VLV Location, LV Function
 - Portage - HV Location, LV Function
 - Portage - HV Location, VLV Location
 - Portage - LV Location, HV Function
 - Portage - LV Location, VLV Function
 - Portage - High Hazard
 - Potential Launch - HV Location, MV Function
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 - Potential Launch - LV Location, LV Function
 - Bridge - High Hazard
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 - Rapid - No Hazard
 - Tributary - No Hazard
 - Tree - Very High Hazard
 - Tree - High Hazard
 - Tree - Medium Hazard
 - Tree - Low Hazard
 - Sign
- Equestrian Trailhead
 Formal Trailhead
 Informal Trailhead
 Trail Access Point
 Parking - Formal Paved
 Parking - Informal Unpaved
 Restrooms
 Highways
 City Boundary
 Jordan River
 Streams
 Canals
 Light Rail
 Railroad
 County Boundary
 Meander Corridor
 Wetlands (WAIDS 2007)
 Golf Course
 Park
 Publicly Owned Land
 Utility Owned Land
- HV = High Value
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- Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.
 Waypoint locations have been modified for readability.



Segment 4: Riverton/Draper/ South Jordan

Valle Street to Warm Creek Road



4.5 Segment 5: South Jordan/ Sandy/West Jordan/Midvale (Warm Creek Road to 8450 South) (GSL segments 7 and 8)

Boaters then travel north under Shield's Lane (10000 South), past waypoint 7-02 (zero hazard) where a sewer drain pipe crosses the river underground near the southern end of the River Oaks Golf Course. Waypoint 7-2a, created during this planning process, would be a more appropriate launch site that could be developed instead of launches at waypoints 6-17 at 10600 South and 7-13 at 9000 South. About 100 yards downstream of the sewer line, water trail users encounter some flood debris in the outer edge of the channel at waypoint 7-03 (low hazard). The river then flows past waypoint 7-04, a warning sign informing boaters that they are approaching a dangerous area and that all boats must exit. This warning of the 9600 South dam has high functionality and a high value location. A little further north, another sign at waypoint 7-05 warns users to "Stop: Exit Here". This sign has medium functionality and a medium value location. A final sign at 7-06 warns that no canoes are allowed in this area. This sign has medium functionality and a medium value location.

Water trail users must exit at waypoint 7-07, the portage for the 9600 South dam. This portage has low functionality, but is a high value location. The river flows through waypoint 7-08 (very high hazard), the 9600 South irrigation dam, which users must portage around and then re-enter at waypoint 7-09. This portage on the north side of 9600 South dam has low value functionality, and a high value location, the County should coordinate with the River Oaks Golf Course to improve the quality of these portage points. The current re-entry portage has steep banks. The water trail then continues north past two footbridges in the River Oaks Golf Course at waypoints 7-10 (zero hazard) and 7-11 (zero hazard) and under the 9000 South bridge at waypoint 7-12 (zero hazard).

There is a potential launch site at the 9000 South bridge, waypoint 7-13 that was originally designated with very low value functionality and a high value location, but this launch could be consolidated along with waypoint 6-17 to one launch location at Shield's Lane. Downstream of



Figure 4.5.1 Flood debris in outer channel (WP 7-03)

the bridge, there is a rapid at waypoint 8-01 (zero hazard) and a waterfall at waypoint 8-01a (high hazard), north of the 9000 South substation. This waterfall is created by a sewer pipe. The right side of the channel is more passable - the left side is blocked, and rocks divert boats into the trees.



Figure 4.5.2 Warning sign "dangerous area" (WP 7-04)



Figure 4.5.3 Warning sign "exit here" (WP 7-05)



Figure 4.5.4 Warning sign "no canoes" (WP 7-06)



Figure 4.5.5 Portage at 9600 South (WP 7-07)



Figure 4.5.6 Irrigation dam at 9600 South (WP 7-08)



Figure 4.5.7 Launch north side of 9600 South dam (WP 7-09)



Figure 4.5.8 River Oaks Golf Course footbridge (WP 7-10)



Figure 4.5.9 River Oaks Golf Course footbridge (WP 7-11)



Figure 4.5.10 Vehicular bridge at 9000 South (WP 7-12)



Figure 4.5.11 Waterfall created by sewer pipe (WP 8-01A)

Issues and Opportunities

- The existing portage at waypoint 7-07 is an important location, but it has poor functionality.
- The existing launch at waypoint 7-09 is also important, and has poor functionality.
- There is a potential launch at waypoint 7-13, near 9000 south that is important to the water trail corridor.
- A sewer pipe at waypoint 8-01a creates a dangerous situation for boaters.

Recommendations

4.5 A Launch at Shields Lane (Waypoint 7-02a)

Provide a launch and parking in this location to replace launches previously considered at 10600 South and 9000 South.

4.5 B Portage for the 9600 South Dam (Waypoint 7-09)

Redesign and reconstruct this highly needed portage allowing portage (takeout and launch) around the dam. Coordination with the canal company and golf course will be needed.

4.5 C Sewer Pipe (Waypoint 8-01a)

This sewer pipe should be lowered or relocated so that it does not create hazardous rapids/waterfalls.

Construction Costs

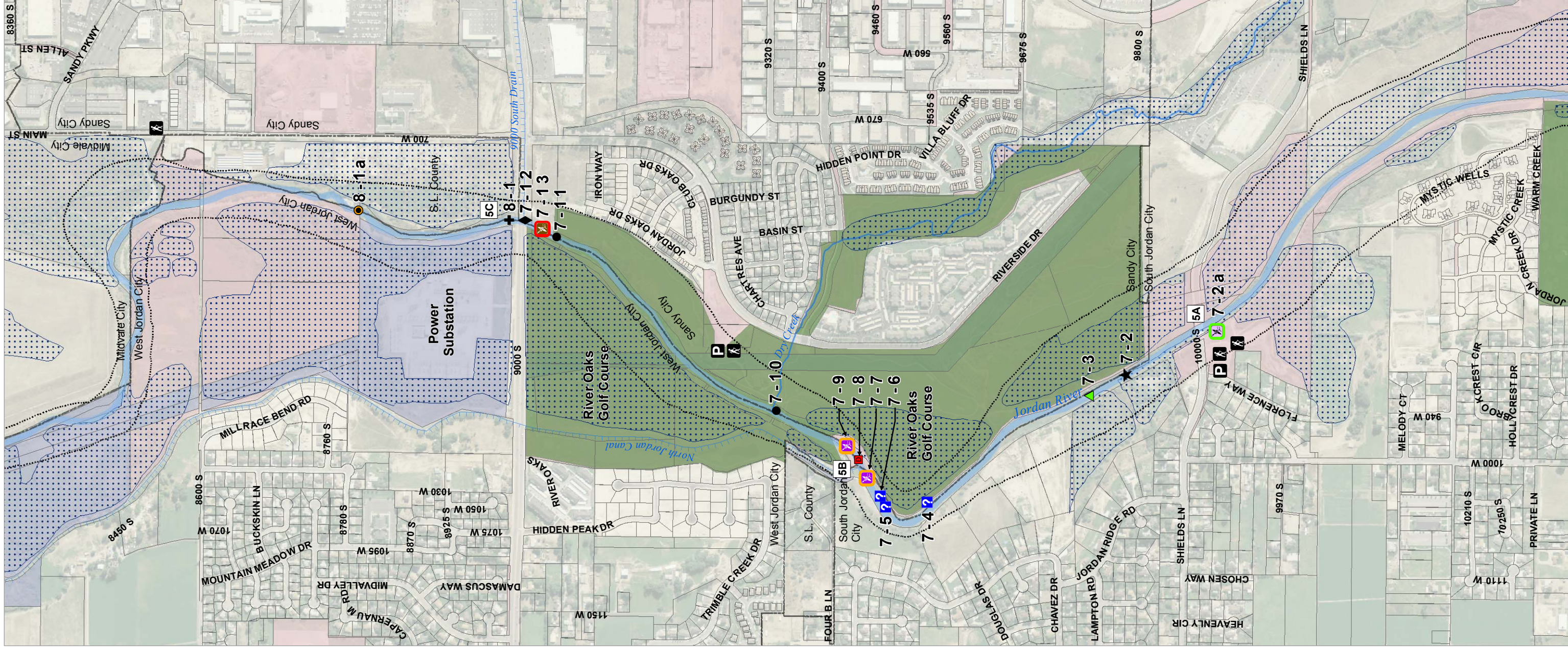
Table 4.5.1- Construction Costs: Water Trail Segment 5		
Feature Category	Map Code	CIP Budget Implication
Launch/Trailhead	4.5 A	\$185,000
Portage	4.5 B	\$5,000
Hazard Removal	4.5 C	

Jordan River Trail Master Plan

Urban Water Trail Recommendations

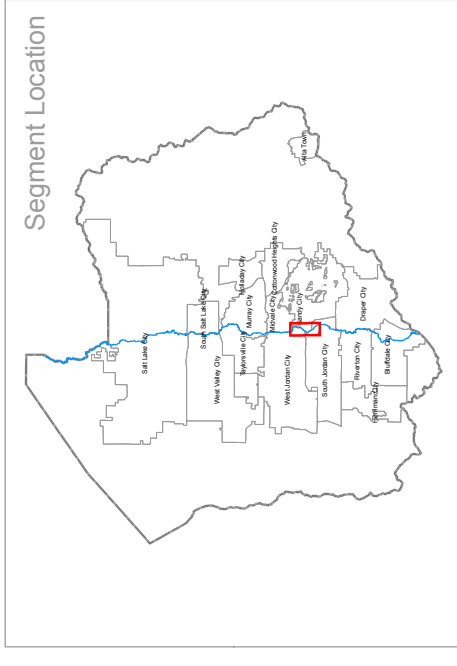
Recommendations

- Launch - HV Location, HV Function
 - Launch - HV Location, MV Function
 - Launch - HV Location, LV Function
 - Launch - MV Location, MV Function
 - Launch - MV Location, LV Function
 - Launch - VLV Location, LV Function
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 - Footbridge - Medium Hazard
 - Footbridge - Low Hazard
 - Footbridge - No Hazard
 - Railroad Bridge - High Hazard
 - Railroad Bridge - Medium Hazard
 - Railroad Bridge - No Hazard
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 - Tree - High Hazard
 - Tree - Medium Hazard
 - Tree - Low Hazard
 - Sign
- Equestrian Trailhead
 Formal Trailhead
 Informal Trailhead
 Trail Access Point
 Parking - Formal Paved
 Parking - Informal Unpaved
 Restrooms
 Highways
 City Boundary
 Jordan River
 Streams
 Canals
 Light Rail
 Railroad
 County Boundary
 Meander Corridor
 Wetlands (WAIDS 2007)
 Golf Course
 Park
 Publicly Owned Land
 Utility Owned Land
- HV = High Value
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- Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.
 Waypoint locations have been modified for readability.



Segment 5: South Jordan/Sandy/ West Jordan/Midvale

Warm Creek Road to 8450 South



4.6 Segment 6: West Jordan/Midvale/Murray/Taylorsville (8450 South to Winchester Street) (GSL segments 8 and 9)

Waypoint 8-02 (zero hazard) is at the Bingham Creek and Plumb Creek confluence, and waypoint 8-03 (zero hazard) is an historic bridge in West Jordan. Boaters then encounter a fiber optic line at waypoint 8-04 (high hazard) just south of the 7800 South bridge, waypoint 8-05 (zero hazard). The river passes through more dangerous hazards at waypoint 8-06 (very high hazard), the Midvale slag millrace and waypoint 8-07 (high hazard), and the railroad crossing. The millrace will be removed with redevelopment of the Sharon Steel and Bingham Junction sites. The railroad crossing is a high hazard due to the collection of flood debris.

The trail continues past waypoint 8-08 (low hazard) where the sewage plant discharges into the river at approximately 7400 South. The level of this hazard increases with the amount of flow discharged by the sewage plant, and can catch boaters off guard.

Nearing the 7200 South Trailhead, there is a potential launch site at waypoint 8-09 with low functionality and a low value location. The river flows under waypoint 8-10 (zero hazard), the 7200 South bridge, and shortly after entering Murray City boundaries trail users encounter waypoint 8-11, a warning sign stating "All Boats Exit". This sign, south of the Winchester Street bridge has high functionality and a high value location. Boaters must portage at waypoint 8-12, which has low functionality and a high value location, with a long connection around the hazards north of Winchester Street. This portage is too steep and is heavily vegetated, making access difficult. There are more warning signs at waypoint 9-01a. The river proceeds north under waypoint 9-01 (high hazard), the 6400 South vehicular bridge. The hydraulics under this bridge can suck boaters in and send them into hazards downstream. There are major rapids at waypoint 9-02 (very high hazard) where water and sewer pipelines cross the river, and at waypoint 9-03 (high hazard). An overhead pipeline for a Qwest cable crossing is located at waypoint 9-04 (zero hazard).

Continuing north near Winchester Park, there is a potential launch site and portage at 9-05 (also evaluated as waypoint 8-13 in GSL/NPS study), with low functionality and a high value location. At waypoint 9-06 there is an existing launch with three launch decks. This site has low functionality and a low value location because of the long distance from the upstream portage (waypoint 8-12) and to the parking area.



Figure 4.6.1 Brigham Creek Confluence (WP 8-02)



Figure 4.6.2 West Jordan Historic Bridge (WP 8-03)



Figure 4.6.3 Qwest fiber-optic pipeline before bridge (WP 8-04)



Figure 4.6.10 Portage, sign (WP 8-11)



Figure 4.6.11 Potential launch site at 6400 South (WP 8-12)



Figure 4.6.4 Vehicular bridge at 7800 South (WP 8-05)



Figure 4.6.5 Midvale slag millrace (WP 8-06)



Figure 4.6.6 Railroad crossing bridge (WP 8-07)



Figure 4.6.7 Sewage plant discharge at 7400 South (WP 8-08)



Figure 4.6.8 Potential launch site at 7200 South (WP 8-09)



Figure 4.6.9 Warning sign "all boats exit" (WP 8-10)

Issues and Opportunities

- A fiber optic line at waypoint 8-08 creates a high hazard for water trail users.
- Waypoint 8-09 is not an important location for a launch and is not very functional.
- The existing portage at 8-12 is important to the trail corridor but has poor functionality.
- Need to have barrels near portage 8-12 because of dangers north of Winchester bridge.
- There is a good potential launch site at waypoint 9-05 that would be closer to portage 8-12 than the launch at 9-06.
- The launch at 9-06 should be abandoned for water trail use, and adapted for use with the paved, multi-purpose trail as overlooks and/or fishing docks if appropriate.

Recommendations

4.6 A Fiber Optic Line (Waypoint 8-04)

The fiber optic line that crosses in the river creates changes in hydraulics that are dangerous. This hazard should be signed, indicating that changes in water flow affect conditions in the area.

4.6 B Midvale Slag Millrace and Railroad Crossing (Waypoint 8-06)

This area is typically high hazard because of flood debris that accumulates and affects water flows and protruding metal pieces. These structures will be removed with redevelopment in the area; however, checking and cleaning the channel regularly is recommended.

Providing signing in this area indicating that water may be contaminated due to soil pollutants.

4.6 C TRAX Bridge Reconstruction (Waypoint 8-07)

The railroad bridge will be reconstructed for expanded TRAX development. Coordinate with UTA to remove any obstructions in the river.

4.6 D Hazardous Conditions (Waypoint 8-08)

Provide signing to warn boaters of river conditions which force boaters

against the banks.

4.6 E Launch Near the 7200 South Trailhead (Waypoint 8-09)

A launch is not recommended in this location.

4.6 F Portage Under 6400 South/Winchester St. (Waypoints 8-09 and 9-06)

This portage exits on the south side of the street, crosses under the 6400 South bridge through a tunnel, and launches at waypoint 9-5. The portage is needed to avoid dangerous conditions under the bridge. Provide barrels prohibiting boaters from going under the bridge and forcing them to use the portage.

Portages/launches at wayside 8-09 and 9-06 are not recommended or needed. The launch at 9-06 could be redesigned and adapted for users on the paved trail as an overlook and/or fishing dock.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Signage	4.6 A	\$500
Signage	4.6 B	\$500
Hazard Removal	4.6 C	
Signage	4.6 D	\$1,000
Launch	4.6 E	N/A
Safety Device, Launch/ Portage Consolidation	4.6 F	\$15,000

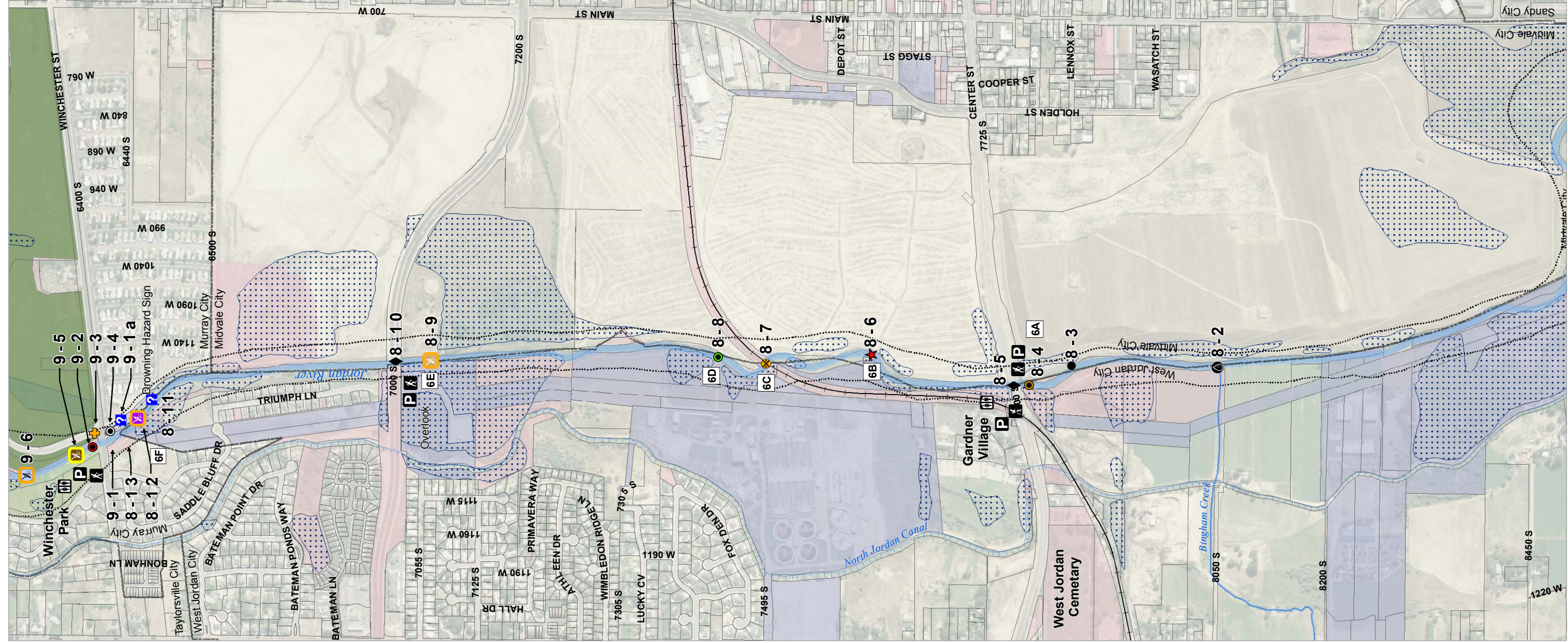
Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
- Launch - HV Location, MV Function
- Launch - HV Location, LV Function
- Launch - MV Location, VLV Function
- Launch - MV Location, MV Function
- Launch - LV Location, LV Function
- Launch - VLV Location, LV Function
- Portage - HV Location, HV Function
- Portage - LV Location, HV Function
- Portage - LV Location, VLV Function
- Portage - High Hazard
- Potential Launch - HV Location, MV Function
- Potential Launch - HV Location, LV Function
- Potential Launch - HV Location, VLV Function
- Potential Launch - LV Location, LV Function
- Bridge - High Hazard
- Bridge - Medium Hazard
- Bridge - Low Hazard
- Bridge - No Hazard
- Footbridge - Very High Hazard
- Footbridge - Medium Hazard
- Footbridge - Low Hazard
- Footbridge - No Hazard
- Railroad Bridge - High Hazard
- Railroad Bridge - Medium Hazard
- Railroad Bridge - No Hazard
- Dam - Very High Hazard
- Dam - High Hazard
- Dam - Medium Hazard
- Dam - Low Hazard
- Weir - Very High Hazard
- Weir - High Hazard
- Flood Debris - Very High Hazard
- Flood Debris - High Hazard
- Flood Debris - Medium Hazard
- Flood Debris - Low Hazard
- Pipe - Very High Hazard
- Pipe - High Hazard
- Pipe - Medium Hazard
- Pipe - Low Hazard
- Pipe - No Hazard
- Cultural Element - Low Hazard
- Cultural Element - No Hazard
- Miscellaneous Debris - High Hazard
- Miscellaneous Debris - Medium Hazard
- Miscellaneous Debris - Low Hazard
- Miscellaneous Structure - Very High Hazard
- Miscellaneous Structure - High Hazard
- Miscellaneous Structure - Medium Hazard
- Miscellaneous Structure - Low Hazard
- Miscellaneous Structure - No Hazard
- Rapid - High Hazard
- Rapid - Medium Hazard
- Rapid - Low Hazard
- Rapid - No Hazard
- Tributary - No Hazard
- Tree - Very High Hazard
- Tree - High Hazard
- Tree - Medium Hazard
- Tree - Low Hazard
- Sign

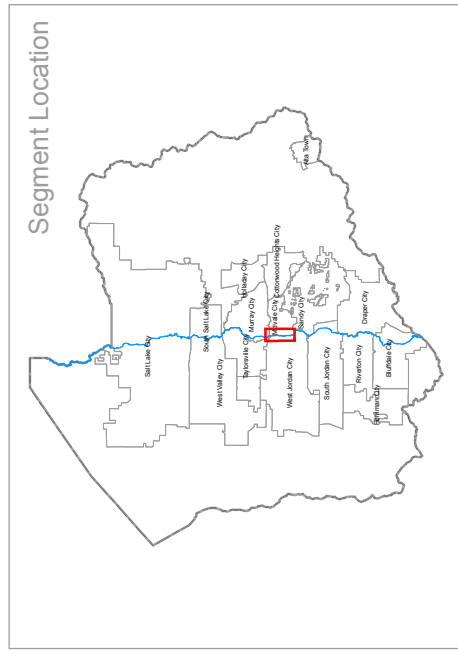
HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value

Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001. Waypoint locations have been modified for readability.



Segment 6: West Jordan/Midvale/ Murray/Taylorsville

8450 South to Winchester Street



4.7 Segment 7: Murray/Taylorsville (Winchester Street to Murray Taylorsville Road) (GSL segment 9)

Recreational boaters travel north past Winchester Park and the Murray Parkway Golf Course, under a footbridge at waypoint 9-07 (zero hazard) and under the Interstate 215 vehicular bridge at waypoint 9-08 (zero hazard). North of Interstate 215, just west of Cottonwood Grove Park, there is an existing launch site at waypoint 9-09. This site has community access boat ramp potential, with low existing functionality and a low value location because of the close proximity to the existing launch at Winchester Street. The trail then flows under the Bullion Street vehicular bridge, waypoint 9-10 (zero hazard), and under a footbridge south of 5400 south at waypoint 9-11 (zero hazard).

Further downstream near Walden Park, there are underwater pipelines across the channel at waypoints 9-12 (medium hazard) and 9-13 (medium hazard).

The 5400 South vehicular bridge abutment has debris piling up, waypoint 9-14 (zero hazard), which could be a potential hazard in the future. North of the 5400 South bridge, there is a boat launch at waypoint 9-15 with high functionality and a high value location. As the water trail flows north and east, there is a footbridge near 5200 South at waypoint 9-16 (zero hazard) and a section near the Kennecott Environmental Center where trees have fallen into the channel and block almost half of the river, waypoint 9-17 (low hazard).



Figure 4.7.5 Potential launch site at 6400 South (WP 9-05)



Figure 4.7.6 Launch decks at 6400 South (WP 9-06)



Figure 4.7.7 Vehicular bridge at Interstate 215 (WP 9-08)



Figure 4.7.8 Potential launch site at Bouillon Street (WP 9-09)



Figure 4.7.9 Vehicular bridge at Bouillon Street (WP 9-10)



Figure 4.7.10 Vehicular bridge at 5400 South, flood debris (WP 9-14)



Figure 4.7.1 Vehicular Bridge at 6400 South (WP 9-01)



Figure 4.7.2 Pipeline, bridge at 6400 South (WP 9-02)



Figure 4.7.3 Rapids at 6400 South (WP 9-03)



Figure 4.7.4 Qwest cable crossing river (WP 9-04)



Figure 4.7.11 Boat launch at 5400 South (WP 9-15)



Figure 4.7.12 Fallen trees block half of channel near Murray Nature Center (WP 9-17)

Issues and Opportunities

- There is a good potential launch site at waypoint 9-09 that has value as a potential community launch for larger groups.
- There are underwater pipelines at waypoint 9-12 and 9-13 that are dangerous for boaters.

Recommendations

4.7 A Community Boat Launch in Murray (Waypoint 9-09)

This launch is in disrepair and is not needed. The launch should be removed.

4.7 B Underwater Pipelines (Waypoints 9-12 and 9-13)

Underwater pipelines at these locations cause disturbances in the water. Provide signing indicating that depending on water level, flow characteristics may change.

4.7 C Boat Launch at (Waypoint 9-15)

This launch has high value as a community launch point. It should be redesigned and reconstructed.

Construction Costs

Table 4.7.1– Construction Costs: Water Trail Segment 7		
Feature Category	Map Code	CIP Budget Implication
Launch Removal	4.7 A	\$5,000
Signage	4.7 B	\$500
Launch Upgrade	4.7 C	\$10,000

4.8 Segment 8: Murray/Taylorville/West Valley/South Salt Lake (Murray Taylorville Road to 3300 South) (GSL segments 9 and 10)

Little Cottonwood Creek joins the Jordan River at waypoint 9-18 (zero hazard), just south of Arrowhead Park. Just north of this confluence, there is a launch site with very low functionality and a low value location at waypoint 9-19. This launch was closed because of inappropriate use by children on bicycles. Boaters continue north under waypoint 9-20 (zero hazard), the 4800 South vehicular bridge, and past waypoint 9-20a, a potential launch site with very low functionality and a high value location. The water trail then flows under a footbridge near 4800 South, waypoint 9-21 (zero hazard) and to waypoint 9-22, a portage around the 4600 South weir, with very low functionality and a low value location. The location of this take-out for the portage is too close to the weir.

The 4600 South weir, waypoint 9-23 (very high hazard), is one of the most dangerous hazards on the Jordan River, with a drop of 5 feet at the time of the data collection. Users then launch again using the portage at waypoint 9-24, just downstream of the weir, which has very low functionality and a high value location. As the water trail continues, flood debris is piled up in the middle of the channel near the 4500 South vehicular bridge at waypoint 9-25 (zero hazard).

Big Cottonwood Creek then joins the Jordan River at waypoint 9-26 (zero hazard), east of Meadowbrook Golf Course. Near the north end of the Golf Course at waypoint 9-27 (low hazard) there is a fallen tree on Rocky Mountain Power's property that covers almost half of the river channel. As the river trail winds to the west and north towards Meadowbrook Parkway, boaters pass under waypoint 9-28 (zero hazard), a footbridge, and through rapids at waypoint 9-29 (zero hazard). Near the 3900 South vehicular bridge at waypoint 9-30 (very high hazard), flood debris has collected at the abutments, and clearance is too low during high water flow.

Just downstream near General Holm Park, waypoint 9-31, there is a potential launch site, although it is a long haul to the parking area. This site has low functionality and a high value location. Users then pass waypoint 10-01 which was rated as a medium hazard because of a van that was submerged almost 90% into the middle of the river. Most of the vehicle body has since been removed, and this is now a low hazard. The trail then takes users past flood debris at waypoints 10-02 (high hazard), which has woody debris in two clusters with lots of garbage, and waypoint 10-03 (high hazard) where some trees have fallen along the right side of the river and debris has collected in the middle.



Figure 4.8.1 Launch site at 4800 South (WP 9-19)



Figure 4.8.2 Vehicular and foot bridges at 4800 South (WP 9-20)



Figure 4.8.9 Potential launch site at 3900 South (WP 9-31)



Figure 4.8.10 Van submerged in middle of river (WP 10-01)



Figure 4.8.3 Portage near 4600 South dam (WP 9-22)



Figure 4.8.4 Weir at 4600 South (WP 9-23)



Figure 4.8.11 Wood debris in river, lots of garbage (WP 10-02)



Figure 4.8.12 Trees on right side of river, debris in middle of river (WP 10-03)



Figure 4.8.5 Launch at 4600 South (WP 9-24)



Figure 4.8.6 Flood debris at 4500 South bridge (WP 9-25)



Figure 4.8.7 Fallen tree covers half of channel (WP 9-27)



Figure 4.8.8 Flood debris at vehicular bridge at 3900 South (WP 9-30)

Issues and Opportunities

- The existing launch site at waypoint 9-19 in Arrow Park that has been abandoned. This site is important to the urban water trail corridor and needs to be restored to a functioning launch.
- There is a potential launch site at waypoint 9-20a. The existing portage at waypoint 9-22 is in a poor location and has poor functionality. There needs to be a safe portage in this area to get users around the 4600 South weir.
- The existing portage south of the 4600 South weir is in a good location but is not safely functional.
- North of General Holm Park, at waypoint 9-31 is a potential launch site. It has a high value location but is a long distance from parking.

Recommendations

4.8 A Existing Boat Launch (Waypoint 9-19)

Remove this boat launch, and move to waypoint 9-20a. This is a highly desirable location; design and construct a new launch waypoint 9-20a. This locations also functions as a logical end-point for boaters who do not want to portage at the 4600 South Weir.

4.8 B Portage at 4600 South Weir (Waypoints 9-22 and 9-24)

Provide barrels prohibiting boaters from encountering highly dangerous conditions and forcing them to use the portage/launch at waypoints 9-22 and 9-24.

4.8 C Hazardous Water Conditions at 3900 South (Waypoints 9-28a, 9-30, and 9-31)

To avoid this dangerous condition, develop a portage (waypoint 9-28a) which allows boaters to exit the river, use the proposed sky bridge over 3900 South, and re-access the water at the launch at waypoint 9-31.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Launch Removal	4.8 A	\$5,000
Safety Device	4.8 B	\$5,000
Portage	4.8 C	\$10,000

4.9 Segment 9: West Valley/South Salt Lake/Salt Lake City (3300 South to 1700 South) (GSL segments 10 and 11)

The water trail continues almost a mile north to an existing launch at waypoint 10-04, near 3300 South on the right side. This launch has low functionality and a medium value location. The distance from the launch to the parking lot is somewhat long, and the existing platforms are difficult to use, especially at high water flows. At waypoint 10-05 (zero hazard), the 3300 South vehicular underpass, flood debris has built up at the bridge abutment.

Recreational boaters travel north, winding past the Oxbow Jail and Oxbow Trailhead, and under waypoint 10-06 (zero hazard), a footbridge. The river flows north past the Redwood Nature Area, where Mill Creek joins the Jordan River from the east at waypoint 10-07 (zero hazard), as does the Decker Lake outflow tributary at waypoint 10-08 (zero hazard) from the west. Downstream near Redwood Trailhead Park and Paul Workman Ball Park, there is an existing boat launch at waypoint 10-09 with three launches for three different levels of water. This launch has high functionality and a high value location, although the distance from parking is somewhat long, and the platforms can be difficult to access depending on the water level. The river then passes under waypoint 10-10 (zero hazard), the SR-201 vehicular bridge.

As users travel north to waypoint 10-11 (low hazard), a lot of woody debris has built up at the front of 2100 South vehicular bridge pillars. There is a sign at waypoint 10-11a, warning users to exit before they approach the Surplus Canal weir and diversion dam. This warning sign, near 2100 South, has medium functionality and a medium value location. North of the 2100 South vehicular bridge at waypoint 10-12 (zero hazard), a pipeline crosses the river at the south end of the Glendale Golf Course. Boaters must portage around waypoint 10-13 (medium hazard), the diversion dam for the Surplus Canal. There is a portage at waypoint 10-14 on the right side of the Jordan River, which is gravel with no path. This portage has low functionality and a high value location. Waypoint 10-15 (high hazard), next to the diversion dam, is the Surplus Canal weir which spans the entire width of river. The portage for users to put-in after the dam and weir is located at waypoint 11-01 on the left side of the river, which is a semi-steep gravel trail. This site has low functionality and a high value location. A more appropriate site for water users wishing to start a trip in this area is at 1700 South.

The river trail then continues north past waypoint 11-02 (low hazard), a tree that is covering 50% of river on the right side, and under waypoint 11-03 (zero hazard), a footbridge. Boaters then pass waypoint 11-04 (medium hazard), where overhanging trees narrow the access along both sides of the river banks and pass under waypoint 11-05 (low hazard) where an overhead pipe with protective screening that could be hazardous in high water crosses the river. The river

flows through overhanging trees with a lot of snags at waypoint 11-06 (high hazard), which extends for about 30 yards on both sides of the river's banks. As the river trail approaches the low railroad bridge at waypoint 11-07 (high hazard), they will encounter flood debris that has collected at base, and must then pass through the low 4 feet clearance under the bridge.



Figure 4.9.1 Boat launch at 3300 South (WP 10-04)



Figure 4.9.2 Vehicular bridge at 3300 South, built up debris (WP 10-05)



Figure 4.9.3 Boat launch at 2350 South (WP 10-09)



Figure 4.9.4 Bridge at SR-201 (WP 10-10)



Figure 4.9.5 Woody debris in front of pillars (WP 10-11)



Figure 4.9.6 Warning sign at 2100 South (WP 10-11A)



Figure 4.9.7 Pipeline across river near 2100 South (WP 10-12)



Figure 4.9.8 Diversion dam at 2100 South downstream (WP 10-13)



Figure 4.9.9 Diversion dam for surplus canal at 2100 South (WP 10-13)



Figure 4.9.10 Surplus dam, gravel with no path (WP 10-14)



Figure 4.9.11 Weir at surplus canal at 2100 South (WP 10-16)



Figure 4.9.12 Portage, potential launch site, near 2100 South (WP 11-01)



Figure 4.9.13 Tree covering half of the river (WP 11-02)



Figure 4.9.14 Trees hanging over river, narrow access on both sides (WP 11-04)



Figure 4.9.15 Trees overhanging the river, 30 yards on both sides (WP 11-06)



Figure 4.9.16 Debris collected at base of railroad bridge (WP 11-07)

Construction Costs

Table 4.9.1– Construction Costs: Water Trail Segment 9		
Feature Category	Map Code	CIP Budget Implication
Launch Upgrade	4.9 A	\$10,000
Launch Upgrade	4.9 B	\$10,000
Portage	4.9 C	\$10,000
Signage	4.9 D	\$500

Issues and Opportunities

- The existing launch at waypoint 10-04 has a decent location along the river trail, but not as significant as others. The functionality is low.
- The existing portage at waypoint 10-14 is important to allow boaters to avoid the Surplus Canal weir and dam, but has low functionality.
- There is a potential launch site for users north of the dam and weir at waypoint 11-01.
- The railroad bridge at waypoint 11-07 is too low.

Recommendations

4.9 A Launch at 3300 South (Waypoint 10-04)

Redesign and reconstruct this launch.

4.9 B Launch (Waypoint 10-09)

Redesign and reconstruct this launch to provide better accessibility and functionality at different water levels.

4.9 C Portage (Waypoint 10-14)

Redesign and reconstruct this portage at waypoint 10-14 and launch at 11-01 to avoid the diversion dam for the Surplus Canal.

4.9 D Portage (Waypoint 11-07)

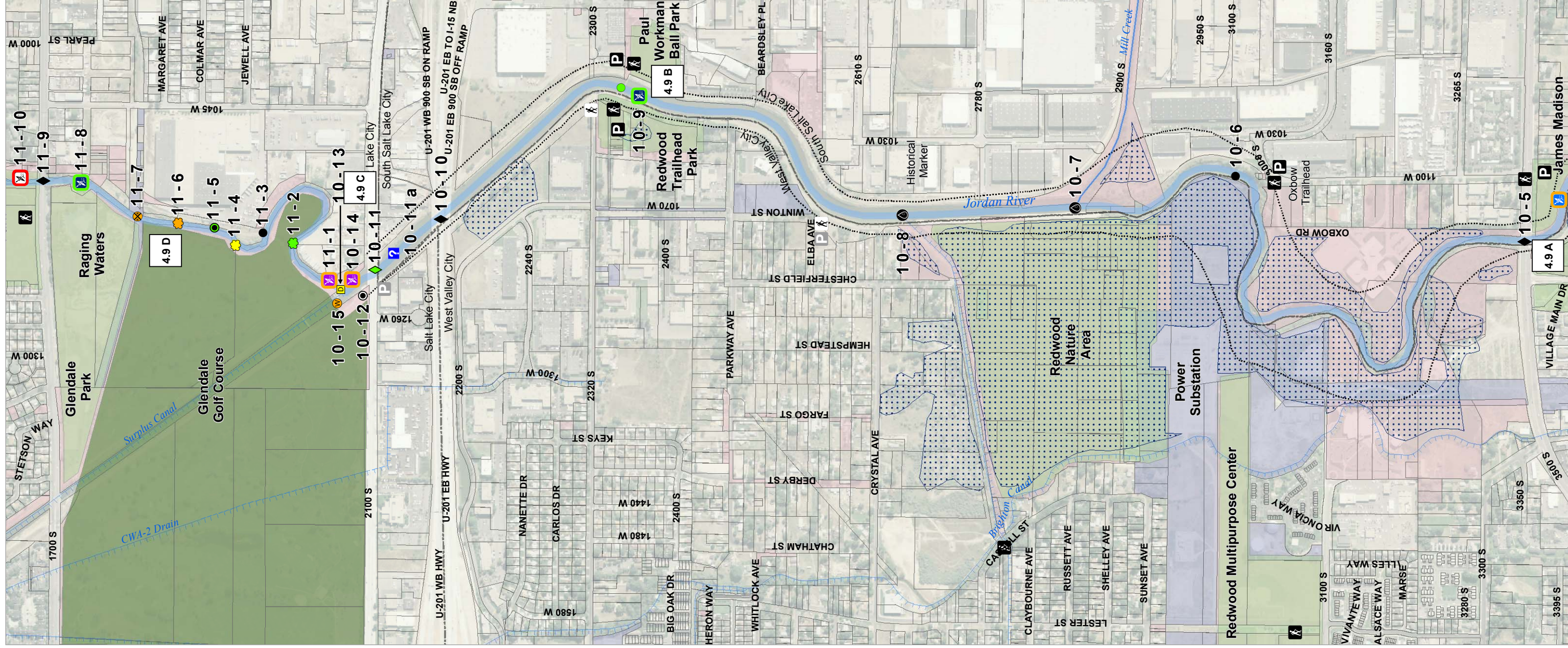
A low railroad bridge at waypoint 11-07 presents a hazard. Provide a sign indicating the up-coming hazard suggesting that boaters portage at 10-14, follow the paved trail north to the launch at Raging Waters (waypoint 11-8) to continue on, or end their trip at this point.

Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
- Launch - HV Location, MV Function
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- Portage - HV Location, VLV Function
- Portage - LV Location, HV Function
- Portage - LV Location, VLV Function
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- Potential Launch - HV Location, MV Function
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- Potential Launch - HV Location, VLV Function
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- Pipe - Medium Hazard
- Pipe - Low Hazard
- Pipe - No Hazard
- Cultural Element - Low Hazard
- Cultural Element - No Hazard
- Miscellaneous Debris - High Hazard
- Miscellaneous Debris - Medium Hazard
- Miscellaneous Debris - Low Hazard
- Miscellaneous Structure - Very High Hazard
- Miscellaneous Structure - High Hazard
- Miscellaneous Structure - Medium Hazard
- Miscellaneous Structure - Low Hazard
- Miscellaneous Structure - No Hazard
- Rapid - High Hazard
- Rapid - Medium Hazard
- Rapid - Low Hazard
- Rapid - No Hazard
- Tributary - No Hazard
- Tree - Very High Hazard
- Tree - High Hazard
- Tree - Medium Hazard
- Tree - Low Hazard
- Sign

HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value



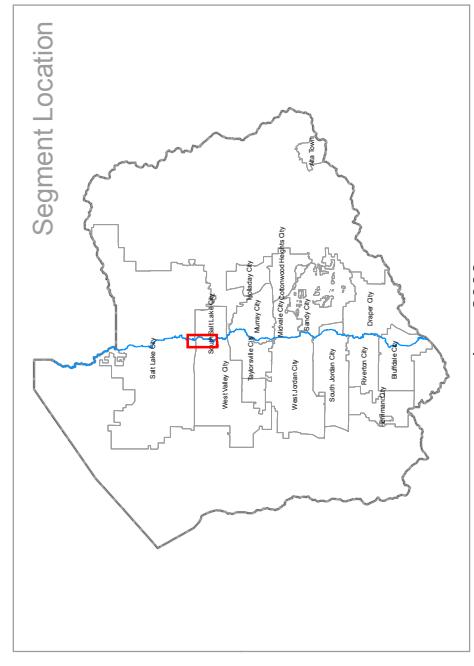
Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

Waypoint locations have been modified for readability.

*No data available north of Surplus Canal.

Segment 9: West Valley/South Salt Lake/Salt Lake City

3300 South to 1700 South



4.10 Segment 10: Salt Lake City (1700 South to Interstate 80) (GSL segments 11 and 12)

The river continues north to waypoint 11-08 with a launch at Raging Waters just south of 1700 South. This launch has high functionality and a high value location, and is the preferred design for a water launch, especially for larger groups of people and those with special needs. Watercraft users then proceed north under the 1700 South vehicular bridge, waypoint 11-09 (zero hazard). Immediately north of 1700 South, there used to be a launch on the left side of the river at - waypoint 11-10 in Glendale Park, which has been removed since the 2000 study. At this location, there is also a storm water lift station, which releases flow into the river after storm events.

As boaters continue north through Glendale park, they encounter overhanging trees on the left side of the river at waypoint 11-11 (low hazard), and pass under a footbridge at the Peace Trees site, waypoint 11-12 (zero hazard). Waypoint 11-12a, created during this planning process, is a small rapid created by a pipeline just north of the footbridge at waypoint 11-12. Waypoint 11-12b The river flows under a footbridge at waypoint 11-13 (zero hazard) just south of 1300 South, then under the 1300 South vehicular bridge with an adjacent pipeline, waypoint 11-14 (medium hazard). The river passes a small neighborhood park and flows to the east, passing a tributary and storm drain at waypoint 11-15 (zero hazard), then bends back to the north, where a fallen tree at waypoint 11-16 has been removed since the GSL study. Waypoint 11-16a, created as part of this planning process, is a new launch that was installed after the data gathering. This launch, also called the Modesto launch, is a floating dock system and functions well.

The trail continues under a footbridge on 1100 South at waypoint 11-17 (zero hazard), and past some buildup of flood debris at the base of the Fremont Avenue bridge at waypoint 11-18 (low hazard). As the Jordan River flows north through Jordan Park, it passes under waypoint 11-19 (zero hazard), a footbridge crossing the river, and skirts around waypoint 11-20, a large wooden dock that sticks out into the water. The dock is used for viewing and fishing from the east bank

of the river and has zero functionality and zero value location for the urban water trail. River trail users then bend around the north side of Jordan Park, passing waypoint 11-21 (low hazard) where a tree has fallen along right side of river.

As boaters approach the 900 South railroad bridge, they encounter a lot of debris at the foot of the bridge, which covers half of the channel under the bridge, waypoint 11-22 (medium hazard). Waypoint 11-22a (low hazard), created as part of this planning process, is a footbridge that was installed after the 2000 study. North of the Ninth South Park, there is a boat launch at waypoint 11-23 with low functionality and a medium value location. The parking lot is too far away from the launch point in this location. The trail then flows under waypoint 11-24 (zero hazard), the vehicular bridge at Indiana Avenue (800 South), and past a tributary entry point at waypoint 11-25 (zero hazard). Boaters then pass under waypoint 11-26 (zero hazard), a good example of a high bridge, and under several more bridges at waypoints 11-27 (zero hazard), the 700 South vehicular bridge; waypoint 11-28 (low hazard), the 600 South footbridge; waypoint 11-29 (medium hazard), the 500 South bridge which has low clearance; waypoint 11-30 (low hazard), the 400 South vehicular bridge; and waypoint 11-31 (zero hazard), the 300 South vehicular bridge. Under waypoint 11-31, there is a sign which is hard to notice. This sign, waypoint 11-32 has low functionality and a low value location. It is worn out and is in a spot that is difficult for boaters to see.

The river then continues to waypoint 11-33, near 300 South at the Alzheimer's Association Wildlife Grove, which was a portage sign hidden in trees on the right side that has been removed since the GSL study. This sign has high value functionality and very low value location. The takeouts are on the right side of the river at waypoint 12-01 and have about 10 feet of gravel between the path and the concrete launch ramps. The ramps need repair and have high functionality and a high value location. Across the river from the takouts is waypoint 12-02, a tree that has been removed since the 2000 study.



Figure 4.10.5 Storm drain, tributary at 1200 South (WP 11-15)



Figure 4.10.6 Bridge at California Avenue (WP 11-16)



Figure 4.10.7 Tree fallen into river at 1200 South (WP 11-16)



Figure 4.10.8 Wood dock near Jordan Park (WP 11-20)



Figure 4.10.9 Tree in river near Matter Horn at Jordan Park (WP 11-21)



Figure 4.10.10 Railroad bridge at 900 South, debris covers half of channel (WP 11-22)



Figure 4.10.1 Launch at Raging Waters (WP 11-08)



Figure 4.10.2 Vehicular bridge at 1700 South (WP 11-09)



Figure 4.10.3 Launch left side of river at 1700 South (WP 11-10)



Figure 4.10.4 Overhanging trees on left side of river (WP 11-11)



Figure 4.10.11 Boat launch at 850 South (WP 11-23)



Figure 4.10.12 Vehicular bridge at Indiana Avenue (WP 11-24)



Figure 4.10.13 Tributary at 800 South (WP 11-25)



Figure 4.10.14 High footbridge (WP 11-26)



Figure 3.10.21 Tree across river at 300 South (WP 12-02)



Figure 4.10.15 Vehicular bridge at 700 South (WP 11-27)



Figure 4.10.16 Low bridge at 500 South (WP 11-29)



Figure 3.10.17 Vehicular bridge at 400 South (WP 11-30)



Figure 3.10.18 Vehicular bridge at 300 South (WP 11-31)



Figure 3.10.19 Warning sign on bridge, hard to notice (WP 11-32)



Figure 3.10.20 Portage sign hidden by trees at 300 South (WP 11-33)

Issues and Opportunities

- The water launch at waypoint 11-10 in Glendale Park is redundant due to the launch at Raging Waters.
- The footbridge and pipe at waypoint 11-14 present a hazard to water trail users.
- North of the Ninth South Park at waypoint 11-23, there is an existing boat launch with a decent location, though other launches are more important to the water trail corridor. The functionality of this launch is poor.
- The 600 South footbridge at waypoint 11-29 has low clearance.
- The signs at waypoints 11-32 and 11-33 are in poor locations, and are not adequate with respect to the hazard.

Recommendations

4.10 A Raging Waters Launch (Waypoint 11-08)

This launch is considered highly functional and in a good location; it is the best example of a launch in the river corridor and should be considered an example. It is suitable for larger groups and for persons with special needs.

4.10 B Pipeline (Waypoint 11-09)

The pipeline in the river creates rapids. Sign the to warn boaters that depending on water levels, water characteristics may change. This condition continues to waypoint 11-10 where a storm water lift station creates changing water conditions depending on water levels.

4.10 C Pipeline (Waypoint 11-12)

The pipeline in the river creates rapids. Sign to warn boaters that depending on water levels, water characteristics may change.

4.10 D Parley's Creek, Red Butte Creek, Emigration Creek Drainage (Waypoint 11-15)

This confluence creates some changes in water characteristics depending on flows.

Potential interpretive opportunity as well.

4.10 E 900 South Rail Road Bridge (Waypoint 11-22)

When the rail road tracks on 900 South are abandoned in 2008, this

bridge should be removed and all elements of the bridge in the river should also be removed.

4.10 F Removed Launch (Waypoint 11-23)

This launch has been removed and had limited value to the water trail corridor except as an interim rest stop/comfort station. Design and construct a small ramp that provides access to facilities.

4.10 G Footbridge at 600 South (Waypoint 11-29)

Replace and redesign this footbridge, as it has a low clearance.

4.10 H Warning Signs Prior to the Canoe Chute (Waypoint 11-31)

The last opportunity to exit the river prior to the Canoe Chute, which is not suited to novice boaters, needs to be signed more often and in more visible locations. Existing signs need to be redone, relocated, and separated to provide plenty of warning to boaters. Warning signs should be of a consistent design, clearly identifying them as warning.

4.10 I Boat Takeout at 300 South (Waypoint 12-01)

Repair and upgrade this boat takeout, which is highly desired and appropriately located.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Standards	4.10 A	N/A
Signage	4.10 B	\$500
Signage	4.10 C	\$500
Signage	4.10 D	\$500
Hazard Removal	4.10 E	N/A
Access	4.10 F	\$5,000
Bridge	4.10 G	\$100,000
Signage	4.10 H	\$1,500
Launch Upgrade	4.10 I	\$10,000

Jordan River Trail Master Plan

Urban Water Trail Recommendations

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- Weir - Very High Hazard
- Weir - High Hazard
- Flood Debris - Very High Hazard
- Flood Debris - High Hazard
- Flood Debris - Medium Hazard
- Flood Debris - Low Hazard
- Pipe - Very High Hazard
- Pipe - High Hazard
- Pipe - Medium Hazard
- Pipe - Low Hazard
- Pipe - No Hazard
- Cultural Element - Low Hazard
- Cultural Element - No Hazard
- Miscellaneous Debris - High Hazard
- Miscellaneous Debris - Medium Hazard
- Miscellaneous Debris - Low Hazard
- Miscellaneous Structure - Very High Hazard
- Miscellaneous Structure - High Hazard
- Miscellaneous Structure - Medium Hazard
- Miscellaneous Structure - Low Hazard
- Miscellaneous Structure - No Hazard
- Rapid - High Hazard
- Rapid - Medium Hazard
- Rapid - Low Hazard
- Rapid - No Hazard
- Tributary - No Hazard
- Tree - Very High Hazard
- Tree - High Hazard
- Tree - Medium Hazard
- Tree - Low Hazard
- Sign

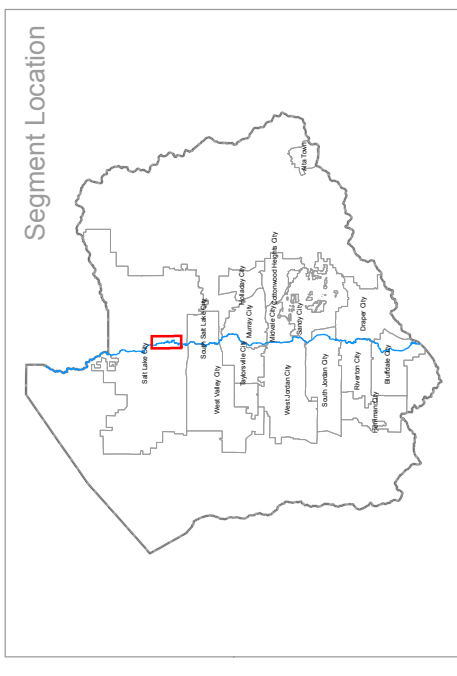
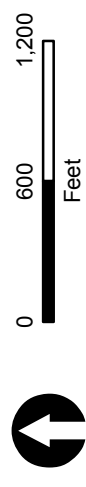
HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value

Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

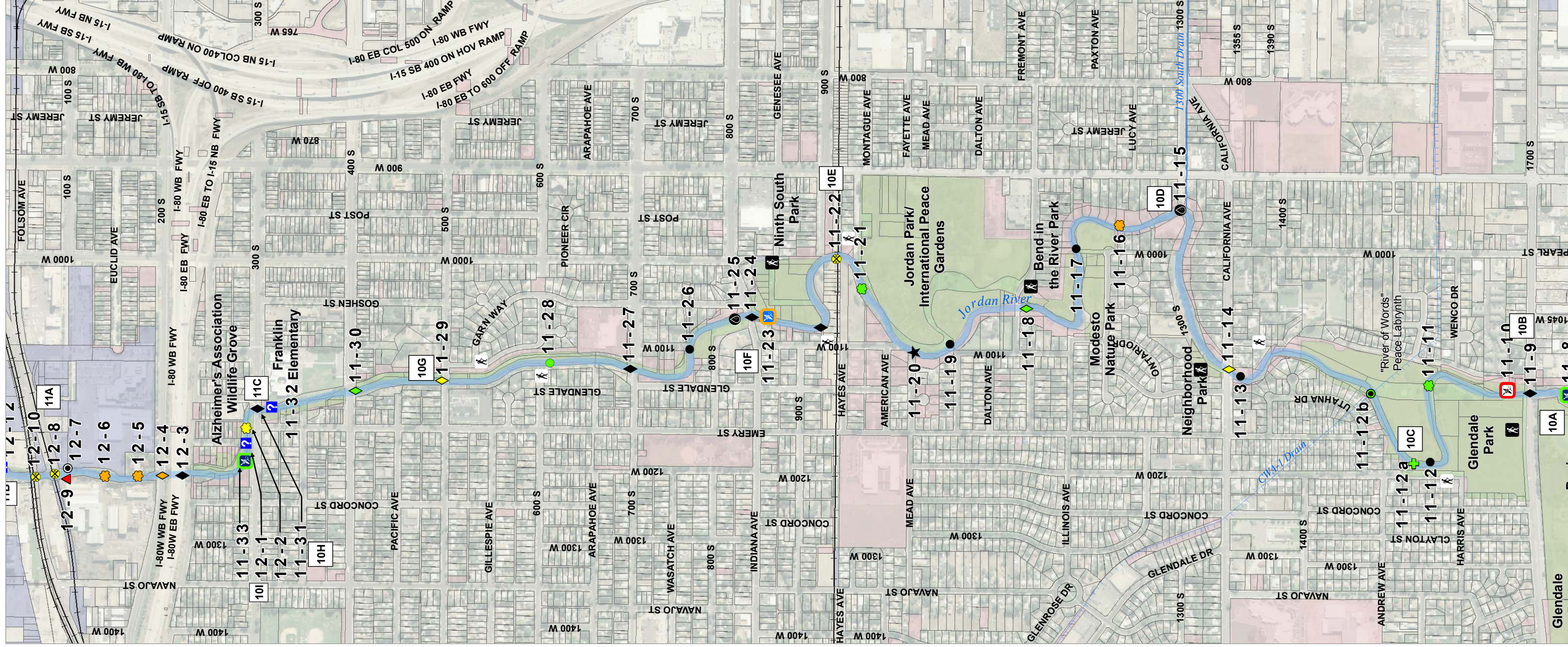
Waypoint locations have been modified for readability.
*No data available north of Surplus Canal.

Segment 10: Salt Lake City

1700 South to Interstate 80



June 2008



4.11 Segment 11: Salt Lake City (Interstate 80 to DuPont Avenue) (GSL segments 12 and 13)

Boaters then travel downstream, under the Interstate 80 vehicular bridge, waypoint 12-03 (zero hazard), then under the 200 South vehicular bridge which has very low clearance and has major flood debris collected underneath, waypoint 12-04 (high hazard). North of 200 South, there is a low limb across the stream at waypoint 12-05 (high hazard), and three low limbs in a row at approximately 150 South, waypoint 12-06 (high hazard). Some limbs have been removed and the hazard may be lower.

A high pipeline crosses the river at waypoint 12-07 (zero hazard) before passing under a concrete railroad bridge at waypoint 12-08 (medium hazard), which is a concrete railroad bridge. Flood debris has collected against the railroad bridge, creating a one-foot drop. There are two more railroad bridges at waypoint 12-10 (medium hazard) where debris has collected in the middle of the channel.

Traveling north, the trail approaches waypoint 12-11 (high hazard), a Rocky Mountain Power weir that with a canoe chute near South Temple. Waypoint 12-12 is a warning sign for the weir and chute, and has high functionality and a high value location. Downstream, water trail users pass waypoint 12-13 (low hazard), old trestle piers, and waypoint 12-14 (low hazard), where flood debris has collected downstream on both sides of the river. Waypoint 12-15 (medium hazard) is a pair of railroad bridges. The second bridge is lower, and a pipe crosses on the downstream side of the second bridge. As the river continues towards North Temple, a fallen tree has collected debris at waypoint 12-16 (low hazard) and an overhead pipeline and cable cross the river at waypoint 12-17 (zero hazard). The river flows under the North Temple vehicular bridge, waypoint 12-18 (zero hazard), where debris has collected. Another pipeline at waypoint 12-19 (zero hazard) crosses the Jordan River just downstream of North Temple, where City Creek joins the river through a concrete culvert, waypoint 12-20 (low hazard). An additional twelve inch pipe crosses overhead just north of North Temple at waypoint 12-21 (zero hazard).



Figure 4.11.1 Interstate 80 Bridge (WP 12-03)



Figure 4.11.2 Low bridge at 200 South, debris (WP 12-04)



Figure 4.11.3 Low limb across river at 200 South (WP 12-05)



Figure 4.11.4 Low limbs at 150 South, Hobo camp along river (WP 12-06)

Water users continue north to waypoint 12-22 (medium hazard) where a tree is laying across half of the river channel near the east bank. The river then flows under a footbridge at the Utah State Fair Park, waypoint 12-23 (zero hazard) to waypoint 12-24, a potential launch site on the west bank. This site has low functionality and a high value location. The river trail continues downstream under another footbridge at waypoint 12-25 (zero hazard), and passes an old boat launch at waypoint 12-26, near the south end of Constitution Park. This old concrete platform site has low functionality and a low value location.

The river flows under another footbridge at waypoint 12-27 (zero hazard) near Cottonwood Park, past a downed tree at waypoint 12-28 (high hazard) which blocks the entire river channel near the Utah State Department of Agriculture Building. At waypoint 12-29 in Cottonwood Park, there used to be a boat launch, but it has been removed. This launch has low functionality and a low value location. The water trail continues under the footbridge at the Agriculture Building, waypoint 12-30 (zero hazard), the 500 north vehicular bridge at waypoint 12-31 (zero hazard), and a footbridge north of 500 North at waypoint 12-32 (zero hazard).

As the river bends to the east, trees from each side meet in the middle of the river, waypoint 12-33 (high hazard). Water users then pass the Backman Elementary School on the east bank of the river, heading north again, and travel under the 700 North vehicular bridge, waypoint 12-34 (medium hazard), which has low clearance. Boaters pass under waypoint 12-35 (zero hazard), a footbridge at 800 North at the north end of Riverside Park. Continuing north, the water trail passes east of the Day Riverside Library, where a boat launch is located at waypoint 12-36 near 918 North. This launch has low functionality and a low value location - the ramp is dilapidated. Just a short distance downstream there used to be a boat launch at 1000 North, waypoint 12-37. This site has a high value location and facilities need to be reinstalled at an appropriate location to accommodate the boating needs in this section of the river.

Boaters then pass under waypoint 12-38 (low hazard), the 1000 North

vehicular bridge, and past waypoint 13-01 (low hazard), a tree that is overhanging half of river on the right side near the Jordan River Golf Course. Waypoint 13-02 was a boat launch near the Jordan River Golf Course has been removed in conjunction with a river restoration project.



Figure 4.11.5 High pipeline (WP 12-07)



Figure 4.11.6 Concrete railroad bridge (WP 12-08)



Figure 4.11.7 Flood debris at railroad bridge (WP 12-09)



Figure 4.11.8 Two railroad bridges with debris (WP 12-10)



Figure 4.11.9 Rocky Mtn. Power weir with canoe chute at South Temple (WP 12-11)



Figure 4.11.10 Warning sign for boaters (WP 12-12)



Figure 4.11.11 Old trestle piers (WP 12-13)



Figure 4.11.12 Railroad bridge near South Temple, debris (WP 12-14)



Figure 4.11.19 Bridge at 500 North (WP 12-31)



Figure 4.11.20 Trees over river from both sides at 600 North (WP 12-33)



Figure 4.11.13 Fallen tree upstream from North Temple, debris (WP 12-16)



Figure 4.11.14 Overhead pipeline and cable at North Temple (WP 12-17)



Figure 4.11.21 Bridge at 700 North, low clearance (WP 12-34)



Figure 4.11.22 Boat launch at 918 North (WP 12-36)



Figure 4.11.15 Potential launch site at Utah State Fair Park (WP 12-24)



Figure 4.11.16 Old concrete platform boat launch at 100 North (WP 12-26)



Figure 4.11.23 Boat launch at 1000 North (WP 12-37)



Figure 4.11.24 Vehicular bridge at 1000 North (WP 12-38)

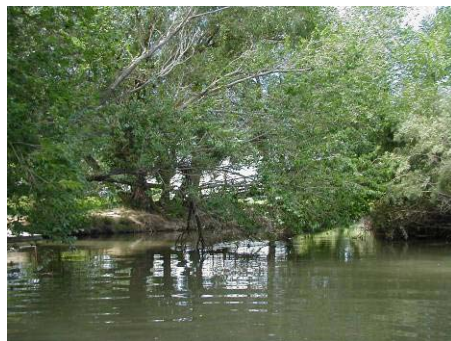


Figure 4.11.17 Tree across entire river at Agriculture Building (WP 12-28)



Figure 4.11.18 Concrete boat launch at Agriculture Building (WP 12-29)



Figure 4.11.25 Tree overhanging half the river (WP 13-01)



Figure 4.11.26 Boat launch near Jordan River Golf Course (WP 13-02)

Issues and Opportunities

- The 200 South vehicular bridge is too low and has collected major flood debris underneath.
- The canoe chute at waypoint 12-11 can be dangerous for inexperienced boaters. Waypoint 12-15 is the site of two railroad bridges. The second bridge is too low, and a pipe crossing the river on the downside poses a hazard.
- A potential site for a launch is located at waypoint 12-24, near the Utah State Fair Park. This site is important to the water trail corridor.
- The old concrete launch at waypoint 12-27 near Cottonwood Park is not in a good location and is not functional. It should be removed from the water corridor.
- The existing boat launch at waypoint 12-36 is in a poor location and has poor functionality. It should be abandoned.
- The existing boat launch at waypoint 12-27 is in a good location and has decent functionality, though it needs to be improved.
- An existing boat launch at waypoint 13-02 is in a poor location and has poor functionality. It should be abandoned.

Recommendations

4.11 A South Temple Corridor Railroad Consolidation (Waypoint 12-08 and 12-10)

These waypoints are part of a rail consolidation plan for this South Temple/City Creek corridor. The rail will be consolidated to one track; as the bridge is reconstructed, it should be designed with more clearance and in ways that do not catch debris and cause blockages in the river.

4.11 B Canoe Chute/Rocky Mountain Power Weir (Waypoint 12-11)

This is a high hazard for novice and beginner boaters and should be avoided. Only experienced boaters should attempt this section. See 4.10 H, page 4-28 for additional information.

4.11 C 200 South Bridge (Waypoint 11-31)

This bridge has a low clearance. During high water events, boaters should either terminate their trip at the Alzheimer's Association Wildlife Grove (waypoint 12-1) or portage around the bridge.

4.11 D Rail Road Bridges (Waypoint 12-15)

The second of two bridges is low and a pipeline in the river creates rapids. Sign to warn boaters that depending on water levels, water characteristics may change.

4.11 E City Creek Pipe (Waypoint 12-20)

Where City Creek empties into the Jordan River, cross-currents affect the river's flow. Sign to warn boaters that depending on water levels, water characteristics may change.

4.11 F Trailhead: North Temple (Waypoint 12-24)

Plans for a trailhead on the west side of the river on the south side of North Temple should be abandoned. There is an existing parking lot

on the north side of North Temple, west of the Jordan River that could serve a shared- use as a paved and urban water trailhead as well as parking for the Utah State Fair Park. This would require negotiation with the Utah Division of Facilities Construction and Management.

4.11 G Boat Ramps at Cottonwood Park (Waypoint 12-19) and 918 North (Waypoint 12-36)

The ramp at Cottonwood Park has been removed, and the one at waypoint 12-26 is dilapidated, non-functional and should be removed, leaving a deficit in launch facilities between North Temple and 1000 North.

4.11 H Boat Launch at 1000 North (Waypoint 12-37)

The existing boat launch needs to be upgraded and improved, but is needed. This should be a combined boat launch/trail head. The boat launch at waypoint 13-02 has been removed; launching activities are now concentrated at this better location at 1000 North.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Bridge	4.11 A	\$100,000
Trailhead	4.11 B/F	\$140,000
Signage	4.11 C	\$500
Signage	4.11 D	\$500
Signage	4.11 E	\$500
Launch Removal	4.11 G	\$5,000
Launch Removal	4.11 H	\$115,000

Jordan River Trail Master Plan

Urban Water Trail Recommendations

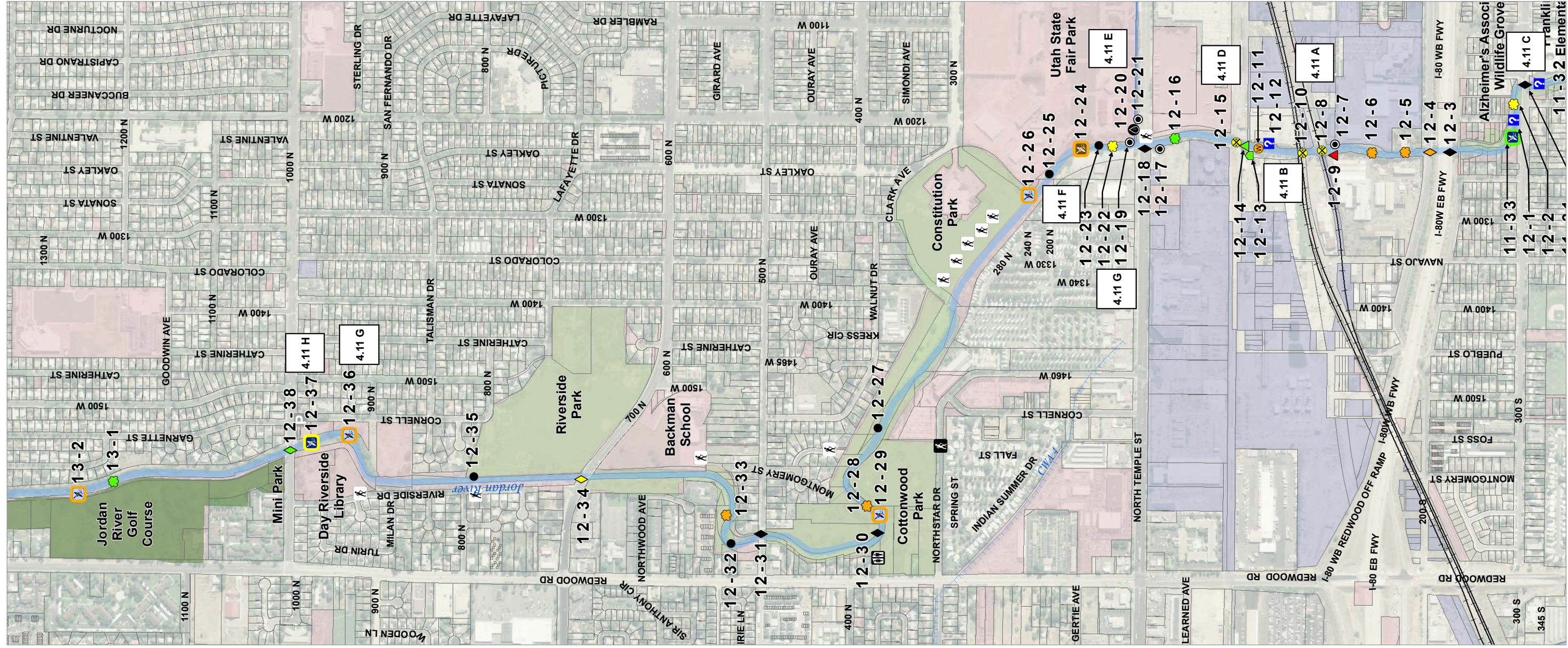
	Equestrian Trailhead
	Formal Trailhead
	Informal Trailhead
	Trail Access Point
	Parking - Formal Paved
	Parking - Informal Unpaved
	Restrooms
	Highways
	City Boundary
	Jordan River
	Streams
	Canals
	Light Rail
	Railroad
	County Boundary
	Meander Corridor
	Wetlands (WAIDS 2007)
	Golf Course
	Park
	Publicly Owned Land
	Utility Owned Land

	Launch - HV Location, HV Function
	Launch - HV Location, MV Function
	Launch - HV Location, LV Function
	Launch - MV Location, MV Function
	Launch - MV Location, LV Function
	Launch - VLV Location, LV Function
	Portage - HV Location, LV Function
	Portage - HV Location, MV Function
	Portage - LV Location, HV Function
	Portage - LV Location, LV Function
	Portage - High Hazard
	Potential Launch - HV Location, MV Function
	Potential Launch - HV Location, LV Function
	Potential Launch - MV Location, VLV Function
	Potential Launch - LV Location, LV Function
	Bridge - High Hazard
	Bridge - Medium Hazard
	Bridge - Low Hazard
	Bridge - No Hazard
	Footbridge - Very High Hazard
	Footbridge - Medium Hazard
	Footbridge - Low Hazard
	Footbridge - No Hazard
	Railroad Bridge - High Hazard
	Railroad Bridge - Medium Hazard
	Railroad Bridge - No Hazard
	Dam - Very High Hazard
	Dam - High Hazard
	Dam - Medium Hazard
	Dam - Low Hazard
	Weir - Very High Hazard
	Weir - High Hazard
	Flood Debris - Very High Hazard
	Flood Debris - High Hazard
	Flood Debris - Medium Hazard
	Flood Debris - Low Hazard
	Pipe - Very High Hazard
	Pipe - High Hazard
	Pipe - Medium Hazard
	Pipe - Low Hazard
	Pipe - No Hazard
	Cultural Element - Low Hazard
	Cultural Element - No Hazard
	Miscellaneous Debris - High Hazard
	Miscellaneous Debris - Medium Hazard
	Miscellaneous Debris - Low Hazard
	Miscellaneous Structure - Very High Hazard
	Miscellaneous Structure - High Hazard
	Miscellaneous Structure - Medium Hazard
	Miscellaneous Structure - Low Hazard
	Miscellaneous Structure - No Hazard
	Miscellaneous Structure - No Hazard
	Rapid - High Hazard
	Rapid - Medium Hazard
	Rapid - Low Hazard
	Rapid - No Hazard
	Tributary - No Hazard
	Tree - Very High Hazard
	Tree - High Hazard
	Tree - Medium Hazard
	Tree - Low Hazard
	Sign

HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value

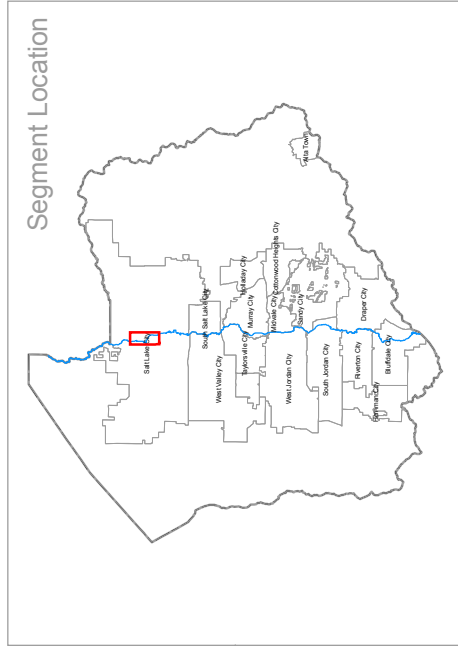
Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

Waypoint locations have been modified for readability.
*No data available north of Surplus Canal.



Segment 11: Salt Lake City

Interstate 80 to DuPont Avenue



4.12 Segment 12: Salt Lake City/Salt Lake County (DuPont Avenue to Interstate 215) (GSL segments 13 and 14)

The water trail continues to flow north through Rose Park Golf Course under the bridge at waypoint 13-03 (low hazard). Several trees are down and blocking the right half of the river at waypoints 13-04 (low hazard) and 13-05 (low hazard). North of the Golf Course is a boat launch at waypoint 13-06. This launch is paved with asphalt and has low functionality and a high value location. Pipelines cross above the river at waypoints 13-07 (zero hazard) and 13-08 (zero hazard). Located immediately north of waypoint 13-06, waypoint 13-09 is an abandoned launch near Redwood Road. This site has very low functionality and a high value location.

The river then flows under waypoints 13-10 (zero hazard), the Redwood Road vehicular bridge; waypoint 14-01, a pipeline 10 feet past Redwood Road; and waypoint 14-02 (medium hazard), an old, unsafe footbridge approximately 50 feet west of Redwood Road. Boaters continue north past several hazards at waypoints 14-03 (medium hazard), a fallen tree that is covering half of the river on the right side; 14-04 (medium hazard), fallen trees that cover half of the river channel; 14-05 (low hazard), a leaning snag that covers 60% of the river channel on the left side; and 14-06 (low hazard), where debris has collected on a snag.

The water trail continues north, under waypoint 14-07 (medium hazard), a pipeline with wood around it that looks like a bridge with cables hanging down and past hazards at waypoints 14-08 (low hazard), a tree with debris along the left side of river; an abandoned irrigation dam with significant debris that blocks the entire river channel just upstream of Interstate 215; 14-10 (medium hazard), a large log sticking up with lots of debris; and 14-11 (low hazard), several logs that have piled up with debris along the right side of the river. The debris at waypoint 14-09 has been removed. The Urban Water Trail then continues under waypoint 14-12 (medium hazard), the Interstate 215 vehicular bridge, and becomes a shared water trail between Salt Lake and Davis Counties. The Urban Water Trail ends at Center Street in Davis County.



Figure 4.12.3 Tree down right half of river, Rose Park Golf Course (WP 13-05)



Figure 4.12.4 Boat launch near Redwood Road (WP 13-06)



Figure 4.12.11 Fallen tree covering 60% of channel (WP 14-05)



Figure 4.12.12 Debris collecting on fallen tree (WP 14-06)



Figure 4.12.1 Bridge at Rose Park Golf Course (WP 13-03)



Figure 4.12.5 Pipeline across river (WP 13-07)



Figure 4.12.6 Abandoned boat launch near Redwood Road (WP 13-09)



Figure 4.12.13 Pipeline covered with wood, cables hang down (WP 14-07)



Figure 4.12.14 Tree with debris, right side of river (WP 14-08)



Figure 4.12.2 Tree down right half of river, Rose Park Golf Course (WP 13-04)



Figure 4.12.7 Vehicular bridge at Redwood Road (WP 13-10)



Figure 4.12.8 Footbridge, pipeline near Redwood Road (WP 14-02)



Figure 4.12.15 Historic irrigation dam, debris blocks entire channel (WP 14-09)



Figure 4.12.16 Log sticking out of river, debris (WP 14-10)



Figure 4.12.9 Fallen trees covering half the river (WP 14-03)



Figure 4.12.10 Fallen trees covering half the channel (WP 14-04)



Figure 4.12.17 Trees piled up on right side of river (WP 14-11)



Figure 4.12.18 Vehicular bridge at Interstate 215 North (WP 14-12)

Issues and Opportunities

- North of the Rose Park Golf Course at waypoint 13-06, there is an existing boat launch. This location is important for the water trail corridor, and should be improved to safe functionality.
- The existing boat launch at waypoint 13-09 that has been abandoned. This site is an important location and should be improved.
- The old bridge at waypoint 14-02, just west of Redwood Road is unsafe, and needs to be replaced with a new bridge that meets safety standards.
- The pipe crossing the river at waypoint 14-07 is too low.
- The bridge at Interstate 215 is too low.

Recommendations

4.12 A Boat Launch (Waypoint 13-06)

Redesign and reconstruct a boat launch in this location near 1800 North at the crossing of Redwood Road. The facility should include boater access, parking, restrooms, and emergency river access.

4.12 B Bridge (Waypoint 14-02)

Replace this old and deteriorated bridge

4.12 C Pipe (Waypoint 14-07)

Provide signing to warn boaters of the low clearance.

4.12 D Davis County/Legacy Parkway Trail Connection (Waypoint 14-12)

At the Salt Lake County boundary, a trail is anticipated on the east side of the river in Davis County to connect to trails along Legacy Parkway. Once the river passes under I-215, environmental concerns suggest that boaters be required to register and receive permits for access.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Launch Upgrade	4.12 A	\$10,000
Bridge	4.12 B	\$100,000
Signage	4.12 C	\$500
Connection	4.12 D	

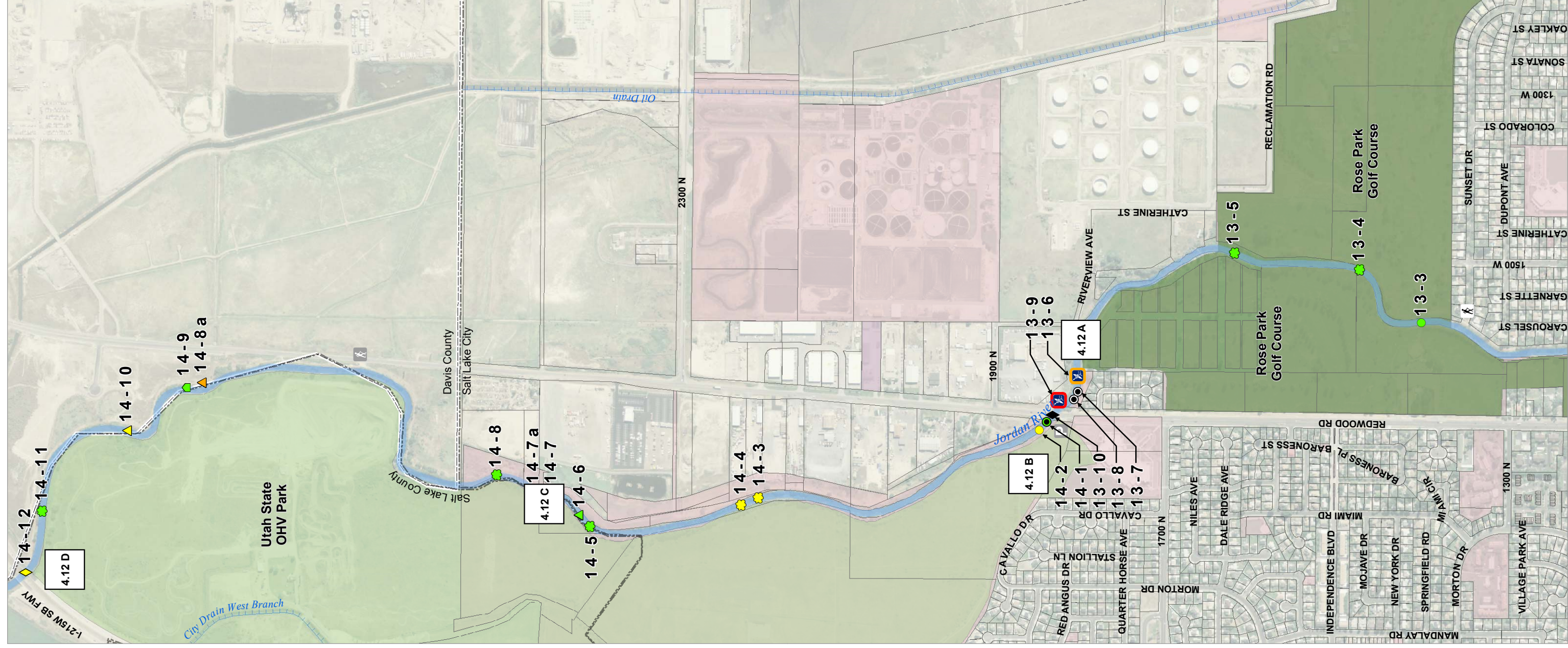
Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
- Launch - HV Location, MV Function
- Launch - HV Location, LV Function
- Launch - MV Location, VLV Function
- Launch - MV Location, MV Function
- Launch - LV Location, LV Function
- Launch - VLV Location, LV Function
- Portage - HV Location, HV Function
- Portage - LV Location, HV Function
- Portage - LV Location, VLV Function
- Portage - High Hazard
- Potential Launch - HV Location, MV Function
- Potential Launch - HV Location, LV Function
- Potential Launch - HV Location, VLV Function
- Potential Launch - LV Location, LV Function
- Bridge - High Hazard
- Bridge - Medium Hazard
- Bridge - Low Hazard
- Bridge - No Hazard
- Footbridge - Very High Hazard
- Footbridge - Medium Hazard
- Footbridge - Low Hazard
- Footbridge - No Hazard
- Railroad Bridge - High Hazard
- Railroad Bridge - Medium Hazard
- Railroad Bridge - No Hazard
- Dam - Very High Hazard
- Dam - High Hazard
- Dam - Medium Hazard
- Dam - Low Hazard
- Weir - Very High Hazard
- Weir - High Hazard
- Flood Debris - Very High Hazard
- Flood Debris - High Hazard
- Flood Debris - Medium Hazard
- Flood Debris - Low Hazard
- Pipe - Very High Hazard
- Pipe - High Hazard
- Pipe - Medium Hazard
- Pipe - Low Hazard
- Pipe - No Hazard
- Cultural Element - Low Hazard
- Cultural Element - No Hazard
- Miscellaneous Debris - High Hazard
- Miscellaneous Debris - Medium Hazard
- Miscellaneous Debris - Low Hazard
- Miscellaneous Structure - Very High Hazard
- Miscellaneous Structure - High Hazard
- Miscellaneous Structure - Medium Hazard
- Miscellaneous Structure - Low Hazard
- Miscellaneous Structure - No Hazard
- Rapid - High Hazard
- Rapid - Medium Hazard
- Rapid - Low Hazard
- Rapid - No Hazard
- Tributary - No Hazard
- Tree - Very High Hazard
- Tree - High Hazard
- Tree - Medium Hazard
- Tree - Low Hazard
- Sign

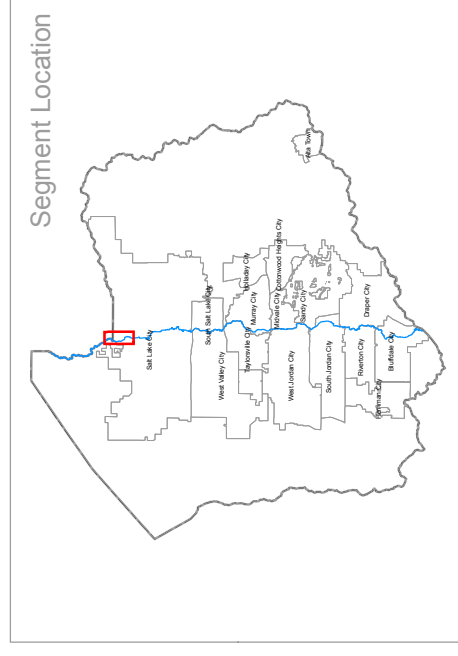
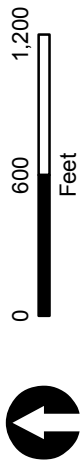
HV = High Value
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Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.
Waypoint locations have been modified for readability.
*No data available north of Surplus Canal.



Segment 12: Salt Lake City/ Salt Lake County

DuPont Avenue to Interstate 15



4.0 Urban Water Trail

One of the key elements of the Jordan River Trail Master Plan is the water trail. Numerous groups and individuals use non-motorized watercraft on the river for recreational purposes; yet to date there has been no documentation or information available to users. It has been a long-standing desire by users, interest groups, and the County to provide valuable information about recreating on the Jordan River, and while a plan was never completed, some data was collected and made available to this planning effort.

Water Trail Data

In 2000, the Great Salt Lakekeeper (GSL), a non-profit organization whose purpose is to protect Great Salt Lake and its tributaries and environment, received a grant from the National Park Service (NPS) RTCA Program, and partnered with the Salt Lake County Division of Parks and Recreation, and SWCA, Inc. and Monson Engineering (both local consulting firms) to gather data with the intent to complete *The Jordan River Navigational Hazards Removal and Recreational Boating Plan* (unpublished). Project supporters included the Utah Division of Forestry, Fire and State Lands; Utah Division of State Parks and Recreation; Salt Lake County Engineering Department; Foundation for the Provo-Jordan River Parkway; Utah Whitewater Association; and SPLORE.

Data was collected during the summer of 2001 using Geographic Information Systems (GIS), and was processed over the winter of 2001. Maps and a database were created, and GSL then began making presentations to municipal representatives and government agencies during 2002. Though results of the data collection were never published, critical and valuable information regarding hazards, river conditions, and recreational opportunities was gathered. The data was generously provided in electronic format and was critical in developing the Urban Water Trail portion of the Master Plan, primarily because it provided the viewpoint of experienced boaters and recreation planners.

According to those who participated in the data collection, the conditions necessitating the study included the following: the Jordan River is largely perceived as unsafe for recreational boating, hazards create liability for responsible parties, the general public is not aware of hazard locations, demand for recreational boating is high, public access is severely limited, existing boating facilities are underdeveloped, the river is not perceived as a public waterway, and no coordinated water trail plan exists.

Desired results of the uncompleted project were to develop maps of hazards and recreation points, and to create navigational hazards removal and recreational boating plans, as well as produce information and marketing tools such as CD ROMS, multi-media shows, and website applications. The goal was an Urban Water Trail Guide Book that would act as a complete trail guide for the Jordan River with information on water trail facilities, wildlife in the Jordan River corridor,

and cultural information.

Two Utah laws corroborate these goals, and designate the unique status of the Jordan River as a public, navigable waterway, and require planning and implementation providing better access for boating and public use.

Administrative Rule R652-70-100: Sovereign Lands

“This rule provides for the management and classification of the surface of sovereign lands in Utah, which include but are not limited to, the beds of Bear Lake, the Great Salt Lake, Utah Lake, the Jordan River... It also provides for the issuance of special use leases, general permits and easements on sovereign lands and the procedures and fees necessary to obtain these rights of use. This rule implements Article XX of the Utah Constitution, and Section 65A-10-1.”

Administrative Rule R652-2-200

“The State of Utah recognizes and declares that the beds of navigable waters within the state are owned by the state and are among the basic resources of the state, and that there exists, and has existed since statehood, a public trust over and upon the beds of these waters. It is also recognized that the public health, interest, safety, and welfare require that all uses on, beneath or above the beds of navigable lakes and streams of the state be regulated, so that the protection of navigation, fish and wildlife habitat, aquatic beauty, public recreation, and water quality will be given due consideration and balanced against the navigational quality or economic necessity or justification for, or benefit to be derived from, any proposed use.”

About Water Hazard Names & Classifications

The existing condition of the Urban Water Trail is described beginning in Section 4.1, and represents the data collected during the GSL/NPS study. They divided the river corridor into 15 different sections, and the hazards and facilities within each section were given an individual waypoint number and mapped using GIS. Descriptions and photographs were also collected as necessary and are included here. Using the expertise of experienced boaters and recreation planners, the project team assigned a level of risk to each hazard and a functionality and location value to each facility using the most dangerous hazard as a point of comparison for other hazards, and the best facilities as points of comparison for water trail facilities.

The project section numbers do not correspond with the segment numbers of this plan, therefore, the waypoints described in the text and shown on the maps indicate the GSL/NPS section number followed by the data point number within that section. This number is followed by the hazard ranking in parentheses. Hazard levels can vary depending upon the level and flow of the river. The rankings shown were assigned at the time of data collection in the summer of 2000, a low water year.

Some waypoints are described as launches, take-outs, or portages. A launch is a put-in where boaters can begin a trip, a take-out is where

boaters end a trip, and a portage is composed of two sites, where boaters must take-out around a hazard and put-in again on the downstream side of the hazard. Some locations may serve as both launches and portages or take-outs and portages. Sites that serve as launches or take-outs need convenient access to parking and restrooms and require larger ramps for staging large groups. Sites that serve only as portages require smaller ramps, and do not need access to restrooms or parking.

Launches, put-ins, and portages were ranked according to current level of functionality in 2002, and the value of the location as a system-wide element. For example, a waypoint that is rated with low functionality and high value location, does not function adequately in its current condition, but is in a good location considering its position within the water trail system as a whole and individual site characteristics.

Overall Issues, Opportunities, and Recommendations

The issues pertaining to the overall water trail corridor are discussed below, followed by an explanation of water hazard documentation in general. Detailed descriptions of existing conditions, issues and opportunities, and recommendations for each of the twelve corridor segments are then provided in section 4.1 through 4.12.

Trail Gaps

Discontinuous trail segments prevent water trail users from enjoying a continuous trip on the Jordan River through Salt Lake County, and the primary factor causing this fractured condition of the water trail is the prevalence of hazards and impassable elements within the river corridor. These hazards range from dams, weirs, and debris to bridges with low clearance. In addition, portage facilities and rights-of-way have not been negotiated to give water trail users a clear, safe route through the entire Jordan River corridor.

One of the most important keys to improving the functional level of the water trail corridor is to “fill in” these gaps by negotiating hazard removal, negotiating and developing appropriate portage facilities around the hazards, and improving safety and awareness through signage and education efforts.

Signage

Signage along the Urban Water Trail currently represents a hodgepodge of designs, information content, and purposes, and presents trail users with incomplete and sometimes confusing, information about the trail facilities. Water trail facility signs are sporadic and inconsistent, and the trail lacks consistent location signs for information and emergency purposes as well as water hazard warning signs.

The Foundation for the Provo-Jordan River Parkway, along with Salt Lake County developed a standard trailhead sign which has been installed at several trailheads along the river, though they typically do not have any information regarding water trail facilities, guidelines,

safety information, or water trail maps. This plan recommends the implementation of a comprehensive signage program to improve the functional level, safety, and awareness of the water trail. Detailed information can be found in section 5.4 of this plan.

Trail Maps

The lack of a comprehensive, easy-to-read, readily available map prevents many potential water trail users from exploring the Jordan River. As discussed in Chapter 5, a comprehensive water trail map should be incorporated as part of the Salt Lake County standard trailhead sign design. In addition, the County should make mapping available online, and in printed format that water trail users can utilize to plan trips, and to take with them on their visits to the Jordan River. These map could take the form of a guide booklet or large foldout map, and could include information on the history of the river, flora and fauna found along the river, and cultural information. These printed guides should be made available online and at County facilities throughout the Salt Lake Valley. An excellent example is the "Willamette River Water Trail Guide," which can be viewed and downloaded at www.willamettewatertrail.org.

Bridges

Too many bridges across the river detract from the quality of the water trail user experience and can present hazards if not designed and installed correctly. Therefore, the number of new bridges should be strictly limited.

Water Trail Hazards

Concrete, flood debris, trees, garbage, and overhanging limbs cause dangerous boating conditions on the Jordan River, particularly those noted as medium, high, or very high hazard in the following text. Water trail hazards can vary with the level of the river. The general public needs to be informed of the risks to make educated decisions about recreation on the Jordan River. Salt Lake County should also be negotiating with responsible parties for hazard removal or safe portage around these facilities, including appropriate signage.

Trailheads

More water trailheads are needed that provide adequate parking in close proximity to launch and portage locations. Interpretive signage at water trailheads should inform water trail users about the history and ecology of the river, including plant and animal species they may encounter on their river trip.

Water Trail Facility Designs

Launches and portages need to be developed with safe, flexible, functional designs that meet water trail user needs at different flow levels of the river, and that accommodate boating parties of varying sizes and skill levels.

Kayak Parks

Kayakers have expressed an interest in developing kayak play areas at 2100 South and 6400 South. This plan recommends additional

studies to explore the feasibility of such parks.

Rowing

Rowers are currently practicing on the Great Salt Lake and Surplus Canal, and have expressed an interest in rowing facilities closer to the cities. The site requirements for rowing cannot be met along the Jordan River and facilities are not included as part of this plan.

Water Level

The ability to travel the entire river length depends on the water level. Sometimes the flow is too low to allow boats to pass, and other times the flow is too high for all recreational users except those who are very skilled and who have first-hand knowledge of the corridor and its hazards.

Comprehensive Planning

A comprehensive regional plan for the Jordan River from its origin at Utah Lake to its destination at the Great Salt Lake should be developed in coordination with this plan to facilitate access to these sovereign waters.

4.1 Segment 1: Bluffdale (Southern County Line to 15400 South) (GSL segments 2,3, and 4)

The Jordan River flows north from Utah Lake, and enters Salt Lake County at the Bluffdale City boundary, near the Jordan Narrows Diversion Dam and Canals.



Figure 4.1.1 Floating barricades at the Narrows, Utah County (WP 2-08)



Figure 4.1.2 Turner Dam at the Narrows (WP 3-01)

There is no official public access to the Jordan River Urban Water Trail in this area. The land is owned by the Jordan Valley Water Conservancy District, Union Pacific Railroad, and the canal companies, and there is no accessible water launch for recreational boaters to begin a trip in this segment, to end a trip from Utah County, or portage through



Figure 4.1.3 Railroad Bridge crossing at the Narrows (WP 3-02)



Figure 4.1.4 Portage put-in for the Turner Dam at the Narrows (WP 3-03)



Figure 4.1.5 Foot bridge for Gauging Station Bridge at the Narrows (WP 3-04)



Figure 4.1.6 Rapids and split channel at the Narrows (WP 3-06)



Figure 4.1.7 Rapids, right channel at the Narrows (WP 3-07)

the area. Water trail users are currently faced with unsafe conditions as they try to pass through the area, or try to begin a river trip in this portion.

As water trail users travel downstream from Utah County, they encounter the Jordan Narrows Pump Station barrels at waypoint 2-08 (high hazard), just south of the Salt Lake County line. This forces boaters to take-out at waypoint 2-09 (high hazard) to get around the Jordan Narrows Pump Station and Turner Dam. This portage has low functionality, and is a high value location - it is a critical portage, but access is unsafe and not formally allowed by the water district or the canal companies. Boaters have been known to take-out in this area and carry their boats along the railroad tracks until they reach the river again, which is illegal and very unsafe because trains travel at high speeds through the area. The river then flows through the Turner Dam, waypoint 3-01 (very high hazard), and underneath the Union Pacific rail bridge, waypoint 3-02 (zero hazard). There is concrete in the channel under the bridge, waypoint 3-02a, (high hazard). Waypoint 3-03 is a portage downstream from the UP rail bridge with low functionality and a low value location due to the long portage around the Pump Station.

Water trail users then travel north a short distance before encountering the Gauge Station Bridge, waypoint 3-04 (very high hazard), which is a small footbridge over the Jordan River with a 4 foot clearance. This bridge is extremely dangerous to boaters in high water conditions

because of the low clearance. Users proceed through small rapids, waypoint 3-05 (zero hazard), and past a mid-stream island which clogs the channel, resulting in a narrow passage, waypoint 3-06 (low hazard). The river splits around the island with the main channel passing to the left, waypoint 3-07 (low hazard), and an area with overhanging limbs along the west shore. There are only 3 islands remaining in the Jordan River Corridor within Salt Lake County - a remnant of the past historical character of the river before settlers modified the river course to meet their agricultural needs.

Next boaters encounter the Rock Garden rapids, waypoint 3-08 (low hazard), passing through the 50-yard-long right-split of the river with a riffle, waypoint 3-09 (zero hazard), and the tail end of the Rock Garden rapids, waypoint 3-10 (medium hazard). The water trail then proceeds through three sets of rapids, waypoints 3-11 (zero hazard), 3-12 (zero hazard) and 3-13 (medium hazard), where the left and right forks of the channel rejoin at waypoint 3-14 (medium hazard).

Boaters proceed north where the river splits again with the main channel going to the west, passing through rapids at waypoint 3-15 (zero hazard), 3-16 (zero hazard), 3-17 (medium hazard), 3-18 (zero hazard), 3-19 (zero hazard) rapids. The forks of the river join near waypoint 3-20 (zero hazard), where the spillway canal also meets the river. The water trail continues through rapids at waypoint 3-21 (zero hazard) where there are large chunks of debris and concrete in the channel. The river passes under waypoint 3-21a (zero hazard), a historic bridge crossing and through waypoint 3-22 (high hazard). Old pilasters of a bridge that have been left in the river channel and along the sides.

The trail flows north about a quarter of a mile to waypoint 3-23 (medium hazard), where flood debris with rapids block 90 percent of the channel. Boaters then pass an old irrigation diversion with one wall remaining on the bank, and an old structure on the west bank, waypoint 3-24 (zero hazard). Water trail users must then take-out before they reach waypoint 3-25 (medium hazard), the lower dam at Jordan Narrows. Waypoint 3-26 is the portage for this dam and is located on west side of the dam between the irrigation and diversion dam. It has low functionality and a high value location. Boaters then launch at waypoint 3-27, a put-in on the west bank composed of a concrete ramp heading down to the river. This launch has low functionality and a high value location.

The trail continues to waypoint 3-28 (high hazard), where flood debris totally chokes off the river and causes a one-foot drop in the water. There is no way to get a boat through this area, and boaters must take-out and put-in between the dam and railroad crossing with no safe portage and launch. The river then flows to waypoint 3-29 (high hazard), where a fallen tree on the west side and two closely-spaced trees impede travel. There is more flood debris at waypoint 3-30 (high hazard), just south of the railroad bridge, and at waypoint 4-01 (medium hazard), where flood debris has collected under the railroad bridge and covers 75 percent of the channel.



Figure 4.1.8 Rapids, rock garden, right channel at the Narrows (WP 3-08)



Figure 4.1.9 Rapids, riffle right split (WP 3-09)



Figure 4.1.16 Another split channel at the Narrows (WP 3-18)



Figure 4.1.17 Split in channel, main channel left (WP 3-19)



Figure 4.1.10 Tail end of rock garden rapids (WP 3-10)



Figure 4.1.11 Rapids after rock garden, right channel (WP 3-12)



Figure 4.1.18 Rapids after Cultural Site at the Narrows (WP 3-21)



Figure 4.1.19 Cultural Site, Historic Bridge Crossing at the Narrows (WP 3-21A)



Figure 4.1.12 Rapids, end of right channel at the Narrows (WP 3-13)



Figure 4.1.13 Right and left fork channels meet (WP 3-14)



Figure 4.1.20 Rapids at base of Cultural Site at the Narrows (WP 3-22)



Figure 4.1.21 Upper narrows below Cultural Site at the Narrows (WP 3-23)



Figure 4.1.14 River splits again, main channel goes left (WP 3-16)



Figure 4.1.15 End of second split channel at the Narrows (WP 3-17)



Figure 4.1.22 Cultural Site, Historic Irrigation Dam at the Narrows (WP 3-24)



Figure 4.1.23 Take out at Lower Narrows Irrigation Dam (WP 3-25)



Figure 4.1.24 Lower irrigation dam at the Narrows (WP 3-27)



Figure 4.1.25 Railroad bridge at Lower Narrows (WP 4-01)



Figure 4.1.32 Overhanging tree at Lower Narrows (WP 4-08)



Figure 4.1.26 Rock garden at Lower Narrows (WP 4-02)



Figure 4.1.27 Rapids at Lower Narrows (WP 4-03)



Figure 4.1.28 Rock garden at Lower Narrows (WP 4-04)



Figure 4.1.29 Rapids at Lower Narrows (WP 4-05A)



Figure 4.1.30 Rapids at Lower Narrows (WP 4-06)



Figure 4.1.31 Rapids at Lower Narrows (WP 4-07)

Issues and Opportunities

- The approach to the barrels and portage are not safe.
- The portage at waypoint 2-09 functions well, but is not in the best location. Water trail users continuing down the Jordan River from Utah Lake have been hauling their canoes along the rail line trying to get to a place where they can safely re-launch. There is not a safe route from the portage to a safe, convenient launch.
- The existing launch at waypoint 3-03 is too far from the existing portage.
- The gauge station bridge at waypoint 3-04 is too low.
- The concrete at waypoint 3-02a should be removed if users will be portaging around this area.
- Old pilasters of a bridge at waypoint 3-22 have been left in the river and are dangerous for boaters.
- The existing portage at waypoint 3-26 for the lower dam is not suitable for safe use.
- The launch at waypoint 3-27 is not suitable for safe use.

Recommendations

4.1 A Boaters Approaching From Utah County and Completing a Trip at the Narrows or Continuing Down-River

Provider signing up-river to warn boaters of the Jordan River Pump Station barrels stretching across the river at the diversion dam.

Provide a safe portage up-river of the pump station and diversion dam for boaters completing a trip from Utah County, or those who wish to continue on the river. The launch portion of the portage should take place on the downriver side of the dam north of the small parking lot and driveway at the caretakers home. This is not an official launch point; it is a fairly long portage allowing boaters to continue on their river trip.

Current land ownership and security issues may make this difficult; however, in the future the portage and limited parking spaces would greatly enhance the boating experience and enhance safety. As improvements in the area are addressed, consideration should be made to accommodate safely exiting the river at this point. A reservation system may also work, where boaters must make prior

arrangements with the property owner to park a car and/or be picked up at the portage. Boaters would also have access to the restroom and parking available at the new Jordan Narrows Trailhead.

4.1 B Gauge Station Bridge (Waypoint 3-04)

Remove, and replace if necessary, as this bridge is too low and causes hazardous conditions.

4.1 C Hazards (Waypoint 3-22)

Remove and appropriately dispose of the concrete pilasters which remain in the river.

4.1 D Portage and Launch (Waypoints 3-26 and 3-27)

Redesign the existing portage and launch, and combine if possible. This is a desirable location for a portage and launch.

4.1 E Interpretive Opportunity (Waypoint 4-05)

A cultural interpretive opportunity exists with the old power station. Interpretation should be addressed corridor-wide and include consistent signing.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Portage/Signage	4.1 A	\$12,000
Bridge	4.2 B	\$140,000
Hazard Removal	4.1 C	n/a
Launch/Portage Upgrade	4.1 D	\$20,000
Signage	4.1 E	\$2,500

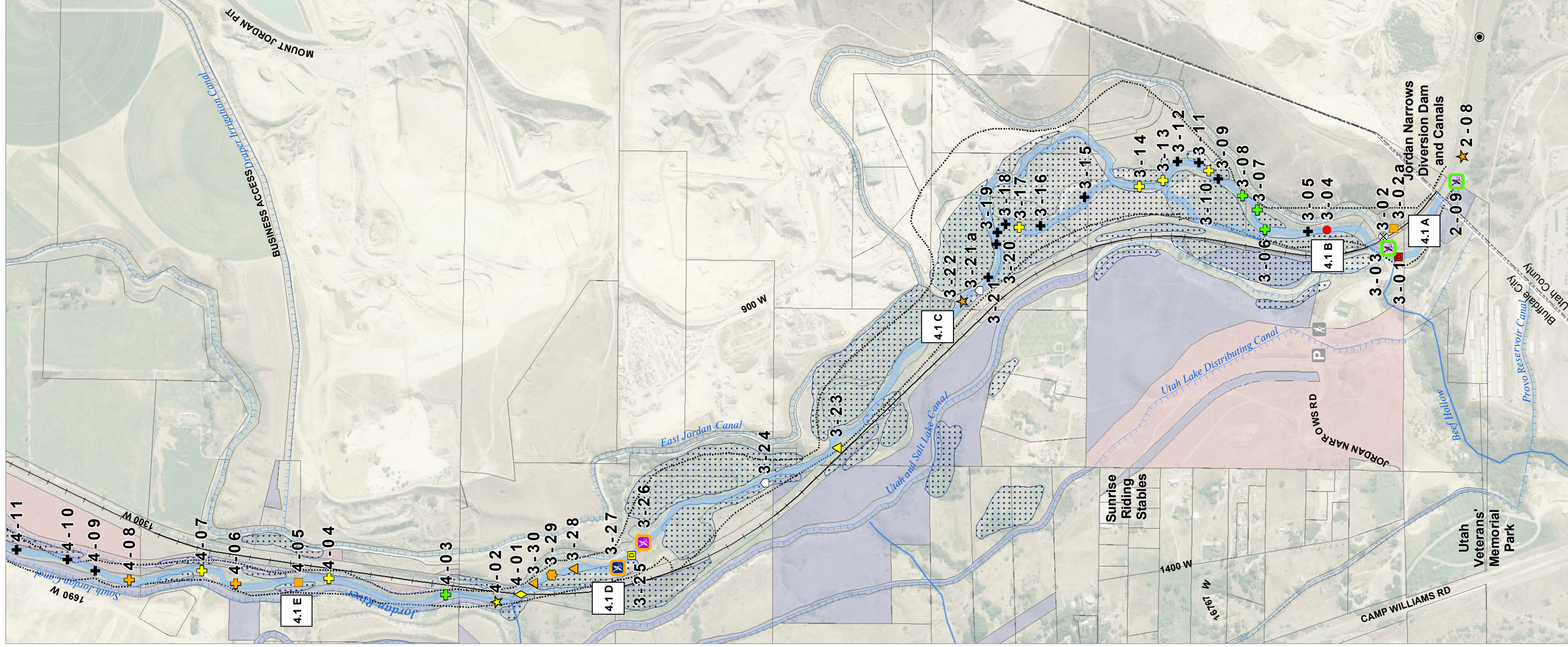
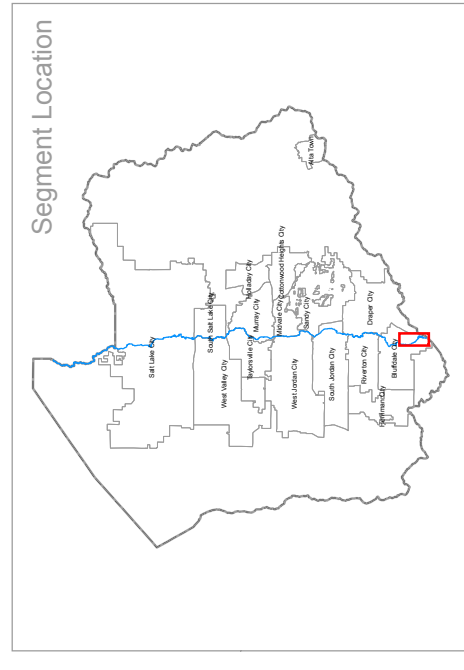
Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
 - Launch - HV Location, MV Function
 - Launch - HV Location, LV Function
 - Launch - MV Location, MV Function
 - Launch - MV Location, LV Function
 - Launch - VLV Location, LV Function
 - Portage - HV Location, LV Function
 - Portage - HV Location, HV Function
 - Portage - LV Location, VLV Function
 - Portage - LV Location, HV Function
 - Portage - High Hazard
 - Potential Launch - HV Location, MV Function
 - Potential Launch - HV Location, LV Function
 - Potential Launch - LV Location, VLV Function
 - Potential Launch - LV Location, LV Function
 - Bridge - High Hazard
 - Bridge - Medium Hazard
 - Bridge - Low Hazard
 - Bridge - No Hazard
 - Footbridge - Very High Hazard
 - Footbridge - Medium Hazard
 - Footbridge - Low Hazard
 - Footbridge - No Hazard
 - Railroad Bridge - High Hazard
 - Railroad Bridge - Medium Hazard
 - Railroad Bridge - No Hazard
 - Dam - Very High Hazard
 - Dam - High Hazard
 - Dam - Medium Hazard
 - Dam - Low Hazard
 - Weir - Very High Hazard
 - Weir - High Hazard
 - Flood Debris - Very High Hazard
 - Flood Debris - High Hazard
 - Flood Debris - Medium Hazard
 - Flood Debris - Low Hazard
 - Pipe - Very High Hazard
 - Pipe - High Hazard
 - Pipe - Medium Hazard
 - Pipe - Low Hazard
 - Pipe - No Hazard
 - Cultural Element - Low Hazard
 - Cultural Element - No Hazard
 - Miscellaneous Debris - High Hazard
 - Miscellaneous Debris - Medium Hazard
 - Miscellaneous Debris - Low Hazard
 - Miscellaneous Structure - Very High Hazard
 - Miscellaneous Structure - High Hazard
 - Miscellaneous Structure - Medium Hazard
 - Miscellaneous Structure - Low Hazard
 - Miscellaneous Structure - No Hazard
 - Rapid - High Hazard
 - Rapid - Medium Hazard
 - Rapid - Low Hazard
 - Rapid - No Hazard
 - Tributary - No Hazard
 - Tree - Very High Hazard
 - Tree - High Hazard
 - Tree - Medium Hazard
 - Tree - Low Hazard
 - Sign
- Equestrian Trailhead
 Formal Trailhead
 Informal Trailhead
 Trail Access Point
 Parking - Formal Paved
 Parking - Informal Unpaved
 Restrooms
 Highways
 City Boundary
 Jordan River
 Streams
 Canals
 Light Rail
 Railroad
 County Boundary
 Meander Corridor
 Wetlands (WAIDS 2007)
 Golf Course
 Park
 Publicly Owned Land
 Utility Owned Land
- HV = High Value
 MV = Medium Value
 LV = Low Value
 VLV = Very Low Value
- Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.
 Waypoint locations have been modified for readability.

Segment 1: Bluffdale

Southern County Line to Business Access Road



4.2 Segment 2: Bluffdale/Riverton (15400 South to Bangerter Highway) (GSL segments 4 and 5)

The trail continues north to another riffle at waypoint 4-11 (zero hazard), through rapids at waypoint 4-12 (low hazard), and past an abandoned irrigation dam at waypoint 4-13 (low hazard). Boaters then encounter rapids with flood debris at waypoint 4-14 (medium hazard) and rapids at waypoint 4-15 (low hazard) along the right fork of another split in the river at approximately 15500 South. A barbed-wire fence at waypoint 4-16 (high hazard) and a fallen tree at waypoint 4-17 (high hazard) completely block the river channel under the Rocky Mountain Power high voltage line, presenting a serious challenge to recreational water users.

The trail then passes through a small rapid at waypoint 4-18 (low hazard) and a rapid with an overhanging tree on the right side of the channel at waypoint 4-19 (high hazard). Another barbed-wire fence occurs at waypoint 4-20 (high hazard) where the river rejoins. If users take the left fork of the river, they will encounter a rock weir at waypoint 4-21 (high hazard), a riffle at waypoint 4-22 (low hazard), and rapids at waypoint 4-23 (zero hazard). The river then flows through more rapids at waypoint 4-24 (zero hazard) and waypoint 4-26 (zero hazard) before reaching an earthen weir at waypoint 4-27 (high hazard).

Trail users pass two fence poles in the middle of the river with rapids at waypoint 4-28 (zero hazard) and proceed north through more rapids at waypoints 4-29 (zero hazard), 4-30 (zero hazard), and 4-31 (zero hazard). There is a launch at waypoint 4-32, on the south side of 14600 South, along the west bank of the river. The launch has low value functionality because it is steep and rocky, and has no parking. However, it is a high value location. There is a pipeline at waypoint 4-33 (zero hazard), just upstream of the 146000 South vehicular bridge, waypoint 4-34 (zero hazard).

There are a series of rapids at waypoints 5-01 (zero hazard), 5-02 (zero hazard), and 5-03 (zero hazard), where the river bends to the east. Boaters then pass waypoint 5-04 (zero hazard), the location of an old barbed-wire fence that used to run across river. A barbed-wire

fence used to run across the river at waypoint 5-05 (high hazard), but has since been modified to keep cattle out while allowing water users through, and now represents no hazard. The fence was changed to a wire crossing the river with pieces of pvc pipe dangling down to scare cattle away from the crossing. Water trail users pass waypoint 5-06 (medium hazard), an island with rapids, rapids at waypoints 5-07 (zero hazard) and 5-08 (zero hazard), and another island with rapids at waypoint 5-09 (zero hazard). More rapids are found at waypoints 5-10 (zero hazard) and 5-11 (low hazard), then at waypoint 5-12 (medium hazard) where the current slams boats into overhanging bulrushes as the river curves back to the north. Just upstream of Bangerter Highway, waypoints 5-13 (zero hazard) and 5-14 (low hazard), more rapids are encountered.



Figure 4.2.1 Rapids, flood debris Lower Narrows (WP 4-11)



Figure 4.2.2 Rapids, ladder at Lower Narrows (WP 4-12)



Figure 4.2.7 Fallen tree Lower Narrows (WP 4-17)



Figure 4.2.8 Rapids, channel splits (WP 4-18)



Figure 4.2.15 Barbed fence across river (WP 5-05)



Figure 4.2.16 Island, rapids at Loumis property (WP 5-06)



Figure 4.2.3 Cultural Site, Historic Dam (WP 4-13)



Figure 4.2.4 Rapids with flood debris (WP 4-14)



Figure 4.2.11 Rock weir (WP 4-21)



Figure 4.2.12 Rapids (WP 4-23)



Figure 4.2.5 Barbed fence at Lower Narrows (WP 4-15)



Figure 4.2.6 Barbed fence, river splits (WP 4-16)



Figure 4.2.13 Earthen weir (WP 4-27)



Figure 4.2.14 Questar Pipeline upstream of bridge (WP 4-32)



Figure 4.2.9 Rapids, overhanging tree, hazard at right (WP 4-19)



Figure 4.2.10 Barbed fence, river splits (WP 4-20)



Figure 4.2.17 Rapids, canoe (WP 5-07)



Figure 4.2.18 Rapids, island (WP 5-09)



Figure 4.2.19 Rapids (WP 5-10)



Figure 4.2.20 Rapids slam boat into overhanging bulrush (WP 5-12)

Construction Costs

Table 4.2.1– Construction Costs: Water Trail Segment 2		
Feature Category	Map Code	CIP Budget Implication
Hazard Removal	4.2 A	n/a
Hazard Removal	4.2 B	n/a
Launch/Portage Upgrade	4.2 C	\$15,000
Hazard Removal/Signage	4.2 D	n/a

Issues and Opportunities

- Barbed-wire fences spanning the river at waypoints 4-16 and 4-20 are dangerous for boaters and prevent access.
- There is a rock weir at waypoint 4-21 is dangerous for boaters.
- An earthen weir spans the river at waypoint 4-27 is hazardous for water trail users.
- The existing launch at 4-32 along 14600 South is an important location, but the functionality of the launch is very low.
- There is another barbed-wire fence at waypoint 5-05 that is hazardous for recreational water users.
- At waypoint 5-12, the vegetation should be removed.

Recommendations

4.2 A Barbed-wire Fencing (Waypoints 4-16 and 4-20)

Remove barbed-wire fencing at these points.

4.2 B Earthen Weir (Waypoint 4-27)

Remove earthen weir in this location.

4.2 C Launch at 14600 South (Waypoint 4-32)

Redesign and reconstruct the launch so that it is safe and provides for some designated parking.

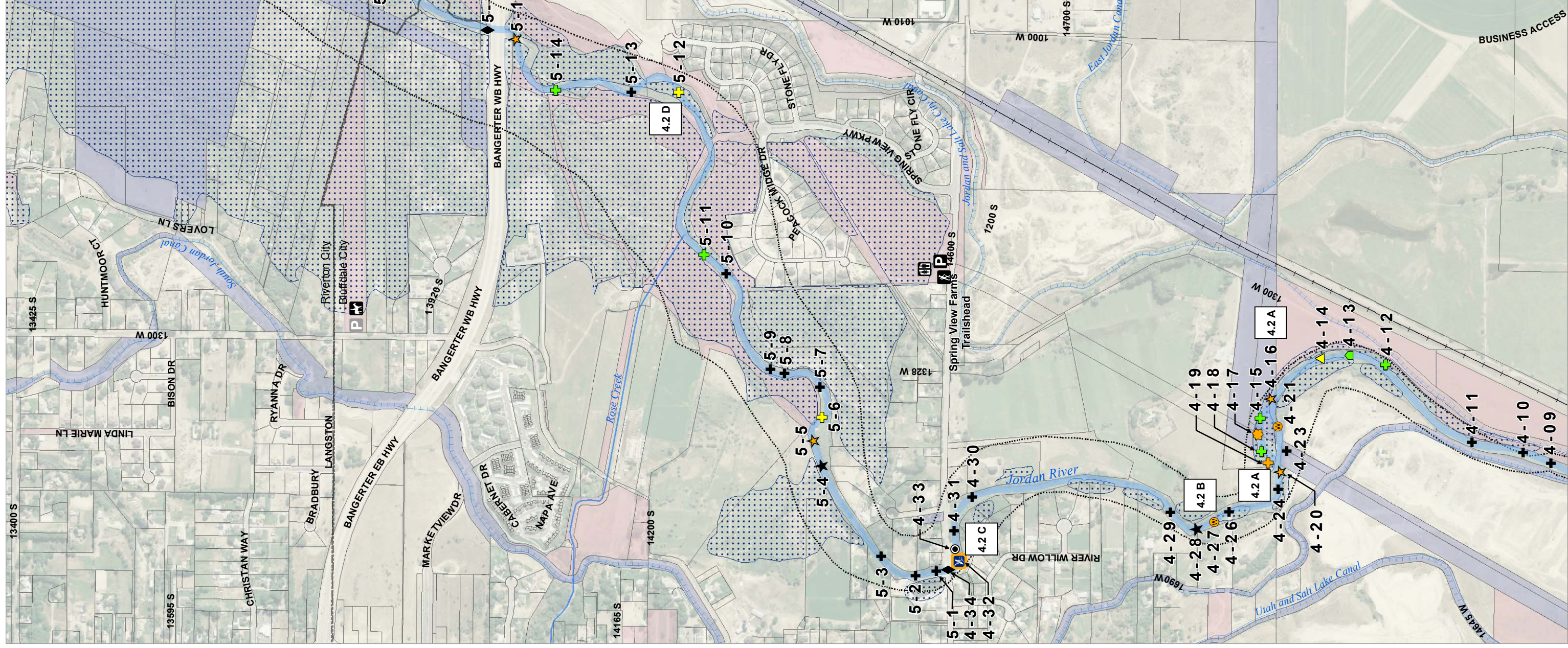
4.2 D Unsafe River Conditions (Waypoint 5-12)

Remove vegetation which may obstruct visibility and provide signing warning boaters that the river changes direction quickly in this location.

Jordan River Trail Master Plan

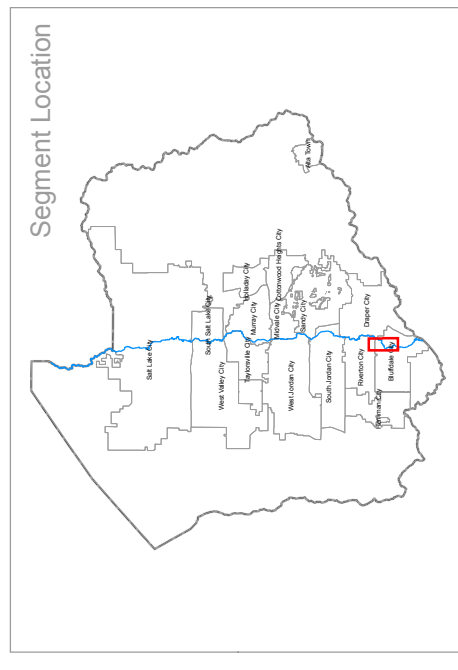
Urban Water Trail Recommendations

- Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
 - Highways
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land
 - Launch - HV Location, HV Function
 - Launch - HV Location, MV Function
 - Launch - HV Location, LV Function
 - Launch - HV Location, VLV Function
 - Launch - MV Location, MV Function
 - Launch - MV Location, LV Function
 - Launch - LV Location, LV Function
 - Launch - VLV Location, LV Function
 - Portage - HV Location, HV Function
 - Portage - HV Location, MV Function
 - Portage - HV Location, VLV Function
 - Portage - High Hazard
 - Potential Launch - HV Location, MV Function
 - Potential Launch - HV Location, LV Function
 - Potential Launch - MV Location, LV Function
 - Potential Launch - LV Location, LV Function
 - Bridge - High Hazard
 - Bridge - Medium Hazard
 - Bridge - Low Hazard
 - Bridge - No Hazard
 - Footbridge - Very High Hazard
 - Footbridge - Medium Hazard
 - Footbridge - Low Hazard
 - Footbridge - No Hazard
 - Railroad Bridge - High Hazard
 - Railroad Bridge - Medium Hazard
 - Railroad Bridge - No Hazard
 - Dam - Very High Hazard
 - Dam - High Hazard
 - Dam - Medium Hazard
 - Dam - Low Hazard
 - Weir - Very High Hazard
 - Weir - High Hazard
 - Flood Debris - Very High Hazard
 - Flood Debris - High Hazard
 - Flood Debris - Medium Hazard
 - Flood Debris - Low Hazard
 - Pipe - Very High Hazard
 - Pipe - High Hazard
 - Pipe - Medium Hazard
 - Pipe - Low Hazard
 - Pipe - No Hazard
 - Cultural Element - Low Hazard
 - Cultural Element - No Hazard
 - Miscellaneous Debris - High Hazard
 - Miscellaneous Debris - Medium Hazard
 - Miscellaneous Debris - Low Hazard
 - Miscellaneous Structure - Very High Hazard
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 - Miscellaneous Structure - Medium Hazard
 - Miscellaneous Structure - Low Hazard
 - Miscellaneous Structure - No Hazard
 - Rapid - High Hazard
 - Rapid - Medium Hazard
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 - Rapid - No Hazard
 - Tributary - No Hazard
 - Tree - Very High Hazard
 - Tree - High Hazard
 - Tree - Medium Hazard
 - Tree - Low Hazard
 - Sign
- HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value
- Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.
- Waypoint locations have been modified for readability.



Segment 2: Bluffdale/Riverton

Business Access Road to Bangerter Highway



4.3 Segment 3: Bluffdale/Riverton/Draper (Bangerter Highway to Vahe Street) (GSL segments 5 and 6)

At the time the original data was collected, another barbed-wire fence ran across the river at waypoint 5-15 (high hazard), immediately south of waypoint 5-16 (zero hazard), the Bangerter Highway bridge. It is believed that this fence has now been removed with the development of Spring View Farms, but this needs to be field verified. North of Bangerter Highway, a sewer line crosses the river at waypoint 5-17 (zero hazard). This line passes overhead, and a distinct foul odor permeates the area for a good distance up and downstream, creating a negative impact for water users. A footbridge then crosses the river at waypoint 5-18 (zero hazard), followed by rapids at waypoint 5-19 (low hazard). As the river begins to bend to the east again, a tree snag and flood debris restrict the channel at waypoint 5-20 (medium hazard), with more flood debris at waypoint 5-21 (medium hazard) as the river begins to flow to the north again. At waypoint 5-22 (zero hazard) north of the hot springs, an old barbed-wire fence used to cross the channel, but no longer poses a hazard.



Figure 4.3.1 Barbed wire fence across river (WP 5-15)

The water trail continues north to a snag at waypoint 5-23 (medium hazard), past waypoint 5-24 (zero hazard) where another old barbed wire fence used to present boaters with a major hazard. The trail then passes through a series of hazards at waypoints 5-25 (low hazard) flood debris, 5-26 (low hazard) a fallen tree, 5-27 (medium hazard) flood debris, 5-28 (low hazard) a fallen tree, 5-29 (low hazard) tree, and 5-30 (medium hazard) a tree in the river current. As the river bends to the west, boaters then pass under waypoint 5-31 (medium hazard), a footbridge for the River Bend Golf Course. This bridge becomes a high hazard during high water flows from spring runoff or releases from Utah Lake. There is an undeveloped dirt bank boat launch at waypoint 6-01 near 12600 South in Draper. The launch has low functionality and a high value location.



Figure 4.3.2 Flood debris snag (WP 5-20)

Just downstream from the launch, there is a snag in the center of the channel at waypoint 6-02 (medium hazard), but is easy to clear. Trail users must then navigate past waypoint 6-03 (low hazard) where bank slumping and assorted debris makes the channel more difficult to clear. As the river curves to the north, waypoint 6-04 (medium hazard) boaters encounter three islands, flood debris, cattail islands, and

cattails growing in mid-channel, a tree and boulders in the current, and boulders fallen from bluff-slumps at waypoint 6-05 (medium hazard). The trail then proceeds north under waypoint 6-06 (zero hazard), the 12600 South street vehicular bridge, and a tree with debris flow north of 12600 south at waypoint 6-07 (medium hazard). Immediately downstream, boaters pass through waypoints 6-08 (low hazard), concrete riprap in the river channel mixed with boulders where garbage is piling up, and 6-09 (zero hazard), the historic footbridge in Riverton at 12300 South.



Figure 4.3.3 Flood debris in path of turn (WP 5-21)



Figure 4.3.4 Snag on river (WP 5-23)



Figure 4.3.5 Flood debris (WP 5-27)



Figure 4.3.6 Fallen tree (WP 5-28)



Figure 4.3.7 Fallen tree (WP 5-29)



Figure 4.3.8 Boat launch, undeveloped ramp (WP 6-01)



Figure 4.3.9 Snag in center of channel (WP 6-02)



Figure 4.3.10 Slumping bank (WP 6-03)



Figure 4.3.11 Three islands, flood debris, cattails (WP 6-04)



Figure 4.3.12 Flood debris, boulders from slumps (WP 6-05)



Figure 4.3.13 Vehicular bridge at 12600 South (WP 6-06)



Figure 4.3.14 Tree flood debris north of 12600 South (WP 6-07)



Figure 4.3.15 Concrete riprap in channel, boulders, debris (WP 6-08)



Figure 4.3.16 Historic footbridge at 12300 South (WP 6-09)

Issues and Opportunities

- A barbed-wire fence spans the river at waypoint 5-15, causing a significant hazard for boaters.
- The footbridge in Riverbend Golf Course at waypoint 5-31 is too low.
- The existing launch at waypoint 6-01 near 12600 South in Draper is an important location but is not functional.

Recommendations

4.3 A Riverbend Golf Course Bridge (Waypoint 5-31)

An existing footbridge at the golf course is low and causes unsafe conditions during high water events. This bridge should be replaced or reconstructed so there is more clearance for boaters.

4.3 B Launch Site at 12600 South (Waypoint 6-01)

Although this is a highly valued launch site, there is no safe access for water trail users from 12600 South, and there is already an existing trailhead with parking facilities on the north side of 12600 South at Jordan River Rotary Park. It is recommended that a new launch be constructed at this trailhead.

4.3 C Vegetation Near Islands (Waypoint 6-04)

The vegetation near islands are choking the waterway and present a hazard. The islands are some of the last remnants of the pre-settlement condition of the Jordan River. The river channel should be cleared of vegetation and debris which presents the boating hazard.

Construction Costs

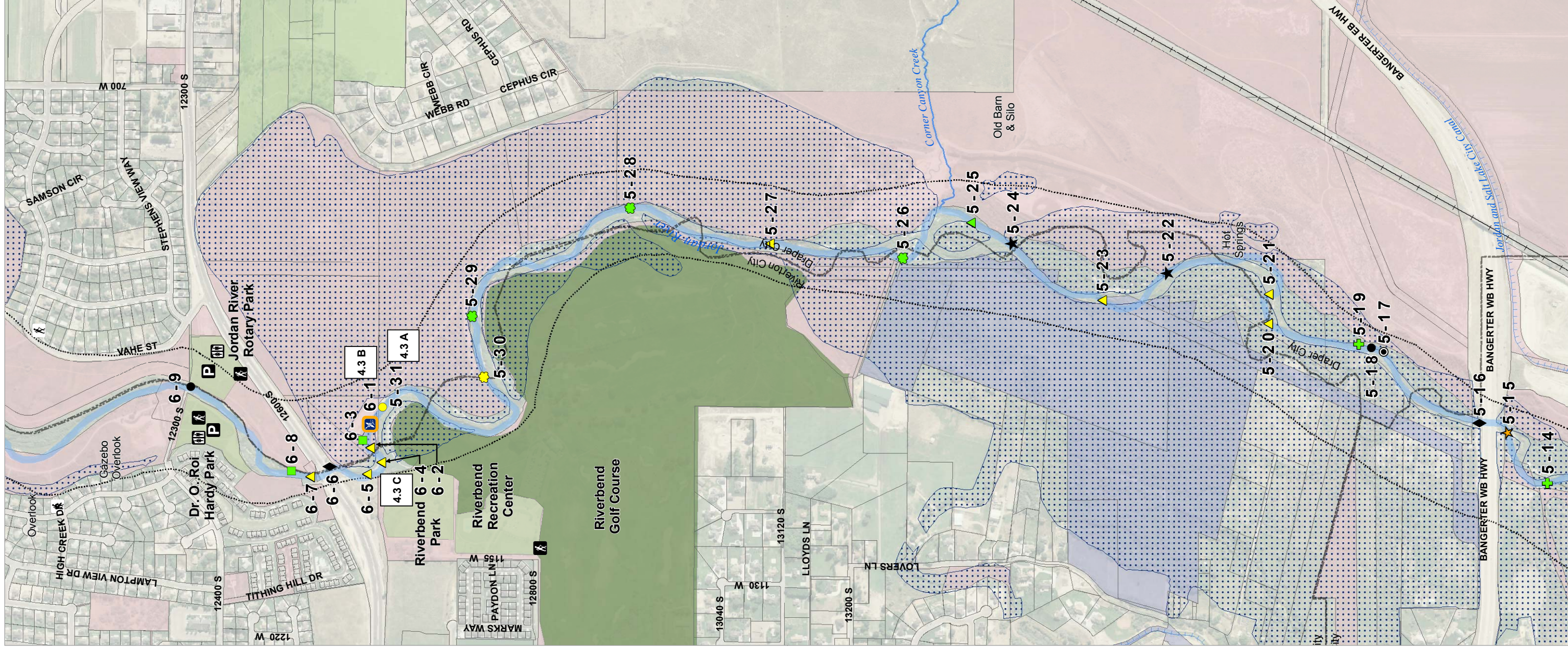
Feature Category	Map Code	CIP Budget Implication
Bridge	4.3 A	\$180,000
Launch Upgrade/Trailhead	4.3 B	\$10,000
Hazard Removal	4.3 C	n/a

Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
- Launch - HV Location, MV Function
- Launch - HV Location, LV Function
- Launch - HV Location, VLV Function
- Launch - MV Location, MV Function
- Launch - MV Location, LV Function
- Launch - LV Location, LV Function
- Launch - VLV Location, LV Function
- Portage - HV Location, LV Function
- Portage - HV Location, VLV Location
- Portage - LV Location, HV Function
- Portage - LV Location, VLV Function
- Portage - High Hazard
- Potential Launch - HV Location, MV Function
- Potential Launch - HV Location, LV Function
- Potential Launch - HV Location, VLV Function
- Potential Launch - LV Location, LV Function
- Bridge - High Hazard
- Bridge - Medium Hazard
- Bridge - Low Hazard
- Bridge - No Hazard
- Footbridge - Very High Hazard
- Footbridge - Medium Hazard
- Footbridge - Low Hazard
- Footbridge - No Hazard
- Railroad Bridge - High Hazard
- Railroad Bridge - Medium Hazard
- Railroad Bridge - No Hazard
- Dam - Very High Hazard
- Dam - High Hazard
- Dam - Medium Hazard
- Dam - Low Hazard
- Weir - Very High Hazard
- Weir - High Hazard
- Flood Debris - Very High Hazard
- Flood Debris - High Hazard
- Flood Debris - Medium Hazard
- Flood Debris - Low Hazard
- Pipe - Very High Hazard
- Pipe - High Hazard
- Pipe - Medium Hazard
- Pipe - Low Hazard
- Pipe - No Hazard
- Cultural Element - Low Hazard
- Cultural Element - No Hazard
- Miscellaneous Debris - High Hazard
- Miscellaneous Debris - Medium Hazard
- Miscellaneous Debris - Low Hazard
- Miscellaneous Structure - Very High Hazard
- Miscellaneous Structure - High Hazard
- Miscellaneous Structure - Medium Hazard
- Miscellaneous Structure - Low Hazard
- Miscellaneous Structure - No Hazard
- Miscellaneous Structure - No Hazard
- Rapid - High Hazard
- Rapid - Medium Hazard
- Rapid - Low Hazard
- Rapid - No Hazard
- Tributary - No Hazard
- Tree - Very High Hazard
- Tree - High Hazard
- Tree - Medium Hazard
- Tree - Low Hazard
- Sign

HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value



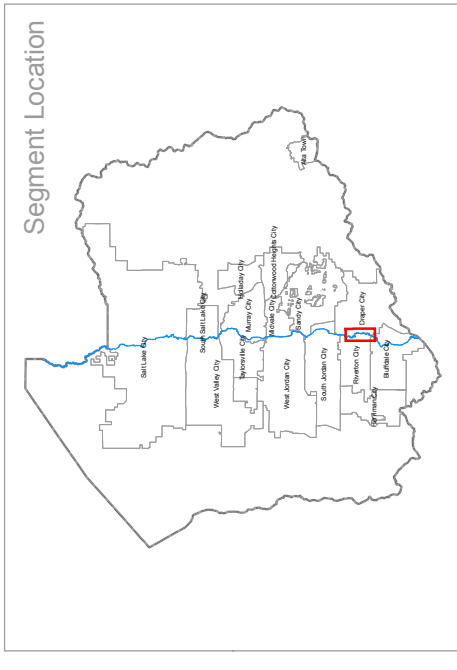
Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

Waypoint locations have been modified for readability.

Segment 3: Bluffdale/Riverton

Draper

Bangerter Highway to Vahe Street



4.4 Segment 4: Riverton/Draper/South Jordan (Vahe Street to Warm Creek Road) (GSL segments 6 and 7)

The Urban Water Trail continues north past waypoint 6-10 (high hazard) where a fallen dead tree covers around half of the channel, and waypoint 6-12 (low hazard) where flood debris is found in the middle of the channel. The river is wide at this point, but the pile is large. Further north, near the Draper and South Jordan City boundaries at waypoint 6-13 (medium hazard), one large tree cross the current and another tree lies parallel to current, blocking access. Just downstream, more trees block the whole river at waypoint 6-14 (very high hazard). As the river swings around the north side of Riverfront Park, a fallen tree lies across the channel at waypoint 6-15 (high hazard), near 11000 South in South Jordan.

Boaters proceed north past waypoint 6-16 (zero hazard), the Willow Creek confluence, to waypoint 6-17, a potential launch at 10600 South. This site was rated with very low functionality, and a high value location, but a launch site at Shield's Lane would be more appropriate considering the potential launch site at 9000. The river then flows under waypoint 6-18 (zero hazard), the 10600 South bridge, and through flood debris located east of Mulligan's Golf at waypoint 7-01 (high hazard).



Figure 4.4.1 Fallen dead tree in channel (WP 6-10)



Figure 4.4.2 Flood debris in middle of channel (WP 6-12)



Figure 4.4.3 Large tree in current blocking access (WP 6-13)



Figure 4.4.4 Tree blocking whole river (WP 6-14)



Figure 4.4.5 Fallen tree across channel at 11000 South (WP 6-15)



Figure 4.4.6 Willow Creek Confluence (WP 6-16)



Figure 4.4.7 Bridge at 10600 South, potential launch site (WP 6-17)



Figure 4.4.8 Vehicular bridge at 10600 South (WP 6-18)



Figure 4.4.9 Flood debris north of 10600 South (WP 7-01)

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Signage	4.4 B	\$500

Issues and Opportunities

- There is a potential launch site at waypoint 6-17.

Recommendations

4.4 A Launch Point (Waypoint 6-17)

A previously considered launch point at this location is not recommended. Rather consolidate launches previously considered at 10600 South and 9000 South to a location identified as waypoint 7-02a at Shields Lane (See Recommendation 4.5 A in the next section).

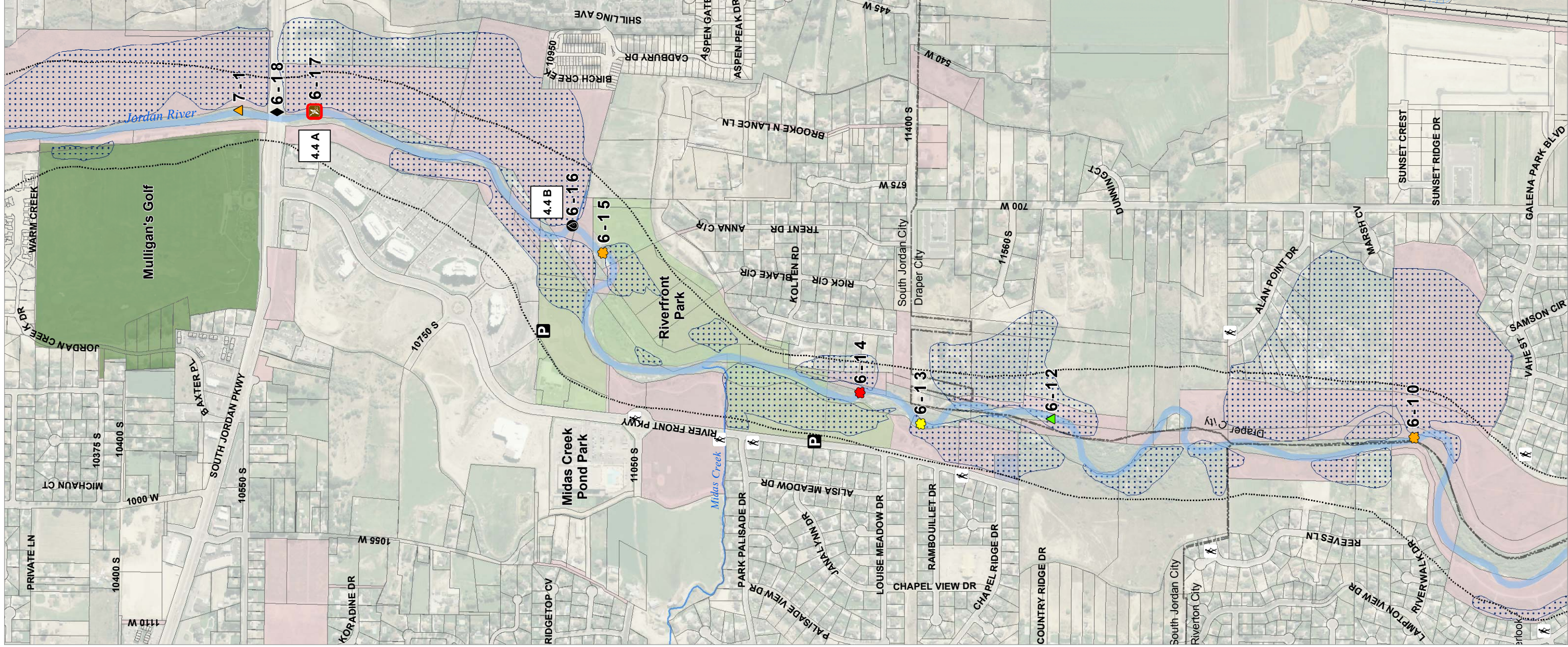
4.4 B Confluence of Willow Creek (Waypoint 6-16)

Provide a sign warning boaters of a potential change in water flow due to the confluence.

Jordan River Trail Master Plan

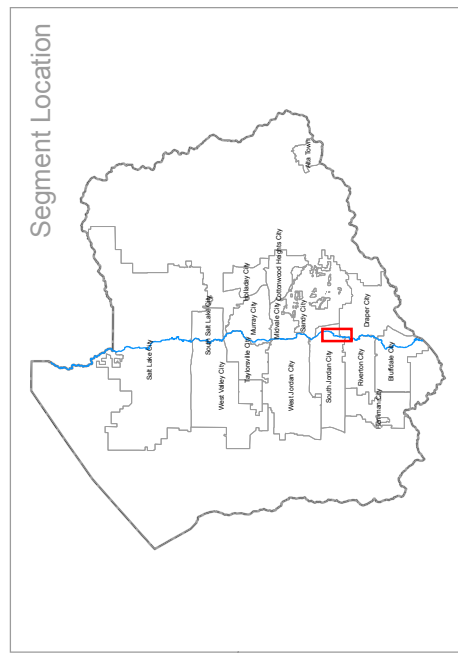
Urban Water Trail Recommendations

- Launch - HV Location, HV Function
 - Launch - HV Location, MV Function
 - Launch - HV Location, LV Function
 - Launch - MV Location, VLV Function
 - Launch - MV Location, MV Function
 - Launch - LV Location, LV Function
 - Launch - VLV Location, VLV Function
 - Launch - VLV Location, LV Function
 - Portage - HV Location, LV Function
 - Portage - HV Location, LV Function
 - Portage - LV Location, HV Function
 - Portage - LV Location, VLV Function
 - Portage - High Hazard
 - Potential Launch - HV Location, MV Function
 - Potential Launch - HV Location, LV Function
 - Potential Launch - HV Location, VLV Function
 - Potential Launch - LV Location, LV Function
 - Bridge - High Hazard
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 - Bridge - Low Hazard
 - Bridge - No Hazard
 - Footbridge - Very High Hazard
 - Footbridge - Medium Hazard
 - Footbridge - Low Hazard
 - Footbridge - No Hazard
 - Railroad Bridge - High Hazard
 - Railroad Bridge - Medium Hazard
 - Railroad Bridge - No Hazard
 - Dam - Very High Hazard
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 - Flood Debris - Low Hazard
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 - Pipe - Medium Hazard
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 - Miscellaneous Structure - No Hazard
 - Rapid - High Hazard
 - Rapid - Medium Hazard
 - Rapid - Low Hazard
 - Rapid - No Hazard
 - Tributary - No Hazard
 - Tree - Very High Hazard
 - Tree - High Hazard
 - Tree - Medium Hazard
 - Tree - Low Hazard
 - Tree - No Hazard
 - Sign
- HV = High Value
 MV = Medium Value
 LV = Low Value
 VLV = Very Low Value
- Equestrian Trailhead
 - Formal Trailhead
 - Informal Trailhead
 - Trail Access Point
 - Parking - Formal Paved
 - Parking - Informal Unpaved
 - Restrooms
 - Highways
 - City Boundary
 - Jordan River
 - Streams
 - Canals
 - Light Rail
 - Railroad
 - County Boundary
 - Meander Corridor
 - Wetlands (WAIDS 2007)
 - Golf Course
 - Park
 - Publicly Owned Land
 - Utility Owned Land
- Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.
- Waypoint locations have been modified for readability.



Segment 4: Riverton/Draper/ South Jordan

Valle Street to Warm Creek Road



4.5 Segment 5: South Jordan/ Sandy/West Jordan/Midvale (Warm Creek Road to 8450 South) (GSL segments 7 and 8)

Boaters then travel north under Shield's Lane (10000 South), past waypoint 7-02 (zero hazard) where a sewer drain pipe crosses the river underground near the southern end of the River Oaks Golf Course. Waypoint 7-2a, created during this planning process, would be a more appropriate launch site that could be developed instead of launches at waypoints 6-17 at 10600 South and 7-13 at 9000 South. About 100 yards downstream of the sewer line, water trail users encounter some flood debris in the outer edge of the channel at waypoint 7-03 (low hazard). The river then flows past waypoint 7-04, a warning sign informing boaters that they are approaching a dangerous area and that all boats must exit. This warning of the 9600 South dam has high functionality and a high value location. A little further north, another sign at waypoint 7-05 warns users to "Stop: Exit Here". This sign has medium functionality and a medium value location. A final sign at 7-06 warns that no canoes are allowed in this area. This sign has medium functionality and a medium value location.

Water trail users must exit at waypoint 7-07, the portage for the 9600 South dam. This portage has low functionality, but is a high value location. The river flows through waypoint 7-08 (very high hazard), the 9600 South irrigation dam, which users must portage around and then re-enter at waypoint 7-09. This portage on the north side of 9600 South dam has low value functionality, and a high value location, the County should coordinate with the River Oaks Golf Course to improve the quality of these portage points. The current re-entry portage has steep banks. The water trail then continues north past two footbridges in the River Oaks Golf Course at waypoints 7-10 (zero hazard) and 7-11 (zero hazard) and under the 9000 South bridge at waypoint 7-12 (zero hazard).

There is a potential launch site at the 9000 South bridge, waypoint 7-13 that was originally designated with very low value functionality and a high value location, but this launch could be consolidated along with waypoint 6-17 to one launch location at Shield's Lane. Downstream of



Figure 4.5.1 Flood debris in outer channel (WP 7-03)

the bridge, there is a rapid at waypoint 8-01 (zero hazard) and a waterfall at waypoint 8-01a (high hazard), north of the 9000 South substation. This waterfall is created by a sewer pipe. The right side of the channel is more passable - the left side is blocked, and rocks divert boats into the trees.



Figure 4.5.2 Warning sign "dangerous area" (WP 7-04)



Figure 4.5.3 Warning sign "exit here" (WP 7-05)



Figure 4.5.4 Warning sign "no canoes" (WP 7-06)



Figure 4.5.5 Portage at 9600 South (WP 7-07)



Figure 4.5.6 Irrigation dam at 9600 South (WP 7-08)



Figure 4.5.7 Launch north side of 9600 South dam (WP 7-09)



Figure 4.5.8 River Oaks Golf Course footbridge (WP 7-10)



Figure 4.5.9 River Oaks Golf Course footbridge (WP 7-11)



Figure 4.5.10 Vehicular bridge at 9000 South (WP 7-12)



Figure 4.5.11 Waterfall created by sewer pipe (WP 8-01A)

Issues and Opportunities

- The existing portage at waypoint 7-07 is an important location, but it has poor functionality.
- The existing launch at waypoint 7-09 is also important, and has poor functionality.
- There is a potential launch at waypoint 7-13, near 9000 south that is important to the water trail corridor.
- A sewer pipe at waypoint 8-01a creates a dangerous situation for boaters.

Recommendations

4.5 A Launch at Shields Lane (Waypoint 7-02a)

Provide a launch and parking in this location to replace launches previously considered at 10600 South and 9000 South.

4.5 B Portage for the 9600 South Dam (Waypoint 7-09)

Redesign and reconstruct this highly needed portage allowing portage (takeout and launch) around the dam. Coordination with the canal company and golf course will be needed.

4.5 C Sewer Pipe (Waypoint 8-01a)

This sewer pipe should be lowered or relocated so that it does not create hazardous rapids/waterfalls.

Construction Costs

Table 4.5.1- Construction Costs: Water Trail Segment 5		
Feature Category	Map Code	CIP Budget Implication
Launch/Trailhead	4.5 A	\$185,000
Portage	4.5 B	\$5,000
Hazard Removal	4.5 C	

4.6 Segment 6: West Jordan/Midvale/Murray/Taylorsville (8450 South to Winchester Street) (GSL segments 8 and 9)

Waypoint 8-02 (zero hazard) is at the Bingham Creek and Plumb Creek confluence, and waypoint 8-03 (zero hazard) is an historic bridge in West Jordan. Boaters then encounter a fiber optic line at waypoint 8-04 (high hazard) just south of the 7800 South bridge, waypoint 8-05 (zero hazard). The river passes through more dangerous hazards at waypoint 8-06 (very high hazard), the Midvale slag millrace and waypoint 8-07 (high hazard), and the railroad crossing. The millrace will be removed with redevelopment of the Sharon Steel and Bingham Junction sites. The railroad crossing is a high hazard due to the collection of flood debris.

The trail continues past waypoint 8-08 (low hazard) where the sewage plant discharges into the river at approximately 7400 South. The level of this hazard increases with the amount of flow discharged by the sewage plant, and can catch boaters off guard.

Nearing the 7200 South Trailhead, there is a potential launch site at waypoint 8-09 with low functionality and a low value location. The river flows under waypoint 8-10 (zero hazard), the 7200 South bridge, and shortly after entering Murray City boundaries trail users encounter waypoint 8-11, a warning sign stating "All Boats Exit". This sign, south of the Winchester Street bridge has high functionality and a high value location. Boaters must portage at waypoint 8-12, which has low functionality and a high value location, with a long connection around the hazards north of Winchester Street. This portage is too steep and is heavily vegetated, making access difficult. There are more warning signs at waypoint 9-01a. The river proceeds north under waypoint 9-01 (high hazard), the 6400 South vehicular bridge. The hydraulics under this bridge can suck boaters in and send them into hazards downstream. There are major rapids at waypoint 9-02 (very high hazard) where water and sewer pipelines cross the river, and at waypoint 9-03 (high hazard). An overhead pipeline for a Qwest cable crossing is located at waypoint 9-04 (zero hazard).

Continuing north near Winchester Park, there is a potential launch site and portage at 9-05 (also evaluated as waypoint 8-13 in GSL/NPS study), with low functionality and a high value location. At waypoint 9-06 there is an existing launch with three launch decks. This site has low functionality and a low value location because of the long distance from the upstream portage (waypoint 8-12) and to the parking area.



Figure 4.6.1 Brigham Creek Confluence (WP 8-02)



Figure 4.6.2 West Jordan Historic Bridge (WP 8-03)



Figure 4.6.3 Qwest fiber-optic pipeline before bridge (WP 8-04)



Figure 4.6.10 Portage, sign (WP 8-11)



Figure 4.6.11 Potential launch site at 6400 South (WP 8-12)



Figure 4.6.4 Vehicular bridge at 7800 South (WP 8-05)



Figure 4.6.5 Midvale slag millrace (WP 8-06)



Figure 4.6.6 Railroad crossing bridge (WP 8-07)



Figure 4.6.7 Sewage plant discharge at 7400 South (WP 8-08)



Figure 4.6.8 Potential launch site at 7200 South (WP 8-09)



Figure 4.6.9 Warning sign "all boats exit" (WP 8-10)

Issues and Opportunities

- A fiber optic line at waypoint 8-08 creates a high hazard for water trail users.
- Waypoint 8-09 is not an important location for a launch and is not very functional.
- The existing portage at 8-12 is important to the trail corridor but has poor functionality.
- Need to have barrels near portage 8-12 because of dangers north of Winchester bridge.
- There is a good potential launch site at waypoint 9-05 that would be closer to portage 8-12 than the launch at 9-06.
- The launch at 9-06 should be abandoned for water trail use, and adapted for use with the paved, multi-purpose trail as overlooks and/or fishing docks if appropriate.

Recommendations

4.6 A Fiber Optic Line (Waypoint 8-04)

The fiber optic line that crosses in the river creates changes in hydraulics that are dangerous. This hazard should be signed, indicating that changes in water flow affect conditions in the area.

4.6 B Midvale Slag Millrace and Railroad Crossing (Waypoint 8-06)

This area is typically high hazard because of flood debris that accumulates and affects water flows and protruding metal pieces. These structures will be removed with redevelopment in the area; however, checking and cleaning the channel regularly is recommended.

Providing signing in this area indicating that water may be contaminated due to soil pollutants.

4.6 C TRAX Bridge Reconstruction (Waypoint 8-07)

The railroad bridge will be reconstructed for expanded TRAX development. Coordinate with UTA to remove any obstructions in the river.

4.6 D Hazardous Conditions (Waypoint 8-08)

Provide signing to warn boaters of river conditions which force boaters

against the banks.

4.6 E Launch Near the 7200 South Trailhead (Waypoint 8-09)

A launch is not recommended in this location.

4.6 F Portage Under 6400 South/Winchester St. (Waypoints 8-09 and 9-06)

This portage exits on the south side of the street, crosses under the 6400 South bridge through a tunnel, and launches at waypoint 9-5. The portage is needed to avoid dangerous conditions under the bridge. Provide barrels prohibiting boaters from going under the bridge and forcing them to use the portage.

Portages/launches at wayside 8-09 and 9-06 are not recommended or needed. The launch at 9-06 could be redesigned and adapted for users on the paved trail as an overlook and/or fishing dock.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Signage	4.6 A	\$500
Signage	4.6 B	\$500
Hazard Removal	4.6 C	
Signage	4.6 D	\$1,000
Launch	4.6 E	N/A
Safety Device, Launch/Portage Consolidation	4.6 F	\$15,000

Jordan River Trail Master Plan

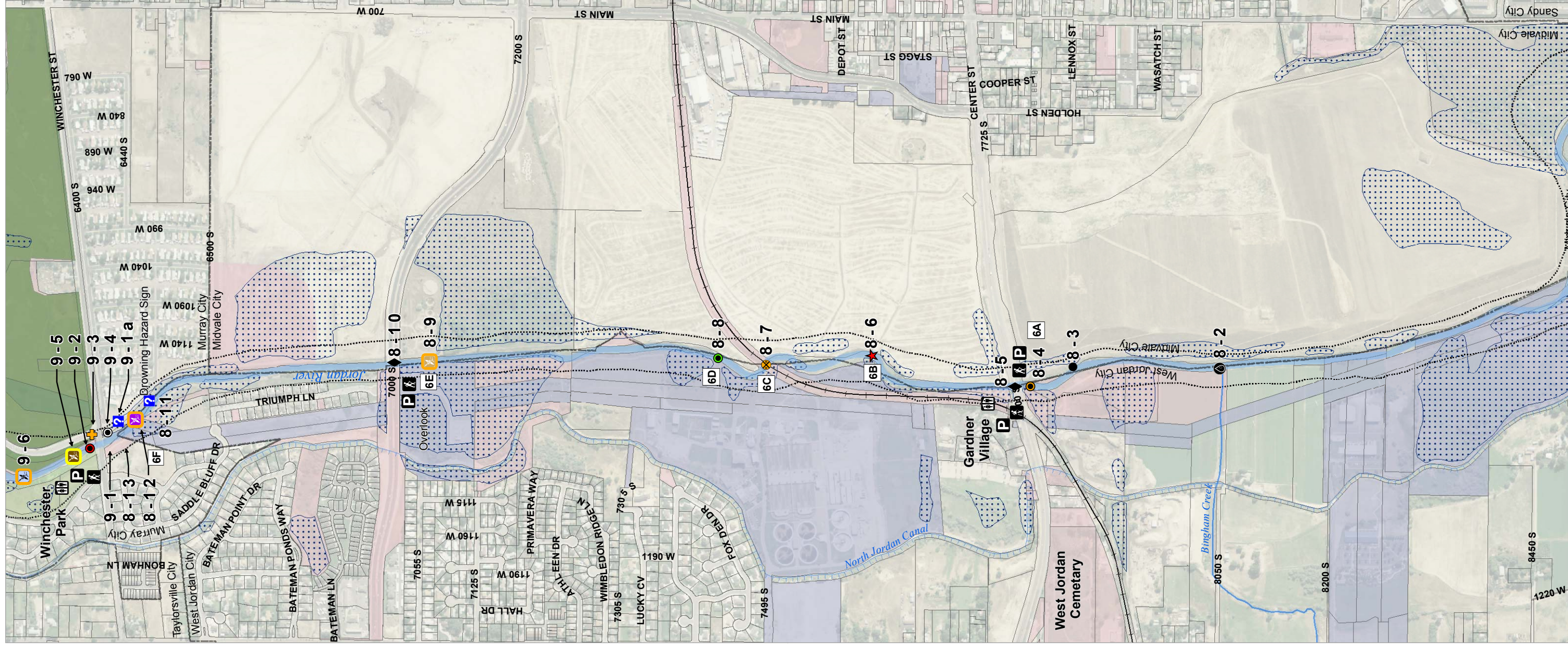
Urban Water Trail Recommendations

	Launch - HV Location, HV Function		Equestrian Trailhead
	Launch - MV Location, MV Function		Formal Trailhead
	Launch - LV Location, LV Function		Informal Trailhead
	Launch - VLV Location, VLV Function		Trail Access Point
	Portage - HV Location, HV Function		Parking - Formal Pavement
	Portage - MV Location, MV Function		Parking - Informal Unpaved
	Portage - LV Location, LV Function		Restrooms
	Portage - VLV Location, VLV Function		Highways
	Potential Launch - HV Location, MV Function		City Boundary
	Potential Launch - MV Location, LV Function		Jordan River
	Potential Launch - LV Location, VLV Function		Streams
	Bridge - High Hazard		Canals
	Bridge - Medium Hazard		Light Rail
	Bridge - Low Hazard		Railroad
	Bridge - No Hazard		County Boundary
	Footbridge - Very High Hazard		Meander Corridor
	Footbridge - Medium Hazard		Wetlands (WAIDS 2007)
	Footbridge - Low Hazard		Golf Course
	Footbridge - No Hazard		Park
	Railroad Bridge - High Hazard		Publicly Owned Land
	Railroad Bridge - Medium Hazard		Utility Owned Land
	Railroad Bridge - No Hazard		
	Dam - Very High Hazard		
	Dam - High Hazard		
	Dam - Medium Hazard		
	Dam - Low Hazard		
	Weir - Very High Hazard		
	Weir - High Hazard		
	Flood Debris - Very High Hazard		
	Flood Debris - High Hazard		
	Flood Debris - Medium Hazard		
	Flood Debris - Low Hazard		
	Pipe - Very High Hazard		
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	Miscellaneous Debris - High Hazard		
	Miscellaneous Debris - Medium Hazard		
	Miscellaneous Debris - Low Hazard		
	Miscellaneous Structure - Very High Hazard		
	Miscellaneous Structure - High Hazard		
	Miscellaneous Structure - Medium Hazard		
	Miscellaneous Structure - Low Hazard		
	Miscellaneous Structure - No Hazard		
	Rapid - High Hazard		
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	Rapid - No Hazard		
	Tributary - No Hazard		
	Tree - Very High Hazard		
	Tree - High Hazard		
	Tree - Medium Hazard		
	Tree - Low Hazard		
	Sign		

HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value

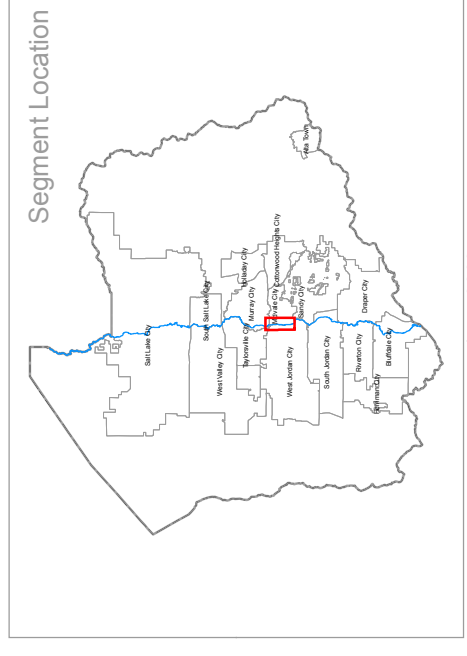
Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

Waypoint locations have been modified for readability.



Segment 6: West Jordan/Midvale/ Murray/Taylorsville

8450 South to Winchester Street



4.7 Segment 7: Murray/Taylorsville (Winchester Street to Murray Taylorsville Road) (GSL segment 9)

Recreational boaters travel north past Winchester Park and the Murray Parkway Golf Course, under a footbridge at waypoint 9-07 (zero hazard) and under the Interstate 215 vehicular bridge at waypoint 9-08 (zero hazard). North of Interstate 215, just west of Cottonwood Grove Park, there is an existing launch site at waypoint 9-09. This site has community access boat ramp potential, with low existing functionality and a low value location because of the close proximity to the existing launch at Winchester Street. The trail then flows under the Bullion Street vehicular bridge, waypoint 9-10 (zero hazard), and under a footbridge south of 5400 south at waypoint 9-11 (zero hazard).

Further downstream near Walden Park, there are underwater pipelines across the channel at waypoints 9-12 (medium hazard) and 9-13 (medium hazard).

The 5400 South vehicular bridge abutment has debris piling up, waypoint 9-14 (zero hazard), which could be a potential hazard in the future. North of the 5400 South bridge, there is a boat launch at waypoint 9-15 with high functionality and a high value location. As the water trail flows north and east, there is a footbridge near 5200 South at waypoint 9-16 (zero hazard) and a section near the Kennecott Environmental Center where trees have fallen into the channel and block almost half of the river, waypoint 9-17 (low hazard).



Figure 4.7.5 Potential launch site at 6400 South (WP 9-05)



Figure 4.7.6 Launch decks at 6400 South (WP 9-06)



Figure 4.7.7 Vehicular bridge at Interstate 215 (WP 9-08)



Figure 4.7.8 Potential launch site at Bouillon Street (WP 9-09)



Figure 4.7.9 Vehicular bridge at Bouillon Street (WP 9-10)



Figure 4.7.10 Vehicular bridge at 5400 South, flood debris (WP 9-14)



Figure 4.7.1 Vehicular Bridge at 6400 South (WP 9-01)



Figure 4.7.2 Pipeline, bridge at 6400 South (WP 9-02)



Figure 4.7.3 Rapids at 6400 South (WP 9-03)



Figure 4.7.4 Qwest cable crossing river (WP 9-04)



Figure 4.7.11 Boat launch at 5400 South (WP 9-15)



Figure 4.7.12 Fallen trees block half of channel near Murray Nature Center (WP 9-17)

Issues and Opportunities

- There is a good potential launch site at waypoint 9-09 that has value as a potential community launch for larger groups.
- There are underwater pipelines at waypoint 9-12 and 9-13 that are dangerous for boaters.

Recommendations

4.7 A Community Boat Launch in Murray (Waypoint 9-09)

This launch is in disrepair and is not needed. The launch should be removed.

4.7 B Underwater Pipelines (Waypoints 9-12 and 9-13)

Underwater pipelines at these locations cause disturbances in the water. Provide signing indicating that depending on water level, flow characteristics may change.

4.7 C Boat Launch at (Waypoint 9-15)

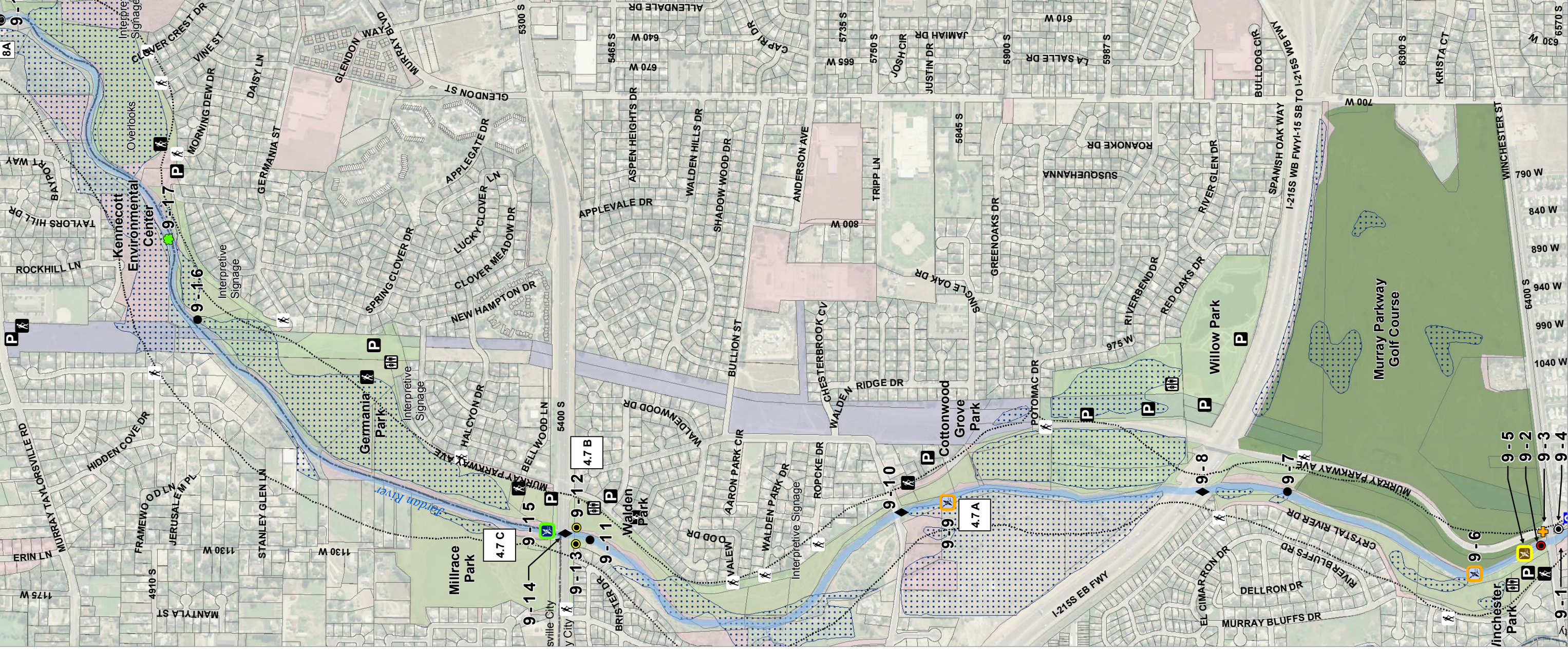
This launch has high value as a community launch point. It should be redesigned and reconstructed.

Construction Costs

Table 4.7.1– Construction Costs: Water Trail Segment 7		
Feature Category	Map Code	CIP Budget Implication
Launch Removal	4.7 A	\$5,000
Signage	4.7 B	\$500
Launch Upgrade	4.7 C	\$10,000

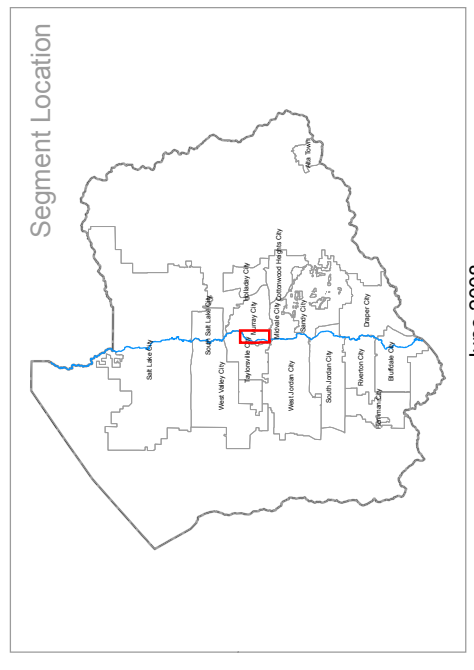
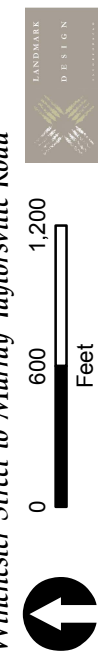
Jordan River Trail Master Plan

Urban Water Trail Recommendations



Segment 7: Murray/Taylorville Road

Winchester Street to Murray Taylorville Road



4.8 Segment 8: Murray/Taylorville/West Valley/South Salt Lake (Murray Taylorville Road to 3300 South) (GSL segments 9 and 10)

Little Cottonwood Creek joins the Jordan River at waypoint 9-18 (zero hazard), just south of Arrowhead Park. Just north of this confluence, there is a launch site with very low functionality and a low value location at waypoint 9-19. This launch was closed because of inappropriate use by children on bicycles. Boaters continue north under waypoint 9-20 (zero hazard), the 4800 South vehicular bridge, and past waypoint 9-20a, a potential launch site with very low functionality and a high value location. The water trail then flows under a footbridge near 4800 South, waypoint 9-21 (zero hazard) and to waypoint 9-22, a portage around the 4600 South weir, with very low functionality and a low value location. The location of this take-out for the portage is too close to the weir.

The 4600 South weir, waypoint 9-23 (very high hazard), is one of the most dangerous hazards on the Jordan River, with a drop of 5 feet at the time of the data collection. Users then launch again using the portage at waypoint 9-24, just downstream of the weir, which has very low functionality and a high value location. As the water trail continues, flood debris is piled up in the middle of the channel near the 4500 South vehicular bridge at waypoint 9-25 (zero hazard).

Big Cottonwood Creek then joins the Jordan River at waypoint 9-26 (zero hazard), east of Meadowbrook Golf Course. Near the north end of the Golf Course at waypoint 9-27 (low hazard) there is a fallen tree on Rocky Mountain Power's property that covers almost half of the river channel. As the river trail winds to the west and north towards Meadowbrook Parkway, boaters pass under waypoint 9-28 (zero hazard), a footbridge, and through rapids at waypoint 9-29 (zero hazard). Near the 3900 South vehicular bridge at waypoint 9-30 (very high hazard), flood debris has collected at the abutments, and clearance is too low during high water flow.

Just downstream near General Holm Park, waypoint 9-31, there is a potential launch site, although it is a long haul to the parking area. This site has low functionality and a high value location. Users then pass waypoint 10-01 which was rated as a medium hazard because of a van that was submerged almost 90% into the middle of the river. Most of the vehicle body has since been removed, and this is now a low hazard. The trail then takes users past flood debris at waypoints 10-02 (high hazard), which has woody debris in two clusters with lots of garbage, and waypoint 10-03 (high hazard) where some trees have fallen along the right side of the river and debris has collected in the middle.



Figure 4.8.1 Launch site at 4800 South (WP 9-19)



Figure 4.8.2 Vehicular and foot bridges at 4800 South (WP 9-20)



Figure 4.8.9 Potential launch site at 3900 South (WP 9-31)



Figure 4.8.10 Van submerged in middle of river (WP 10-01)



Figure 4.8.3 Portage near 4600 South dam (WP 9-22)



Figure 4.8.4 Weir at 4600 South (WP 9-23)



Figure 4.8.11 Wood debris in river, lots of garbage (WP 10-02)



Figure 4.8.12 Trees on right side of river, debris in middle of river (WP 10-03)



Figure 4.8.5 Launch at 4600 South (WP 9-24)



Figure 4.8.6 Flood debris at 4500 South bridge (WP 9-25)



Figure 4.8.7 Fallen tree covers half of channel (WP 9-27)



Figure 4.8.8 Flood debris at vehicular bridge at 3900 South (WP 9-30)

Issues and Opportunities

- The existing launch site at waypoint 9-19 in Arrow Park that has been abandoned. This site is important to the urban water trail corridor and needs to be restored to a functioning launch.
- There is a potential launch site at waypoint 9-20a. The existing portage at waypoint 9-22 is in a poor location and has poor functionality. There needs to be a safe portage in this area to get users around the 4600 South weir.
- The existing portage south of the 4600 South weir is in a good location but is not safely functional.
- North of General Holm Park, at waypoint 9-31 is a potential launch site. It has a high value location but is a long distance from parking.

Recommendations

4.8 A Existing Boat Launch (Waypoint 9-19)

Remove this boat launch, and move to waypoint 9-20a. This is a highly desirable location; design and construct a new launch waypoint 9-20a. This locations also functions as a logical end-point for boaters who do not want to portage at the 4600 South Weir.

4.8 B Portage at 4600 South Weir (Waypoints 9-22 and 9-24)

Provide barrels prohibiting boaters from encountering highly dangerous conditions and forcing them to use the portage/launch at waypoints 9-22 and 9-24.

4.8 C Hazardous Water Conditions at 3900 South (Waypoints 9-28a, 9-30, and 9-31)

To avoid this dangerous condition, develop a portage (waypoint 9-28a) which allows boaters to exit the river, use the proposed sky bridge over 3900 South, and re-access the water at the launch at waypoint 9-31.

Construction Costs

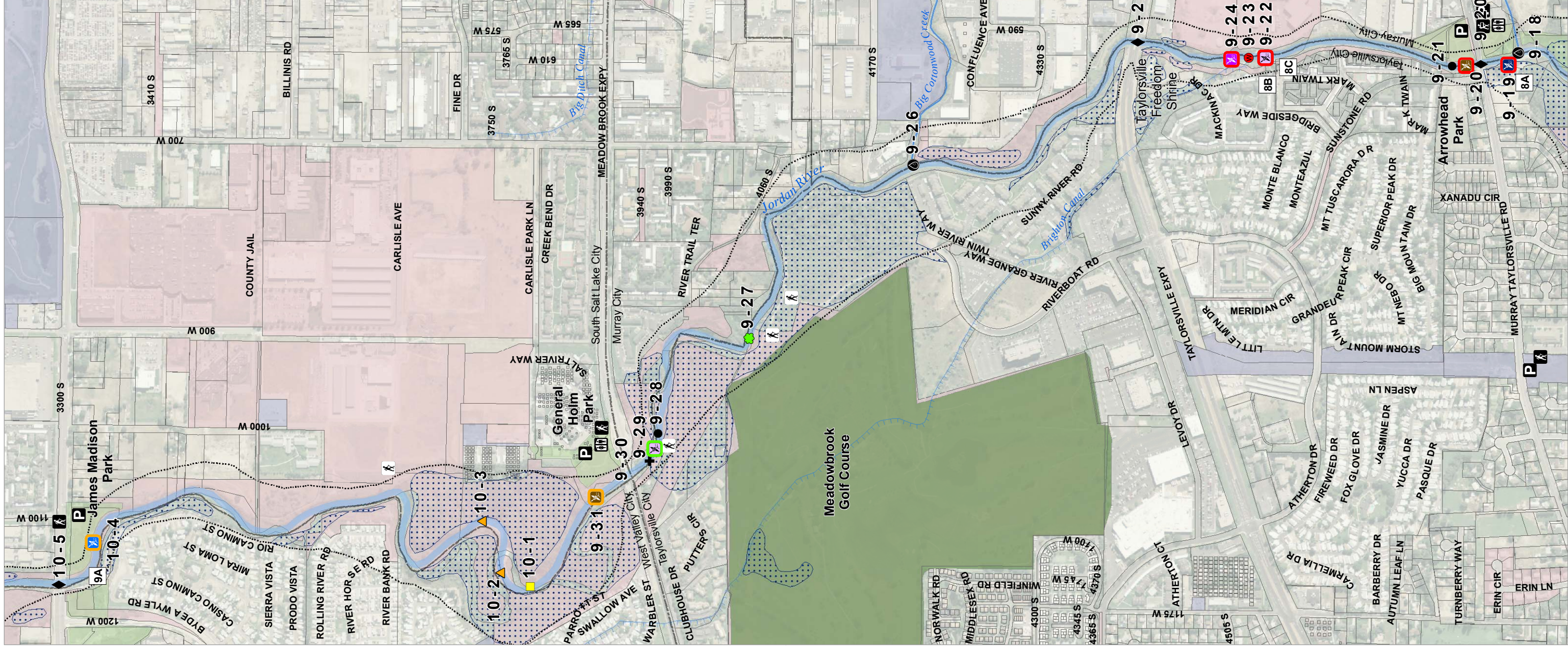
Feature Category	Map Code	CIP Budget Implication
Launch Removal	4.8 A	\$5,000
Safety Device	4.8 B	\$5,000
Portage	4.8 C	\$10,000

Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
- Launch - HV Location, MV Function
- Launch - HV Location, LV Function
- Launch - MV Location, MV Function
- Launch - MV Location, LV Function
- Launch - VLV Location, LV Function
- Portage - HV Location, LV Function
- Portage - HV Location, HV Function
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- Tree - High Hazard
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HV = High Value
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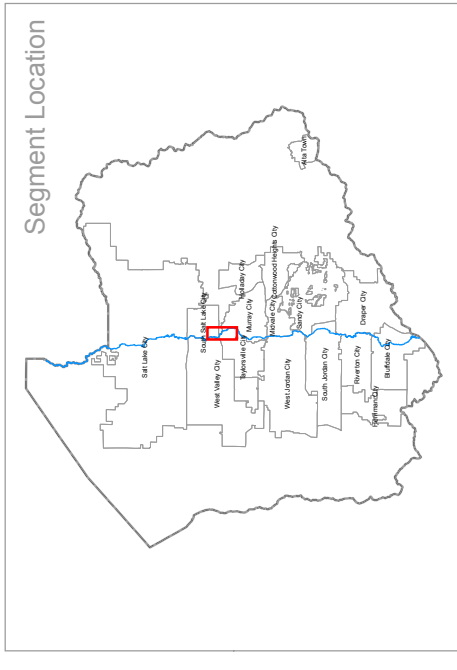


Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

Waypoint locations have been modified for readability.

Segment 8: Murray/Taylorsville/ West Valley/S. Salt Lake

Murray Taylorsville Road to 3300 South



4.9 Segment 9: West Valley/South Salt Lake/Salt Lake City (3300 South to 1700 South) (GSL segments 10 and 11)

The water trail continues almost a mile north to an existing launch at waypoint 10-04, near 3300 South on the right side. This launch has low functionality and a medium value location. The distance from the launch to the parking lot is somewhat long, and the existing platforms are difficult to use, especially at high water flows. At waypoint 10-05 (zero hazard), the 3300 South vehicular underpass, flood debris has built up at the bridge abutment.

Recreational boaters travel north, winding past the Oxbow Jail and Oxbow Trailhead, and under waypoint 10-06 (zero hazard), a footbridge. The river flows north past the Redwood Nature Area, where Mill Creek joins the Jordan River from the east at waypoint 10-07 (zero hazard), as does the Decker Lake outflow tributary at waypoint 10-08 (zero hazard) from the west. Downstream near Redwood Trailhead Park and Paul Workman Ball Park, there is an existing boat launch at waypoint 10-09 with three launches for three different levels of water. This launch has high functionality and a high value location, although the distance from parking is somewhat long, and the platforms can be difficult to access depending on the water level. The river then passes under waypoint 10-10 (zero hazard), the SR-201 vehicular bridge.

As users travel north to waypoint 10-11 (low hazard), a lot of woody debris has built up at the front of 2100 South vehicular bridge pillars. There is a sign at waypoint 10-11a, warning users to exit before they approach the Surplus Canal weir and diversion dam. This warning sign, near 2100 South, has medium functionality and a medium value location. North of the 2100 South vehicular bridge at waypoint 10-12 (zero hazard), a pipeline crosses the river at the south end of the Glendale Golf Course. Boaters must portage around waypoint 10-13 (medium hazard), the diversion dam for the Surplus Canal. There is a portage at waypoint 10-14 on the right side of the Jordan River, which is gravel with no path. This portage has low functionality and a high value location. Waypoint 10-15 (high hazard), next to the diversion dam, is the Surplus Canal weir which spans the entire width of river. The portage for users to put-in after the dam and weir is located at waypoint 11-01 on the left side of the river, which is a semi-steep gravel trail. This site has low functionality and a high value location. A more appropriate site for water users wishing to start a trip in this area is at 1700 South.

The river trail then continues north past waypoint 11-02 (low hazard), a tree that is covering 50% of river on the right side, and under waypoint 11-03 (zero hazard), a footbridge. Boaters then pass waypoint 11-04 (medium hazard), where overhanging trees narrow the access along both sides of the river banks and pass under waypoint 11-05 (low hazard) where an overhead pipe with protective screening that could be hazardous in high water crosses the river. The river

flows through overhanging trees with a lot of snags at waypoint 11-06 (high hazard), which extends for about 30 yards on both sides of the river's banks. As the river trail approaches the low railroad bridge at waypoint 11-07 (high hazard), they will encounter flood debris that has collected at base, and must then pass through the low 4 feet clearance under the bridge.



Figure 4.9.1 Boat launch at 3300 South (WP 10-04)



Figure 4.9.2 Vehicular bridge at 3300 South, built up debris (WP 10-05)



Figure 4.9.3 Boat launch at 2350 South (WP 10-09)



Figure 4.9.4 Bridge at SR-201 (WP 10-10)



Figure 4.9.5 Woody debris in front of pillars (WP 10-11)



Figure 4.9.6 Warning sign at 2100 South (WP 10-11A)



Figure 4.9.7 Pipeline across river near 2100 South (WP 10-12)



Figure 4.9.8 Diversion dam at 2100 South downstream (WP 10-13)



Figure 4.9.9 Diversion dam for surplus canal at 2100 South (WP 10-13)



Figure 4.9.10 Surplus dam, gravel with no path (WP 10-14)



Figure 4.9.11 Weir at surplus canal at 2100 South (WP 10-16)



Figure 4.9.12 Portage, potential launch site, near 2100 South (WP 11-01)



Figure 4.9.13 Tree covering half of the river (WP 11-02)



Figure 4.9.14 Trees hanging over river, narrow access on both sides (WP 11-04)



Figure 4.9.15 Trees overhanging the river, 30 yards on both sides (WP 11-06)



Figure 4.9.16 Debris collected at base of railroad bridge (WP 11-07)

Construction Costs

Table 4.9.1– Construction Costs: Water Trail Segment 9		
Feature Category	Map Code	CIP Budget Implication
Launch Upgrade	4.9 A	\$10,000
Launch Upgrade	4.9 B	\$10,000
Portage	4.9 C	\$10,000
Signage	4.9 D	\$500

Issues and Opportunities

- The existing launch at waypoint 10-04 has a decent location along the river trail, but not as significant as others. The functionality is low.
- The existing portage at waypoint 10-14 is important to allow boaters to avoid the Surplus Canal weir and dam, but has low functionality.
- There is a potential launch site for users north of the dam and weir at waypoint 11-01.
- The railroad bridge at waypoint 11-07 is too low.

Recommendations

4.9 A Launch at 3300 South (Waypoint 10-04)

Redesign and reconstruct this launch.

4.9 B Launch (Waypoint 10-09)

Redesign and reconstruction this launch to provide better accessibility and functionality at different water levels.

4.9 C Portage (Waypoint 10-14)

Redesign and reconstruction this portage at waypoint 10-14 and launch at 11-01 to avoid the diversion dam for the Surplus Canal.

4.9 D Portage (Waypoint 11-07)

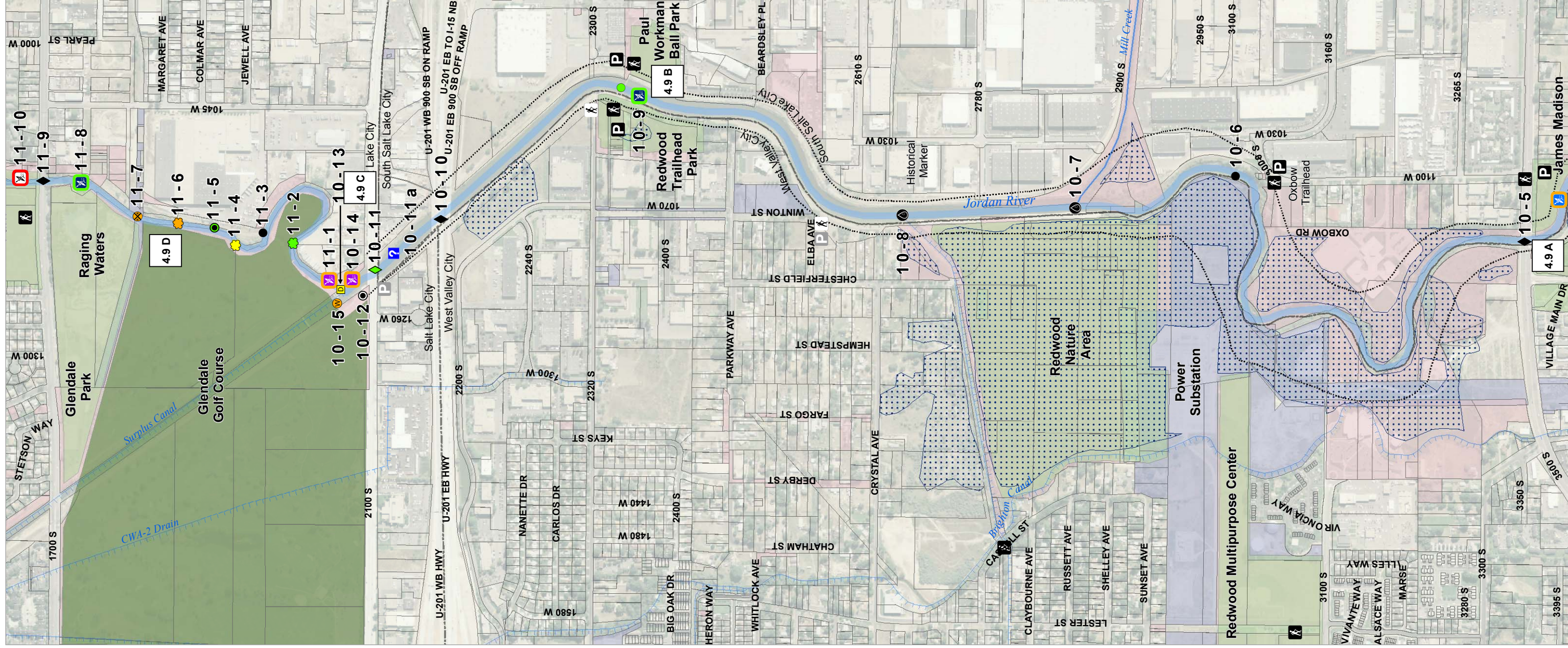
A low railroad bridge at waypoint 11-07 presents a hazard. Provide a sign indicating the up-coming hazard suggesting that boaters portage at 10-14, follow the paved trail north to the launch at Raging Waters (waypoint 11-8) to continue on, or end their trip at this point.

Jordan River Trail Master Plan

Urban Water Trail Recommendations

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- Launch - HV Location, MV Function
- Launch - HV Location, LV Function
- Launch - MV Location, VLV Function
- Launch - MV Location, MV Function
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- Potential Launch - HV Location, LV Function
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- Weir - High Hazard
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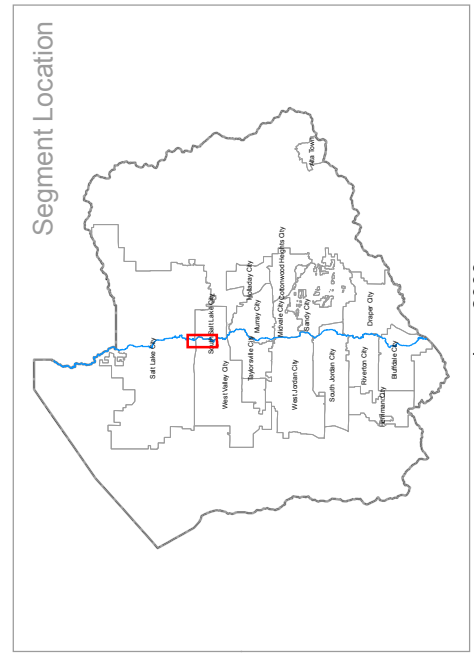
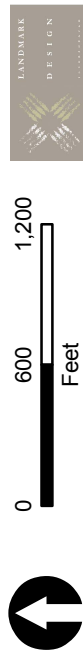
Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

Waypoint locations have been modified for readability.

*No data available north of Surplus Canal.

Segment 9: West Valley/South Salt Lake/Salt Lake City

3300 South to 1700 South



4.10 Segment 10: Salt Lake City (1700 South to Interstate 80) (GSL segments 11 and 12)

The river continues north to waypoint 11-08 with a launch at Raging Waters just south of 1700 South. This launch has high functionality and a high value location, and is the preferred design for a water launch, especially for larger groups of people and those with special needs. Watercraft users then proceed north under the 1700 South vehicular bridge, waypoint 11-09 (zero hazard). Immediately north of 1700 South, there used to be a launch on the left side of the river at - waypoint 11-10 in Glendale Park, which has been removed since the 2000 study. At this location, there is also a storm water lift station, which releases flow into the river after storm events.

As boaters continue north through Glendale park, they encounter overhanging trees on the left side of the river at waypoint 11-11 (low hazard), and pass under a footbridge at the Peace Trees site, waypoint 11-12 (zero hazard). Waypoint 11-12a, created during this planning process, is a small rapid created by a pipeline just north of the footbridge at waypoint 11-12. Waypoint 11-12b The river flows under a footbridge at waypoint 11-13 (zero hazard) just south of 1300 South, then under the 1300 South vehicular bridge with an adjacent pipeline, waypoint 11-14 (medium hazard). The river passes a small neighborhood park and flows to the east, passing a tributary and storm drain at waypoint 11-15 (zero hazard), then bends back to the north, where a fallen tree at waypoint 11-16 has been removed since the GSL study. Waypoint 11-16a, created as part of this planning process, is a new launch that was installed after the data gathering. This launch, also called the Modesto launch, is a floating dock system and functions well.

The trail continues under a footbridge on 1100 South at waypoint 11-17 (zero hazard), and past some buildup of flood debris at the base of the Fremont Avenue bridge at waypoint 11-18 (low hazard). As the Jordan River flows north through Jordan Park, it passes under waypoint 11-19 (zero hazard), a footbridge crossing the river, and skirts around waypoint 11-20, a large wooden dock that sticks out into the water. The dock is used for viewing and fishing from the east bank

of the river and has zero functionality and zero value location for the urban water trail. River trail users then bend around the north side of Jordan Park, passing waypoint 11-21 (low hazard) where a tree has fallen along right side of river.

As boaters approach the 900 South railroad bridge, they encounter a lot of debris at the foot of the bridge, which covers half of the channel under the bridge, waypoint 11-22 (medium hazard). Waypoint 11-22a (low hazard), created as part of this planning process, is a footbridge that was installed after the 2000 study. North of the Ninth South Park, there is a boat launch at waypoint 11-23 with low functionality and a medium value location. The parking lot is too far away from the launch point in this location. The trail then flows under waypoint 11-24 (zero hazard), the vehicular bridge at Indiana Avenue (800 South), and past a tributary entry point at waypoint 11-25 (zero hazard). Boaters then pass under waypoint 11-26 (zero hazard), a good example of a high bridge, and under several more bridges at waypoints 11-27 (zero hazard), the 700 South vehicular bridge; waypoint 11-28 (low hazard), the 600 South footbridge; waypoint 11-29 (medium hazard), the 500 South bridge which has low clearance; waypoint 11-30 (low hazard), the 400 South vehicular bridge; and waypoint 11-31 (zero hazard), the 300 South vehicular bridge. Under waypoint 11-31, there is a sign which is hard to notice. This sign, waypoint 11-32 has low functionality and a low value location. It is worn out and is in a spot that is difficult for boaters to see.

The river then continues to waypoint 11-33, near 300 South at the Alzheimer's Association Wildlife Grove, which was a portage sign hidden in trees on the right side that has been removed since the GSL study. This sign has high value functionality and very low value location. The takeouts are on the right side of the river at waypoint 12-01 and have about 10 feet of gravel between the path and the concrete launch ramps. The ramps need repair and have high functionality and a high value location. Across the river from the takouts is waypoint 12-02, a tree that has been removed since the 2000 study.



Figure 4.10.1 Launch at Raging Waters (WP 11-08)



Figure 4.10.2 Vehicular bridge at 1700 South (WP 11-09)



Figure 4.10.3 Launch left side of river at 1700 South (WP 11-10)



Figure 4.10.4 Overhanging trees on left side of river (WP 11-11)



Figure 4.10.5 Storm drain, tributary at 1200 South (WP 11-15)



Figure 4.10.6 Bridge at California Avenue (WP 11-16)



Figure 4.10.7 Tree fallen into river at 1200 South (WP 11-16)



Figure 4.10.8 Wood dock near Jordan Park (WP 11-20)



Figure 4.10.9 Tree in river near Matter Horn at Jordan Park (WP 11-21)



Figure 4.10.10 Railroad bridge at 900 South, debris covers half of channel (WP 11-22)



Figure 4.10.11 Boat launch at 850 South (WP 11-23)

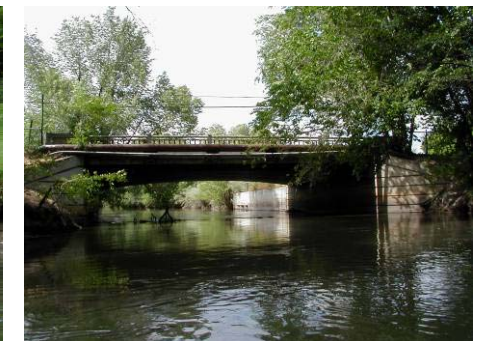


Figure 4.10.12 Vehicular bridge at Indiana Avenue (WP 11-24)



Figure 4.10.13 Tributary at 800 South (WP 11-25)



Figure 4.10.14 High footbridge (WP 11-26)



Figure 3.10.21 Tree across river at 300 South (WP 12-02)



Figure 4.10.15 Vehicular bridge at 700 South (WP 11-27)



Figure 4.10.16 Low bridge at 500 South (WP 11-29)



Figure 3.10.17 Vehicular bridge at 400 South (WP 11-30)



Figure 3.10.18 Vehicular bridge at 300 South (WP 11-31)



Figure 3.10.19 Warning sign on bridge, hard to notice (WP 11-32)



Figure 3.10.20 Portage sign hidden by trees at 300 South (WP 11-33)

Issues and Opportunities

- The water launch at waypoint 11-10 in Glendale Park is redundant due to the launch at Raging Waters.
- The footbridge and pipe at waypoint 11-14 present a hazard to water trail users.
- North of the Ninth South Park at waypoint 11-23, there is an existing boat launch with a decent location, though other launches are more important to the water trail corridor. The functionality of this launch is poor.
- The 600 South footbridge at waypoint 11-29 has low clearance.
- The signs at waypoints 11-32 and 11-33 are in poor locations, and are not adequate with respect to the hazard.

Recommendations

4.10 A Raging Waters Launch (Waypoint 11-08)

This launch is considered highly functional and in a good location; it is the best example of a launch in the river corridor and should be considered an example. It is suitable for larger groups and for persons with special needs.

4.10 B Pipeline (Waypoint 11-09)

The pipeline in the river creates rapids. Sign the to warn boaters that depending on water levels, water characteristics may change. This condition continues to waypoint 11-10 where a storm water lift station creates changing water conditions depending on water levels.

4.10 C Pipeline (Waypoint 11-12)

The pipeline in the river creates rapids. Sign to warn boaters that depending on water levels, water characteristics may change.

4.10 D Parley's Creek, Red Butte Creek, Emigration Creek Drainage (Waypoint 11-15)

This confluence creates some changes in water characteristics depending on flows.

Potential interpretive opportunity as well.

4.10 E 900 South Rail Road Bridge (Waypoint 11-22)

When the rail road tracks on 900 South are abandoned in 2008, this

bridge should be removed and all elements of the bridge in the river should also be removed.

4.10 F Removed Launch (Waypoint 11-23)

This launch has been removed and had limited value to the water trail corridor except as an interim rest stop/comfort station. Design and construct a small ramp that provides access to facilities.

4.10 G Footbridge at 600 South (Waypoint 11-29)

Replace and redesign this footbridge, as it has a low clearance.

4.10 H Warning Signs Prior to the Canoe Chute (Waypoint 11-31)

The last opportunity to exit the river prior to the Canoe Chute, which is not suited to novice boaters, needs to be signed more often and in more visible locations. Existing signs need to be redone, relocated, and separated to provide plenty of warning to boaters. Warning signs should be of a consistent design, clearly identifying them as warning.

4.10 I Boat Takeout at 300 South (Waypoint 12-01)

Repair and upgrade this boat takeout, which is highly desired and appropriately located.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Standards	4.10 A	N/A
Signage	4.10 B	\$500
Signage	4.10 C	\$500
Signage	4.10 D	\$500
Hazard Removal	4.10 E	N/A
Access	4.10 F	\$5,000
Bridge	4.10 G	\$100,000
Signage	4.10 H	\$1,500
Launch Upgrade	4.10 I	\$10,000

Jordan River Trail Master Plan

Urban Water Trail Recommendations

- Launch - HV Location, HV Function
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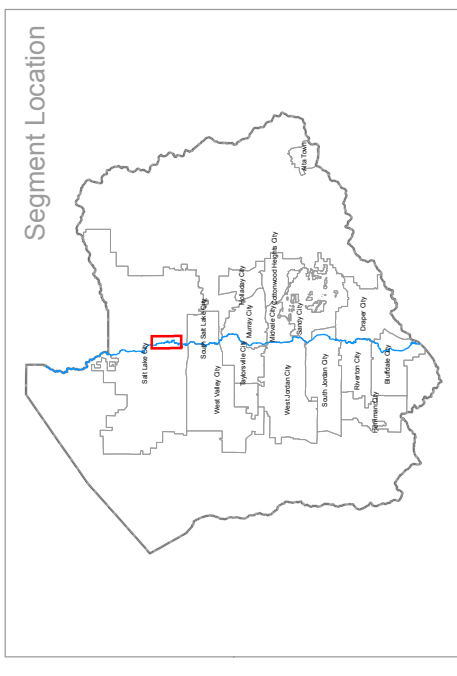
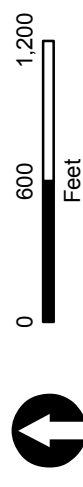
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Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

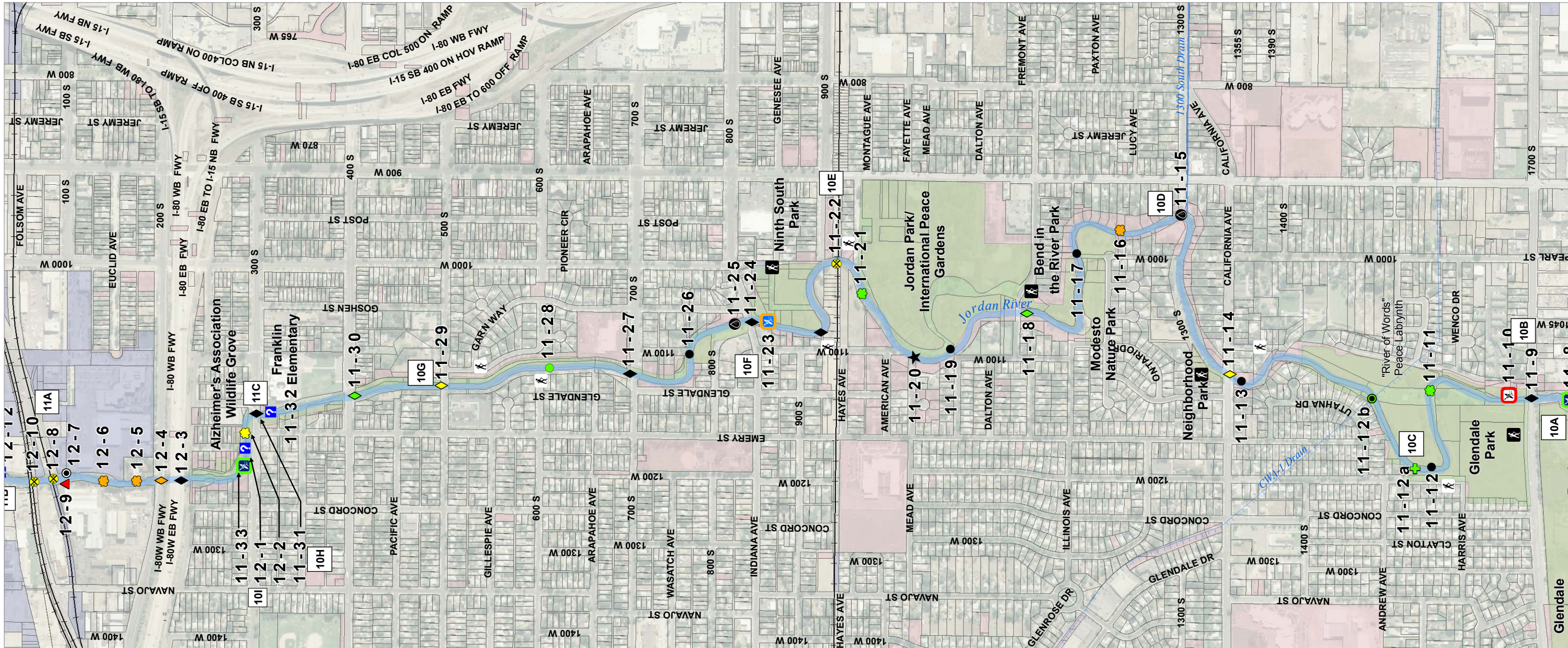
Waypoint locations have been modified for readability.
*No data available north of Surplus Canal.

Segment 10: Salt Lake City

1700 South to Interstate 80



June 2008



4.11 Segment 11: Salt Lake City (Interstate 80 to DuPont Avenue) (GSL segments 12 and 13)

Boaters then travel downstream, under the Interstate 80 vehicular bridge, waypoint 12-03 (zero hazard), then under the 200 South vehicular bridge which has very low clearance and has major flood debris collected underneath, waypoint 12-04 (high hazard). North of 200 South, there is a low limb across the stream at waypoint 12-05 (high hazard), and three low limbs in a row at approximately 150 South, waypoint 12-06 (high hazard). Some limbs have been removed and the hazard may be lower.

A high pipeline crosses the river at waypoint 12-07 (zero hazard) before passing under a concrete railroad bridge at waypoint 12-08 (medium hazard), which is a concrete railroad bridge. Flood debris has collected against the railroad bridge, creating a one-foot drop. There are two more railroad bridges at waypoint 12-10 (medium hazard) where debris has collected in the middle of the channel.

Traveling north, the trail approaches waypoint 12-11 (high hazard), a Rocky Mountain Power weir that with a canoe chute near South Temple. Waypoint 12-12 is a warning sign for the weir and chute, and has high functionality and a high value location. Downstream, water trail users pass waypoint 12-13 (low hazard), old trestle piers, and waypoint 12-14 (low hazard), where flood debris has collected downstream on both sides of the river. Waypoint 12-15 (medium hazard) is a pair of railroad bridges. The second bridge is lower, and a pipe crosses on the downstream side of the second bridge. As the river continues towards North Temple, a fallen tree has collected debris at waypoint 12-16 (low hazard) and an overhead pipeline and cable cross the river at waypoint 12-17 (zero hazard). The river flows under the North Temple vehicular bridge, waypoint 12-18 (zero hazard), where debris has collected. Another pipeline at waypoint 12-19 (zero hazard) crosses the Jordan River just downstream of North Temple, where City Creek joins the river through a concrete culvert, waypoint 12-20 (low hazard). An additional twelve inch pipe crosses overhead just north of North Temple at waypoint 12-21 (zero hazard).



Figure 4.11.1 Interstate 80 Bridge (WP 12-03)



Figure 4.11.2 Low bridge at 200 South, debris (WP 12-04)



Figure 4.11.3 Low limb across river at 200 South (WP 12-05)



Figure 4.11.4 Low limbs at 150 South, Hobo camp along river (WP 12-06)

Water users continue north to waypoint 12-22 (medium hazard) where a tree is laying across half of the river channel near the east bank. The river then flows under a footbridge at the Utah State Fair Park, waypoint 12-23 (zero hazard) to waypoint 12-24, a potential launch site on the west bank. This site has low functionality and a high value location. The river trail continues downstream under another footbridge at waypoint 12-25 (zero hazard), and passes an old boat launch at waypoint 12-26, near the south end of Constitution Park. This old concrete platform site has low functionality and a low value location.

The river flows under another footbridge at waypoint 12-27 (zero hazard) near Cottonwood Park, past a downed tree at waypoint 12-28 (high hazard) which blocks the entire river channel near the Utah State Department of Agriculture Building. At waypoint 12-29 in Cottonwood Park, there used to be a boat launch, but it has been removed. This launch has low functionality and a low value location. The water trail continues under the footbridge at the Agriculture Building, waypoint 12-30 (zero hazard), the 500 north vehicular bridge at waypoint 12-31 (zero hazard), and a footbridge north of 500 North at waypoint 12-32 (zero hazard).

As the river bends to the east, trees from each side meet in the middle of the river, waypoint 12-33 (high hazard). Water users then pass the Backman Elementary School on the east bank of the river, heading north again, and travel under the 700 North vehicular bridge, waypoint 12-34 (medium hazard), which has low clearance. Boaters pass under waypoint 12-35 (zero hazard), a footbridge at 800 North at the north end of Riverside Park. Continuing north, the water trail passes east of the Day Riverside Library, where a boat launch is located at waypoint 12-36 near 918 North. This launch has low functionality and a low value location - the ramp is dilapidated. Just a short distance downstream there used to be a boat launch at 1000 North, waypoint 12-37. This site has a high value location and facilities need to be reinstalled at an appropriate location to accommodate the boating needs in this section of the river.

Boaters then pass under waypoint 12-38 (low hazard), the 1000 North

vehicular bridge, and past waypoint 13-01 (low hazard), a tree that is overhanging half of river on the right side near the Jordan River Golf Course. Waypoint 13-02 was a boat launch near the Jordan River Golf Course has been removed in conjunction with a river restoration project.



Figure 4.11.5 High pipeline (WP 12-07)



Figure 4.11.6 Concrete railroad bridge (WP 12-08)



Figure 4.11.7 Flood debris at railroad bridge (WP 12-09)



Figure 4.11.8 Two railroad bridges with debris (WP 12-10)



Figure 4.11.9 Rocky Mtn. Power weir with canoe chute at South Temple (WP 12-11)



Figure 4.11.10 Warning sign for boaters (WP 12-12)



Figure 4.11.11 Old trestle piers (WP 12-13)



Figure 4.11.12 Railroad bridge near South Temple, debris (WP 12-14)



Figure 4.11.19 Bridge at 500 North (WP 12-31)



Figure 4.11.20 Trees over river from both sides at 600 North (WP 12-33)



Figure 4.11.13 Fallen tree upstream from North Temple, debris (WP 12-16)



Figure 4.11.14 Overhead pipeline and cable at North Temple (WP 12-17)



Figure 4.11.21 Bridge at 700 North, low clearance (WP 12-34)



Figure 4.11.22 Boat launch at 918 North (WP 12-36)



Figure 4.11.15 Potential launch site at Utah State Fair Park (WP 12-24)



Figure 4.11.16 Old concrete platform boat launch at 100 North (WP 12-26)



Figure 4.11.23 Boat launch at 1000 North (WP 12-37)



Figure 4.11.24 Vehicular bridge at 1000 North (WP 12-38)

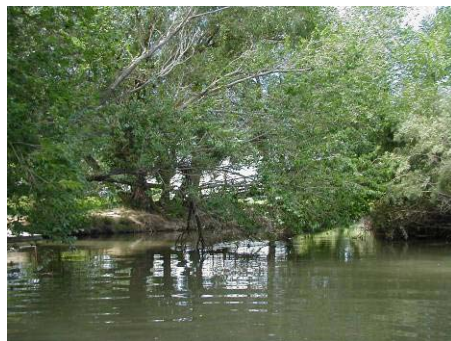


Figure 4.11.17 Tree across entire river at Agriculture Building (WP 12-28)



Figure 4.11.18 Concrete boat launch at Agriculture Building (WP 12-29)



Figure 4.11.25 Tree overhanging half the river (WP 13-01)



Figure 4.11.26 Boat launch near Jordan River Golf Course (WP 13-02)

Issues and Opportunities

- The 200 South vehicular bridge is too low and has collected major flood debris underneath.
- The canoe chute at waypoint 12-11 can be dangerous for inexperienced boaters. Waypoint 12-15 is the site of two railroad bridges. The second bridge is too low, and a pipe crossing the river on the downside poses a hazard.
- A potential site for a launch is located at waypoint 12-24, near the Utah State Fair Park. This site is important to the water trail corridor.
- The old concrete launch at waypoint 12-27 near Cottonwood Park is not in a good location and is not functional. It should be removed from the water corridor.
- The existing boat launch at waypoint 12-36 is in a poor location and has poor functionality. It should be abandoned.
- The existing boat launch at waypoint 12-27 is in a good location and has decent functionality, though it needs to be improved.
- An existing boat launch at waypoint 13-02 is in a poor location and has poor functionality. It should be abandoned.

Recommendations

4.11 A South Temple Corridor Railroad Consolidation (Waypoint 12-08 and 12-10)

These waypoints are part of a rail consolidation plan for this South Temple/City Creek corridor. The rail will be consolidated to one track; as the bridge is reconstructed, it should be designed with more clearance and in ways that do not catch debris and cause blockages in the river.

4.11 B Canoe Chute/Rocky Mountain Power Weir (Waypoint 12-11)

This is a high hazard for novice and beginner boaters and should be avoided. Only experienced boaters should attempt this section. See 4.10 H, page 4-28 for additional information.

4.11 C 200 South Bridge (Waypoint 11-31)

This bridge has a low clearance. During high water events, boaters should either terminate their trip at the Alzheimer's Association Wildlife Grove (waypoint 12-1) or portage around the bridge.

4.11 D Rail Road Bridges (Waypoint 12-15)

The second of two bridges is low and a pipeline in the river creates rapids. Sign to warn boaters that depending on water levels, water characteristics may change.

4.11 E City Creek Pipe (Waypoint 12-20)

Where City Creek empties into the Jordan River, cross-currents affect the river's flow. Sign to warn boaters that depending on water levels, water characteristics may change.

4.11 F Trailhead: North Temple (Waypoint 12-24)

Plans for a trailhead on the west side of the river on the south side of North Temple should be abandoned. There is an existing parking lot

on the north side of North Temple, west of the Jordan River that could serve a shared- use as a paved and urban water trailhead as well as parking for the Utah State Fair Park. This would require negotiation with the Utah Division of Facilities Construction and Management.

4.11 G Boat Ramps at Cottonwood Park (Waypoint 12-19) and 918 North (Waypoint 12-36)

The ramp at Cottonwood Park has been removed, and the one at waypoint 12-26 is dilapidated, non-functional and should be removed, leaving a deficit in launch facilities between North Temple and 1000 North.

4.11 H Boat Launch at 1000 North (Waypoint 12-37)

The existing boat launch needs to be upgraded and improved, but is needed. This should be a combined boat launch/trail head. The boat launch at waypoint 13-02 has been removed; launching activities are now concentrated at this better location at 1000 North.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Bridge	4.11 A	\$100,000
Trailhead	4.11 B/F	\$140,000
Signage	4.11 C	\$500
Signage	4.11 D	\$500
Signage	4.11 E	\$500
Launch Removal	4.11 G	\$5,000
Launch Removal	4.11 H	\$115,000

Jordan River Trail Master Plan

Urban Water Trail Recommendations

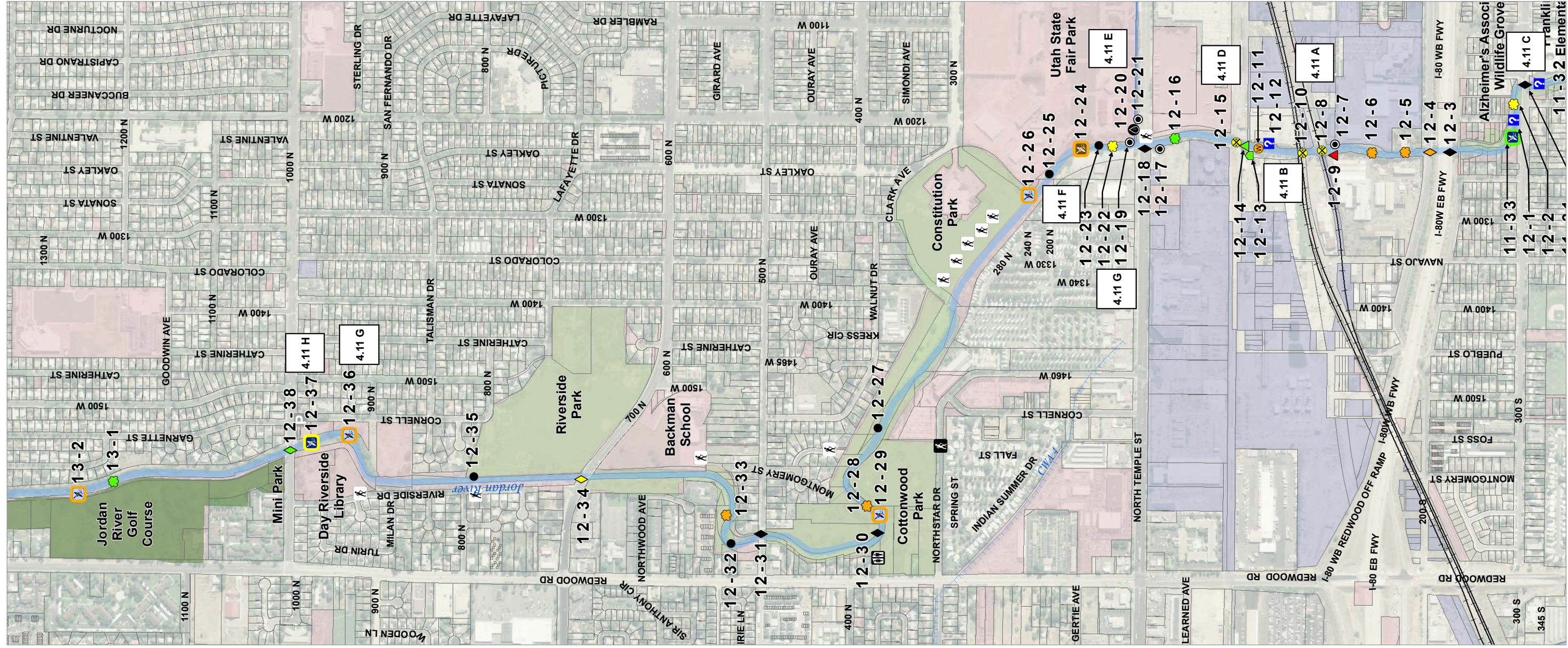
	Equestrian Trailhead
	Formal Trailhead
	Informal Trailhead
	Trail Access Point
	Parking - Formal Paved
	Parking - Informal Unpaved
	Restrooms
	Highways
	City Boundary
	Jordan River
	Streams
	Canals
	Light Rail
	Railroad
	County Boundary
	Meander Corridor
	Wetlands (WAIDS 2007)
	Golf Course
	Park
	Publicly Owned Land
	Utility Owned Land

	Launch - HV Location, HV Function
	Launch - MV Location, MV Function
	Launch - LV Location, LV Function
	Launch - HV Location, LV Function
	Launch - MV Location, LV Function
	Launch - LV Location, LV Function
	Launch - VLV Location, LV Function
	Portage - HV Location, LV Function
	Portage - MV Location, LV Function
	Portage - LV Location, LV Function
	Portage - High Hazard
	Potential Launch - HV Location, MV Function
	Potential Launch - MV Location, LV Function
	Potential Launch - LV Location, LV Function
	Potential Launch - VLV Location, LV Function
	Bridge - High Hazard
	Bridge - Medium Hazard
	Bridge - Low Hazard
	Bridge - No Hazard
	Footbridge - Very High Hazard
	Footbridge - Medium Hazard
	Footbridge - Low Hazard
	Footbridge - No Hazard
	Railroad Bridge - High Hazard
	Railroad Bridge - Medium Hazard
	Railroad Bridge - No Hazard
	Dam - Very High Hazard
	Dam - High Hazard
	Dam - Medium Hazard
	Dam - Low Hazard
	Weir - Very High Hazard
	Weir - High Hazard
	Flood Debris - Very High Hazard
	Flood Debris - High Hazard
	Flood Debris - Medium Hazard
	Flood Debris - Low Hazard
	Pipe - Very High Hazard
	Pipe - High Hazard
	Pipe - Medium Hazard
	Pipe - Low Hazard
	Pipe - No Hazard
	Cultural Element - Low Hazard
	Cultural Element - No Hazard
	Miscellaneous Debris - High Hazard
	Miscellaneous Debris - Medium Hazard
	Miscellaneous Debris - Low Hazard
	Miscellaneous Structure - Very High Hazard
	Miscellaneous Structure - High Hazard
	Miscellaneous Structure - Medium Hazard
	Miscellaneous Structure - Low Hazard
	Miscellaneous Structure - No Hazard
	Miscellaneous Structure - No Hazard
	Rapid - High Hazard
	Rapid - Medium Hazard
	Rapid - Low Hazard
	Rapid - No Hazard
	Tributary - No Hazard
	Tree - Very High Hazard
	Tree - High Hazard
	Tree - Medium Hazard
	Tree - Low Hazard
	Sign

HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value

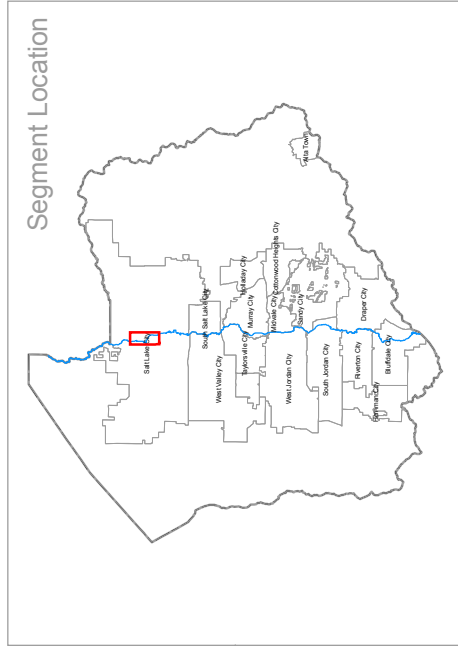
Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

Waypoint locations have been modified for readability.
*No data available north of Surplus Canal.



Segment 11: Salt Lake City

Interstate 80 to DuPont Avenue



4.12 Segment 12: Salt Lake City/Salt Lake County (DuPont Avenue to Interstate 215) (GSL segments 13 and 14)

The water trail continues to flow north through Rose Park Golf Course under the bridge at waypoint 13-03 (low hazard). Several trees are down and blocking the right half of the river at waypoints 13-04 (low hazard) and 13-05 (low hazard). North of the Golf Course is a boat launch at waypoint 13-06. This launch is paved with asphalt and has low functionality and a high value location. Pipelines cross above the river at waypoints 13-07 (zero hazard) and 13-08 (zero hazard). Located immediately north of waypoint 13-06, waypoint 13-09 is an abandoned launch near Redwood Road. This site has very low functionality and a high value location.

The river then flows under waypoints 13-10 (zero hazard), the Redwood Road vehicular bridge; waypoint 14-01, a pipeline 10 feet past Redwood Road; and waypoint 14-02 (medium hazard), an old, unsafe footbridge approximately 50 feet west of Redwood Road. Boaters continue north past several hazards at waypoints 14-03 (medium hazard), a fallen tree that is covering half of the river on the right side; 14-04 (medium hazard), fallen trees that cover half of the river channel; 14-05 (low hazard), a leaning snag that covers 60% of the river channel on the left side; and 14-06 (low hazard), where debris has collected on a snag.

The water trail continues north, under waypoint 14-07 (medium hazard), a pipeline with wood around it that looks like a bridge with cables hanging down and past hazards at waypoints 14-08 (low hazard), a tree with debris along the left side of river; an abandoned irrigation dam with significant debris that blocks the entire river channel just upstream of Interstate 215; 14-10 (medium hazard), a large log sticking up with lots of debris; and 14-11 (low hazard), several logs that have piled up with debris along the right side of the river. The debris at waypoint 14-09 has been removed. The Urban Water Trail then continues under waypoint 14-12 (medium hazard), the Interstate 215 vehicular bridge, and becomes a shared water trail between Salt Lake and Davis Counties. The Urban Water Trail ends at Center Street in Davis County.



Figure 4.12.3 Tree down right half of river, Rose Park Golf Course (WP 13-05)



Figure 4.12.4 Boat launch near Redwood Road (WP 13-06)



Figure 4.12.11 Fallen tree covering 60% of channel (WP 14-05)



Figure 4.12.12 Debris collecting on fallen tree (WP 14-06)



Figure 4.12.1 Bridge at Rose Park Golf Course (WP 13-03)



Figure 4.12.5 Pipeline across river (WP 13-07)



Figure 4.12.6 Abandoned boat launch near Redwood Road (WP 13-09)



Figure 4.12.13 Pipeline covered with wood, cables hang down (WP 14-07)



Figure 4.12.14 Tree with debris, right side of river (WP 14-08)



Figure 4.12.2 Tree down right half of river, Rose Park Golf Course (WP 13-04)



Figure 4.12.7 Vehicular bridge at Redwood Road (WP 13-10)



Figure 4.12.8 Footbridge, pipeline near Redwood Road (WP 14-02)



Figure 4.12.15 Historic irrigation dam, debris blocks entire channel (WP 14-09)



Figure 4.12.16 Log sticking out of river, debris (WP 14-10)



Figure 4.12.9 Fallen trees covering half the river (WP 14-03)



Figure 4.12.10 Fallen trees covering half the channel (WP 14-04)



Figure 4.12.17 Trees piled up on right side of river (WP 14-11)



Figure 4.12.18 Vehicular bridge at Interstate 215 North (WP 14-12)

Issues and Opportunities

- North of the Rose Park Golf Course at waypoint 13-06, there is an existing boat launch. This location is important for the water trail corridor, and should be improved to safe functionality.
- The existing boat launch at waypoint 13-09 that has been abandoned. This site is an important location and should be improved.
- The old bridge at waypoint 14-02, just west of Redwood Road is unsafe, and needs to be replaced with a new bridge that meets safety standards.
- The pipe crossing the river at waypoint 14-07 is too low.
- The bridge at Interstate 215 is too low.

Recommendations

4.12 A Boat Launch (Waypoint 13-06)

Redesign and reconstruct a boat launch in this location near 1800 North at the crossing of Redwood Road. The facility should include boater access, parking, restrooms, and emergency river access.

4.12 B Bridge (Waypoint 14-02)

Replace this old and deteriorated bridge

4.12 C Pipe (Waypoint 14-07)

Provide signing to warn boaters of the low clearance.

4.12 D Davis County/Legacy Parkway Trail Connection (Waypoint 14-12)

At the Salt Lake County boundary, a trail is anticipated on the east side of the river in Davis County to connect to trails along Legacy Parkway. Once the river passes under I-215, environmental concerns suggest that boaters be required to register and receive permits for access.

Construction Costs

Feature Category	Map Code	CIP Budget Implication
Launch Upgrade	4.12 A	\$10,000
Bridge	4.12 B	\$100,000
Signage	4.12 C	\$500
Connection	4.12 D	

Jordan River Trail Master Plan

Urban Water Trail Recommendations

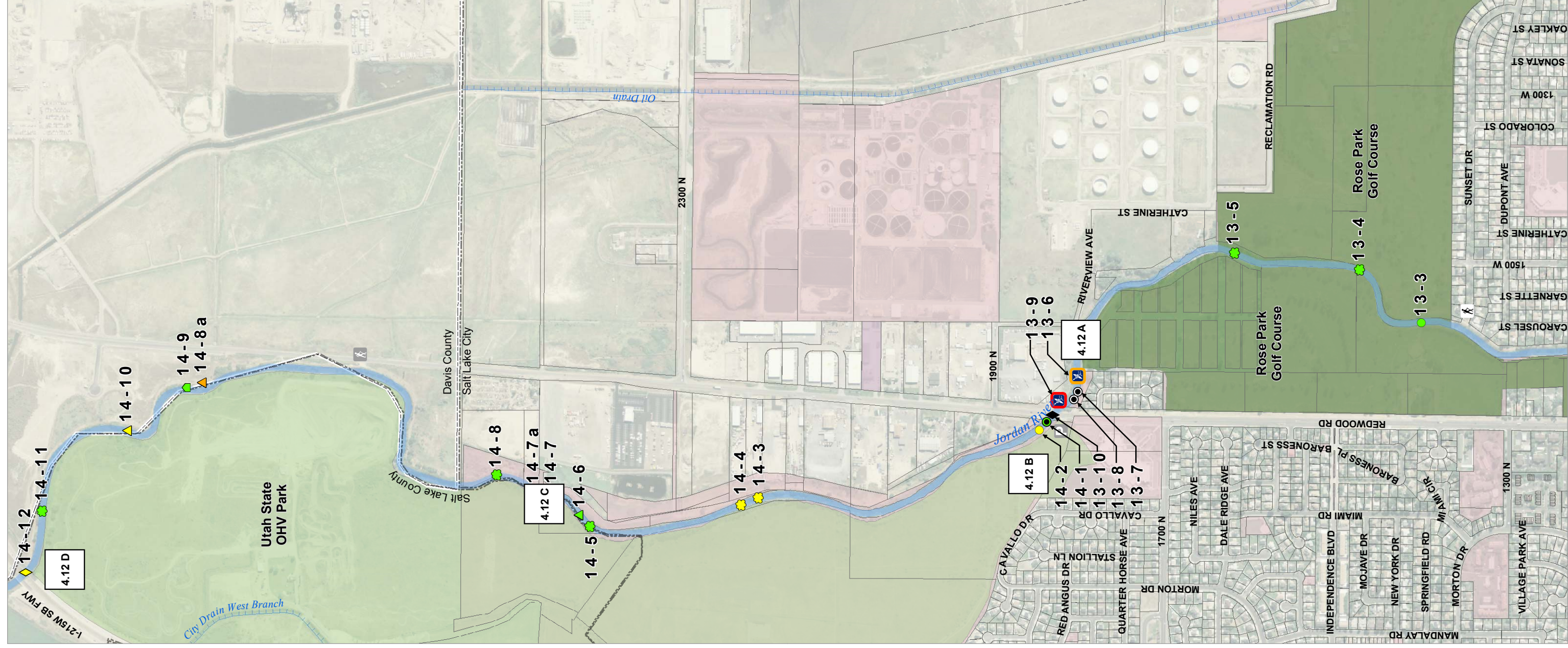
	Launch - HV Location, HV Function		Equestrian Trailhead
	Launch - MV Location, MV Function		Formal Trailhead
	Launch - LV Location, LV Function		Informal Trailhead
	Launch - VLV Location, VLV Function		Trail Access Point
	Portage - HV Location, HV Function		Parking - Formal Paved
	Portage - MV Location, MV Function		Parking - Informal Unpaved
	Portage - LV Location, LV Function		Restrooms
	Portage - VLV Location, VLV Function		Highways
	Potential Launch - HV Location, HV Function		City Boundary
	Potential Launch - MV Location, MV Function		Jordan River
	Potential Launch - LV Location, LV Function		Streams
	Bridge - High Hazard		Canals
	Bridge - Medium Hazard		Light Rail
	Bridge - Low Hazard		Railroad
	Bridge - No Hazard		County Boundary
	Footbridge - Very High Hazard		Meander Corridor
	Footbridge - Medium Hazard		Wetlands (WAIDS 2007)
	Footbridge - Low Hazard		Golf Course
	Footbridge - No Hazard		Park
	Railroad Bridge - High Hazard		Publicly Owned Land
	Railroad Bridge - Medium Hazard		Utility Owned Land
	Railroad Bridge - No Hazard		
	Dam - Very High Hazard		
	Dam - High Hazard		
	Dam - Medium Hazard		
	Dam - Low Hazard		
	Weir - Very High Hazard		
	Weir - High Hazard		
	Flood Debris - Very High Hazard		
	Flood Debris - High Hazard		
	Flood Debris - Medium Hazard		
	Flood Debris - Low Hazard		
	Pipe - Very High Hazard		
	Pipe - High Hazard		
	Pipe - Medium Hazard		
	Pipe - Low Hazard		
	Pipe - No Hazard		
	Cultural Element - Low Hazard		
	Cultural Element - No Hazard		
	Miscellaneous Debris - High Hazard		
	Miscellaneous Debris - Medium Hazard		
	Miscellaneous Debris - Low Hazard		
	Miscellaneous Structure - Very High Hazard		
	Miscellaneous Structure - High Hazard		
	Miscellaneous Structure - Medium Hazard		
	Miscellaneous Structure - Low Hazard		
	Miscellaneous Structure - No Hazard		
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	Tree - Very High Hazard		
	Tree - High Hazard		
	Tree - Medium Hazard		
	Tree - Low Hazard		
	Sign		

HV = High Value
MV = Medium Value
LV = Low Value
VLV = Very Low Value

Data on water trail courtesy of The Great Salt Lakekeeper and National Park Service from Jordan River Navigational Hazards and Recreational Boating Study 2001.

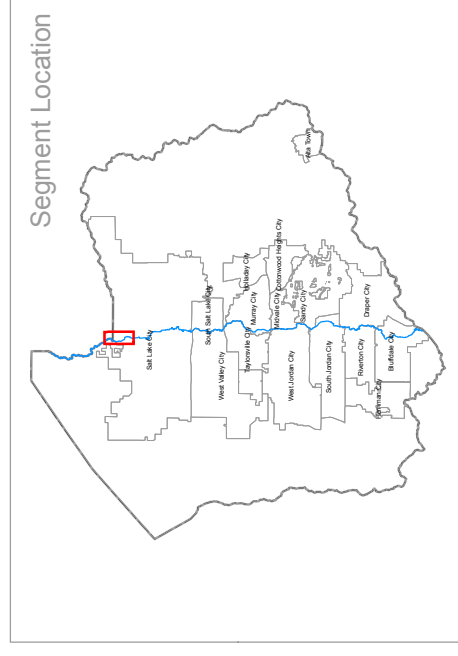
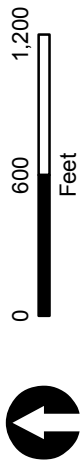
Waypoint locations have been modified for readability.

*No data available north of Surplus Canal.



Segment 12: Salt Lake City/ Salt Lake County

DuPont Avenue to Interstate 15



5.0 Jordan River Trail Development Standards

5.1 Purpose of Trail Development Standards

The development standards that follow are intended to direct the detailed design and construction of recreation facilities along the Jordan River Trail as gaps in the trail are completed and facilities are added or upgraded. Development standards help create a unique identity for the Jordan River Trail corridor while maintaining consistency with other Salt Lake County trail facilities. Standards allow users to become familiar with characteristics of the trail, including their location within the greater context of the trail corridor and the surrounding urban setting, what types of facilities to anticipate, and where to locate additional information.

5.2 General Trail Facility Development Design and Development Principles

- Standards are intentionally simple and consistent.
- Salt Lake County design and construction standards serve as the model for Jordan River Trail facilities. Where not specified or where additional clarification is required, the [Salt Lake County Regional Trails Plan \(1994\)](#) should be referenced for additional ideas and guidance.
- Trail facilities should be designed and developed to provide easy access for pedestrians, cyclists, equestrians, and water trail users, according to the designated use of each trail segment.
- Trailheads and paved multi-purpose trail facilities should be ADA compliant where possible, and designated water trail segments and their associated facilities should enable water trail use by groups such as SPLORE that work with clients having special needs.
- All official Jordan River Trail signs should be highly visible, easy to maintain, and consistent throughout the Jordan River Trail corridor. Unnecessary signage should be avoided and removed.

5.3 Jordan River Trail Standards

The Jordan River Trail corridor is composed of three use designations: (1) paved multi-purpose trails, (2) soft-surface equestrian trails (also allows pedestrians and cyclists), and (3) water trails. The trail classifications dictate the surface materials, design, and standards for associated facilities. For example, equestrian trails will never be hard paved surfaces, and water trails will be navigable waterways in association with facilities required for access.

Paved Multi-Purpose Trails

Paved multi-purpose trails serve a wide variety of users, from cyclists and pedestrians, to in-line skaters and people with strollers or dogs. The surface material on these trails must therefore be hard, smooth, and durable.

Paved multi-purpose trails need to be a minimum of 10' wide, with 2-3' of shoulder and 5-6' of clear zone on each side of the trail. The typical

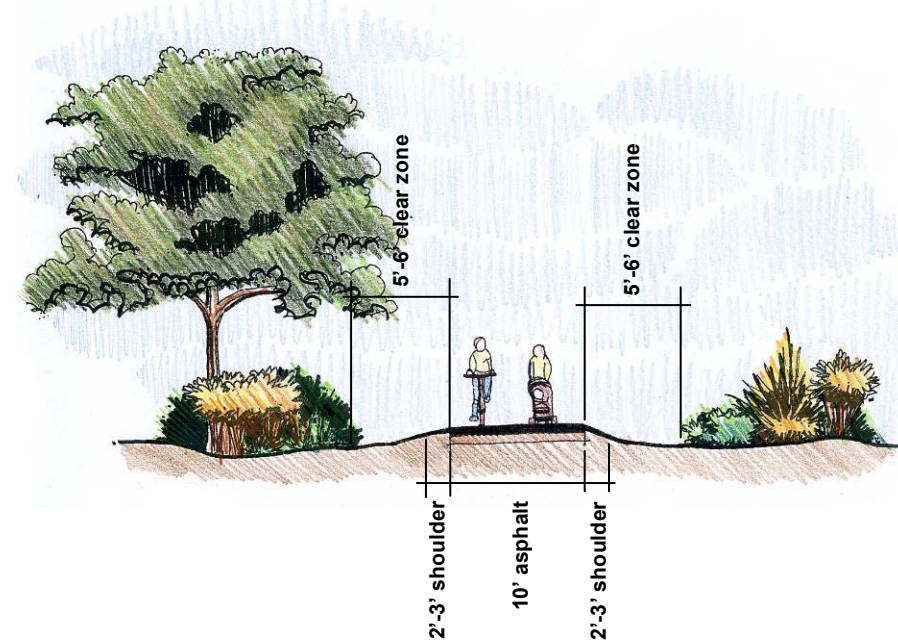


Figure 5.3.1 Paved Surface Trail

paving material is asphalt, but concrete or recycled plastic lumber boardwalks may be used where need or context dictates. Figure 5.3.1 above illustrates the typical Paved Surface Trail.

Equestrian Trails

Equestrian trails are intended to accommodate equestrians and their horses, but also serve as an alternative path for pedestrians and cyclists on wilderness terrain bikes. Equestrian trails are always unpaved, soft surface trails, and therefore restrict the use of in-line skaters and cyclists on road bikes by design.

Equestrian trails may be a maximum of 10' wide, though they are generally 3-5' wide as conditions allow, with 2-3' of cleared shoulder on each side of the trail. The surface material may be finely shredded bark mulch, gravel, or crushed stone. Figure 5.3.2 illustrates a typical Equestrian Trail section.

Special attention needs to be paid to underpasses and bridges that are intended to accommodate horses—clearance under these structures needs to be 12' minimum to allow a rider on horseback to pass through the area without dismounting. It is also desirable to have mounting blocks on both ends of bridges to allow riders to dismount in case they do not feel safe riding under the bridge on their horses. It is recommended that cities and the county maintain as large a separation as possible between the paved multi-purpose trail facilities to prevent conflicts with other trail users and dogs.

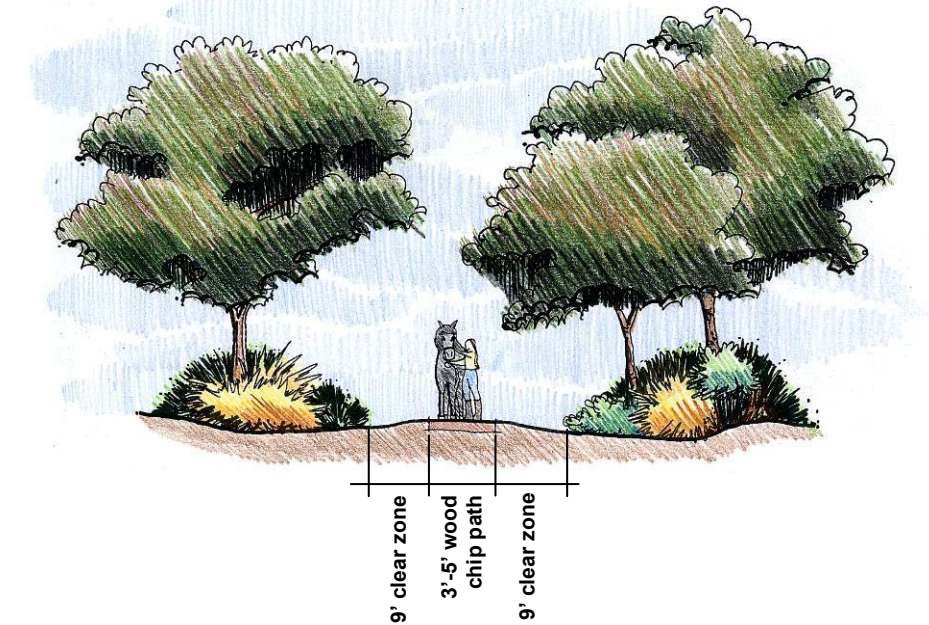


Figure 5.3.2 Equestrian Trail

Water Access Facilities

The National Park Service Rivers, Trails, and Conservation Assistance Program has developed a comprehensive design guide for canoe launches titled *Logical Lasting Launches: Design Guidance for Canoe and Kayak Launches* (see bibliography), which provides overall design considerations and requirements for developing water trail facilities as well as specific designs for a variety of needs and conditions.

As explained in this guide, “A logical, lasting launch provides safe and easy access for paddlers while accommodating the topographic, climatic, and ecological characteristics of its location. Ideally, its construction is cost-efficient and durable and has little impact on the environment and riparian ecology.”

Additionally, “guidelines for newly designed, constructed, and altered recreation facilities issued by the ADA in 2002 require that all public boat launches, *which include fixed and floating structures of all sizes*, comply with ADA Accessibility Guidelines (ADDAAG) standards.”

Water Trail Put-Ins and Take-Outs

The chart below describes the ideal design for creating an accessible put-in or take-out, as mentioned in *Logical Lasting Launches*. These facilities should be located as close as possible to parking facilities to minimize the distance that users must carry their boats to and from water access points. Water trailhead facilities are described below in the “Trailheads” section.

Put-ins and take-outs should be big enough to facilitate the large groups that are common to and recommended for river trips, and also

JORDAN RIVER TRAIL MASTER PLAN

to provide the space necessary to accommodate water trail users with special needs. However, the size and design will be limited by site constraints at individual locations.

Table 5.3.1 and Figure 5.3.3 explain the preferred designs for put-ins and take-outs.

Table 5.3.1 General Recommendations for Designing an Accessible Launch

Height Above Water	Between 9" and 2' from highest expected water level
Width	At least 5' wide, preferably 6' to 12'
Length	At least 25' to allow paddlers "dry" access to entire length of their boats
Slope	ADA Accessibility Guidelines require that slopes not exceed 8.33% wherever possible; A slope exceeding 15% will make transition from land to water difficult for any paddler
Support	Handrails or other support structures, including step-down designs or ropes, help paddlers balance their weight during put-in and take-out
Location	Ideally in areas without heavy flow, erosion, exposure to elements, heavy boat traffic, or fragile riparian habitats

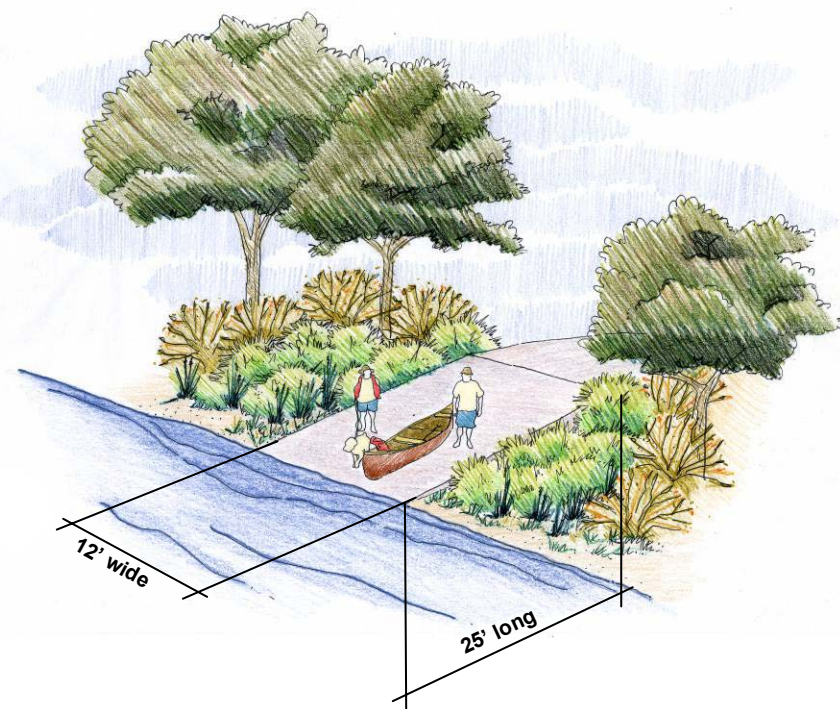


Figure 5.3.3 Preferred Dimensions for Launch/Take-Out Facility

The existing put-in and take-out at 1700 South, waypoint 11-08, near Raging Waters, is the best example of an accessible water trail access point along the Jordan River, and is frequently used by SPLORE and other organizations to launch larger group river trips.

Water Trail Portages

A portage is a combination of a put-in and a take-out facility that allows water trail users to safely avoid dams or other hazards in a waterway by exiting the water upstream of the hazard, and re-launching downstream. As mentioned in *Logical Lasting Launches*, "An effective portage should include:

- Clear, well-marked signage allowing paddlers sufficient time to reach shore before take-out
- A path at least 2' wide around the [hazard], with a slope no steeper than a 1:3
- An ADA accessible portage cannot have a slope that exceeds 8.33% or 1:12
- At least 8' overhead clearance on the path and 4' to 8' clearance on either side
- A vertical distance of 12" or less between the height of boat and shore
- A route that minimizes the distance that paddlers must carry their boats
- Access points located on inside bends or areas of calm water
- An escape path downstream from portage, when possible, giving paddlers an additional area to egress the water if they miss a take-out or portage."

Proper signage is critical for making portages effective because boaters need sufficient warning and time to navigate safely to the portage take-out. Water trail signs are discussed below under the "Signage" section. Additional safety measures such as the barrels at waypoint 2-08 may help warn boaters of major water trail hazards.

Trailheads

Trailheads serve as access points, staging areas, and information stations for trail users. Requirements vary depending on uses for individual trail segments and can be affected by available land and funding.

Given the hot summer climate, it is recommended that parking be adequately shaded. The implementation of a well-designed shading strategy provides a level of order and structure, transforming parking lot surfaces from undifferentiated asphalt expanses into clearly articulated parking areas



Figure 5.3.4 Multi-Purpose Oxbow Trailhead

that are welcome transitions from sun-exposed physical activity along the trail corridor.

Connections between the trail and trailheads should be clear and obvious. Trailhead signs should be well-sited to clearly identify the trailhead from the parking lot and provide a visual clue of where the trail starts.



Figure 5.3.6 Trail Access Point

Trail use during all seasons has been increasing, and in order to accommodate year-round use, this plan recommends that restroom facilities remain open at key trailheads throughout the year.

Trail Entry Barriers

Motorized vehicles of any kind are not allowed on the trail, with the exception of authorized maintenance vehicles. There have been reports and incidents of unauthorized vehicular access in several areas along the trail. Gates and special barriers may be required at certain trailheads to prevent unauthorized motorized vehicles from entering the trail.



Figure 5.3.5 Trail Entry Barriers

Multi-Purpose Trailhead

Trailheads that serve paved multi-purpose trail segments should provide adequate parking for cars and trucks; restroom facilities; drinking fountains; site furnishings such as benches, picnic tables, trash receptacles, bike racks, and lighting; and trailhead informational

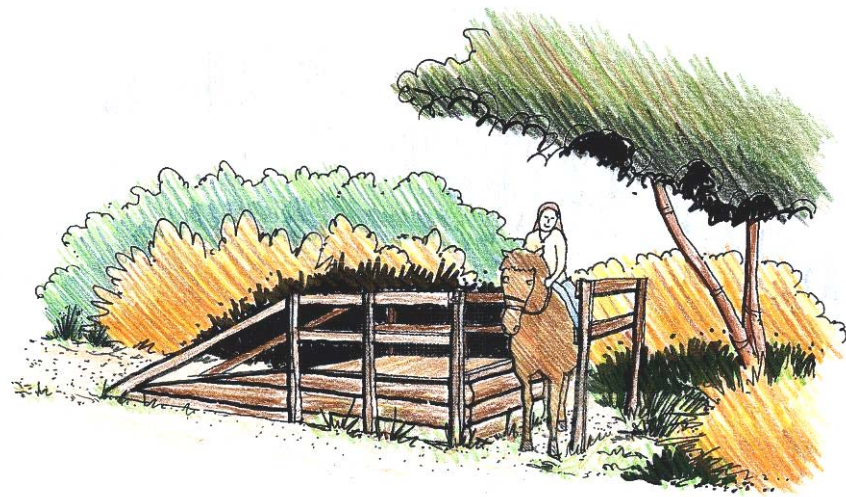


Figure 5.3.6 Equestrian Mounting Platform

signs. Parks located along the trail corridor are often able to accommodate most of these uses, and additional features such as playgrounds or additional recreational facilities while also serving as a trailhead for the Jordan River Trail corridor. Sometimes, however, space is limited, and only minimal trailhead facilities can be accommodated, which include limited parking, trash receptacles, and trailhead informational signs.

Equestrian Trailhead

Equestrian trailheads have similar requirements as the multi-purpose trailheads discussed above, with the additional needs for pull through parking to accommodate horse trailers and the trucks that are pulling them. Other desired facilities are loading/unloading platforms and ramps, tie-up areas, access to fresh water for horses, and small corrals as site constraints allow. Signage should clearly indicate trailheads that accommodate equestrian use.

Water Trailhead

Water trailheads have similar requirements as the multi-purpose trailheads discussed above, with the additional need for parking in close-proximity to well-designed put-ins and take-outs. Parking lots at water trailheads should be big enough to facilitate the large groups that are common to and recommended for river trips, but will be limited by site constraints at individual locations.

Trail Access Points

Trail access points provide small-scale local access to the Jordan River Trail corridor and are typically located within residential neighborhoods, though some are found in more commercial or industrial locations. Facilities are basic and include access via a path, sidewalk, or ramp. Sometimes a trail entry barrier is necessary to prevent unauthorized motorized vehicles from accessing the trail. Trail access points should be signed to raise awareness of the Jordan River Trail corridor.

Lighting

Improperly designed and unnecessary lighting can contribute to light pollution, create undesired impacts to wildlife, and detract from the quality of more natural areas along the river corridor. Therefore, lighting in the trail corridor should be limited to very urban areas and developed parks and recreation facilities where user safety requires lighting. Lighting should minimize light pollution to surrounding areas and be well-designed in coordination with associated site furnishings. Lighting may be more formal, composed of high quality materials, and possibly custom designed to associate the features with a particular city, park, or event. Underpasses need to be lit to a safe level.



Figure 5.3.7 Lighting

Bridges

Bridges limit the natural tendency of the Jordan River to travel within the natural meander corridor, detracts from the quality of water trails users, may create a hazard for water trail users during high water levels, and can add significant cost to trail development projects. Therefore it is recommended that the use of bridges be limited to situations where it is absolutely necessary, and when new bridges are added, they shall be designed to minimize risk and impacts to water trail use. Bridges need to maintain at least 6' clearance at high water level for water trail users to pass safely underneath.



Figure 5.3.8 Bridges Should Provide at Least 6' Clearance During High Water Levels

Vegetation

Appropriate vegetation should be re-established along new trails in accordance with recommendations in the *Jordan River Natural Conservation Corridor Report* and the on-going planning efforts mentioned in Chapter 1 of this plan, improving the quality of wildlife habitat and the experience for trail users.

Park and Recreation Sites

Park and recreation facilities located along the Jordan River Trail will be developed according to the needs and desires of the associated municipalities and organizations. However, the design of these

facilities should adhere to the trailhead and signage standards in this plan where these facilities interface with the Jordan River Trail corridor, serving as either a trailhead, access point, or destination.

Site Furnishings

The Jordan River Trail corridor travels through twelve cities and unincorporated areas of Salt Lake County, traversing a broad spectrum of terrain, from very dense urban developments to more natural, open riparian habitats. Site furnishings located along the trail should fit with the immediate surroundings. In urban locations or more formal parks, site furnishings such as benches, lighting, and trash receptacles may be more formal, composed of high quality materials such as metal, and possibly custom designed to associate the features



Figure 5.3.9 Recommended Park Bench Style



Figure 5.3.10 Recommended Park Trash Receptacle Style



Figure 5.3.11 Recommended Natural Area Bench Style



Figure 5.3.12 Recommended Natural Area Trash Receptacles Style



Figure 5.3.13 Recommended Park Bike Rack Style



Figure 5.3.14 Unique Site Paving



Figure 5.3.15 Artwork in Plaza Along Trail



Figure 5.3.16 Artwork in Park Along Trail

with a particular city, park, or event.

In more natural, open areas furnishings should be more “naturalistic” and simple, consisting of “rustic appearing” materials such as wood or recycled plastic lumber. The frequency and quantity of furnishings in more natural areas should be minimized to preserve the look and feel associated with these areas.

Some areas such as parks or plazas along the trail may feature unique



Figure 5.3.17 Gazebo Style Overlook



Figure 5.3.18 Deck Style Overlook

features such as art or special paving.

Rest Areas and Viewing Sites

Rest areas should be situated in shaded locations, or in areas that offer special beauty or unique viewing opportunities, and should be composed of materials such as wood or recycled plastic lumber that enhance and blend visually with the surroundings.

5.4 Signage Program

This plan recommends the development of specific design standards for signage along the Jordan River Trail. A unified Jordan River Trail

signage program will help increase public awareness about the river, access opportunities, and the recreational possibilities. A coordinated, standard system of signs will also provide safety and use guideline information as well as highlight the natural and cultural elements of the river corridor. The information that follows is primarily an inventory of existing signage types, along with recommendations for future design standard features.

As described in the *Bonneville Shoreline Trail Alignment Plan for Salt Lake County*, all trail signs must adhere to basic principles to fulfill their intended function, which include: fulfilling a need; commanding attention; conveying a clear, simple meaning; commanding respect from trail users; and giving adequate time for proper response.

Sign Standards

The Federal Highway Administration (FHWA) has established color standards for traffic control signage, and these standards should be followed as a comprehensive signage program is implemented in the Jordan River Trail corridor.

- Yellow—General Warning
- Red—Stop or Prohibition
- Blue—Services Guidance or Tourist Information
- Green—Movements Permitted & Directional Guidance
- Brown—Recreation and Scenic Guidance
- Orange—Temporary Traffic Control
- Black—Regulation
- White—Regulation



Figure 5.4.1 Signalized Trail Crossing Directional Sign

The inclusion of the Jordan River Parkway logo in a manner consistent with other trail corridor signage will create a continuity to the entire river corridor, and reinforce the importance of these “official” messages for trail users. In addition, all signage should use consistent fonts for continuity.

Trailhead Direction Signs/Through Trail Signs

Trailhead direction signs should be located along major roads near the Jordan River to easily lead motorists and cyclists to trailheads. These signs will help increase public awareness of the Jordan River Parkway and trailhead locations. This plan recommends two types of directional signs. The first would be incorporated as part of signalized pedestrian crossings, as shown in figure 5.4.1, and should be consistent with the existing road sign style in the vicinity. The second type of sign, intended to be a roadside directional sign, should feature the Jordan River Parkway Trail logo in full color, with the text “Jordan River Parkway Trail,” and have an arrow indicating the direction to the trail access point.

The trail should also be signed at key junctions to notify trail users of the main trail’s direction so that trail users do not end up in neighborhoods or other ‘dead-ends’.

Trailhead Information Signs

Trailhead information signs should be located at formal Jordan River Trailheads to provide important trail information such as:

- Trailhead name;
- Trailhead location;
- A map indicating the trailhead location in relation to the overall

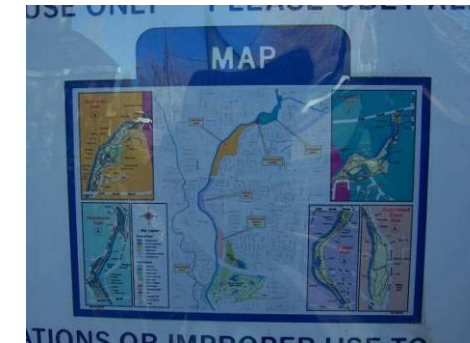


Figure 5.4.2 Good Example of Contextual Trail Map with Insets of Destination Details

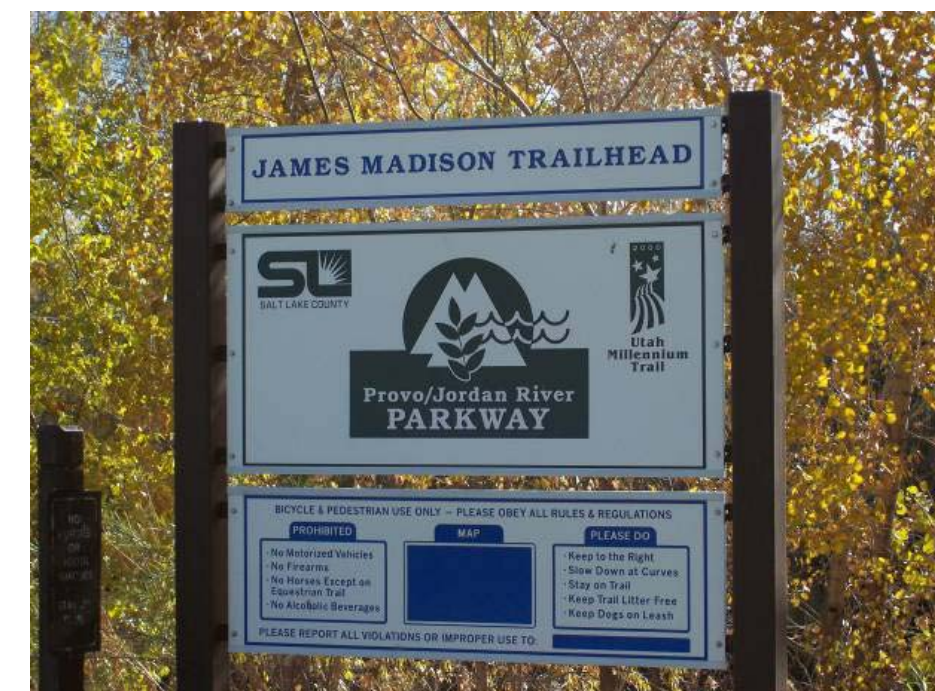


Figure 5.4.3 Typical Trailhead Sign

- trail system, trail junctions, mile marker sign locations, locations of parks and destinations, and unique natural areas;
- A description of permitted uses and hours;
- Trail etiquette;
- Other important information such as emergency phone numbers and contact information for comments or complaints; and
- A box for storing hand-held trail maps.

The Foundation for the Provo Jordan River Parkway, in association with Salt Lake County, recently developed and implemented a new design for trailhead signs, and this new sign standard should be incorporated as new trailhead signs are added or as older signs are

upgraded and improved.

Trail Junction Signs

Trail junction signs should be located where the Jordan River Trail corridor connects to east/west trails, major bikeways, and potential destinations. These signs should be brown with white text, as shown in figure 5.4.4, and should indicate direction and distance to potential destinations.



Figure 5.4.4 Jordan River Trail Junction with River Oaks Golf Course Cart Path

Trail Access Signs

Trail access signs should be located at all trail access points that do not serve as official trailheads with the standard trailhead signage. These trail access signs, as shown in figure 5.4.5, clearly identify the connection to the Jordan River Trail, and provide guidelines and use information for the corridor in general.



Figure 5.4.5 Recommended Trail Access Sign



Figure 5.4.6 Recommended Mile Marker Sign for Salt Lake County

Mile Marker Signs

Milepost marker signs should be contiguous through the corridor, indicating cumulative mileage in both directions from the County boundary. These signs should be located regularly at half-mile intervals. Figure 5.4.6 illustrates the recommended design style, which is similar to the mile markers used by Utah County. Due to the need for consistency and continuity, the mileage marker system should not be installed until all of the gaps are completed.

Location Signs

It is essential that both paved-multi-purpose trail and water trail users are aware of their location in the Jordan River Trail corridor for emergency purposes. A comprehensive system of location signs used in conjunction with mile marker signs not only allows trail users to inform emergency personnel of their location and provides emergency workers with more detailed access information, but also helps users become more familiar with their surroundings, and therefore more comfortable using the trail corridor.



Figure 5.4.7 Recommended Location Sign Design



Figure 5.4.8 Recommended Location/Information Sign Design for Water Trail

Location signs should inform users of their whereabouts as well as the location of adjacent destinations and facilities, such as providing names or where multi-purpose and water trails interface with roads, bridges, parks, or other facilities along the Jordan River Corridor.

This plan recommends the designs shown in figures 5.4.7 and 5.4.8. The first design, simple brown with white lettering, should be located at all major cross streets, parks, trail access points, and special features such as nature centers or points of interest. The second design conveys both location and facilities information at major points along the water trail. These signs should be located near take-outs and portages where appropriate.

Trail Closed Due to Flooding Signs

The level of water flow in the Jordan River varies throughout the year due to many factors, including releases of water from Utah Lake when it reaches “compromise level”, spring runoff from the surrounding watershed, diversions into canals throughout the corridor, and reduced water levels later in the year as the water supply diminishes. Several underpasses along the river are flooded during high water flows, and become impassable to paved multi-purpose and equestrian trail users. In addition, several vehicular and pedestrian bridges that have low clearances become impassable to water trail users during peak water flows.

These trail segments are closed to trail users during high flows and are designated as such with signs indicating the trail closures. These signs should be the same type used for road hazards that fold in half so the warning message is not displayed when the trails are not flooded. This will require dedicated personnel to ensure that these signs are accurately displayed during trail flooding. Signage



Figure 5.4.9 Recommended “Trail Closure” Signage

at trailheads indicating trail closures due to flooding are also recommended.

Water Trail Hazard Signs

There are several hazards located along the Jordan River that are impassable to water trail users, or may be undesirable for less-experienced boaters, requiring portages accompanied by proper warning signs. Chapter 4 discusses existing water trail hazards and provides specific recommendations. As mentioned above in the “Portage” section, proper signage is critical for making portages effective because boaters need sufficient warning and time to navigate safely to the portage take-out.



Figure 5.4.10 Recommended Water Trail Hazard Style

Logical Lasting Launches recommends two resources for detailed information on effective water trail signage: *Publishing Trail Guidance: Maps and Guides, Guidebooks and Signs* (North American Water Trails, 2002); and the Federal Energy Regulatory Commission’s (FERC) website: http://www.ferc.gov/industries/hydropower/safety/guidelines/signage/report/Safety_signage.htm. As stated on the FERC website, “while the specific approach of each agency or group to safety may vary, the basic elements of an effective safety signage program are the same:

- Conduct a thorough needs assessment;
- Develop an effective design;
- Standardize format and message;
- Properly place and locate signs;
- and perform regular maintenance.”

Signs should be highly visible and properly placed, have clear and concise messages indicating type and level of risk and, indicate proper response, and allow plenty of time for user to respond.

Signs for water trail facilities and hazards need to be individually assessed for placement and design to maximize visibility and safety for each element.

Water Trail Put-In and Take-Out Signs

Another component of developing a consistent and effective signage system for the Jordan River Corridor is proper signing of water trail put-ins and take-outs. These signs should be consistent with the other trail corridor signage, following the FERC and FHWA recommendations for safety. Signs indicating take-outs need to be highly visible to watercraft users, and allow plenty of time for boaters to respond accordingly. Put-in signs need to clearly indicate entry access points for people beginning a trip or continuing a trip through a

particular section. These signs may include information similar to that shown in figure 5.4.8 if it is deemed appropriate to the safety needs of water trail users.

Interpretive Signs

Interpretive signs should be distributed at key locations along the paved multi-purpose and water trails to provide educational information related to the trail corridor. Signs may include information about the history of the Jordan River, wildlife and habitat, restoration and protection efforts, unique ecological features, and local history and culture. Due to the unique nature of each interpretive opportunity, a specific design standard is not recommended here. Rather, this plan simply recommends that these signs be well-designed and should incorporate the Jordan River Parkway logo.

General Information Signs

Other information may need to be conveyed that does not fit into the above categories. For these remaining signs, it is recommended that these signs follow the recommendations of FHWA, with the color based on the type of information that needs to be conveyed. Figures 5.4.11 and 5.4.12 show general recreation use information, and are therefore brown. Icons and fonts should be consistent on all signs throughout the corridor, with international symbols that can be



Figure 5.4.11 General Trail Information Sign



Figure 5.4.12 General Trail Information Sign



Figure 5.4.13 Sign warning of Different User Group Interface



Figure 5.4.14 Typical Trailhead Sign

understood by a broad range of trail users. Figure 5.4.13 illustrates a general warning style recommended for intersections of different user groups. In cases of severe danger, bilingual text should be considered in addition to international symbols, as shown in figure 5.4.13.

5.5 Educational Nature Facilities

Educational Nature Facilities are recommended in the Jordan River Corridor in appropriate areas. It is recommended that these facilities have strong connections to Salt Lake County's school districts. Murray School District has an existing Educational Nature Facility along the Jordan River in Murray City, and it would be a valuable addition for the other school districts along the river as well.

5.6 Maintenance

Generally, maintenance of surface trails and the urban water trail are the responsibility of each jurisdiction along the corridor, although there are often opportunities to partner with others to perform the necessary maintenance functions. Maintenance is critical to the success of the trail corridor, and essential in keeping people and animals safe.

Weeds and Invasive Species

Vegetation within the corridor helps control erosion, reduces pollution and sedimentation, and provides habitat and food sources for wildlife. Weeds and invasive plant species need to be controlled to allow naturally occurring vegetation to out-compete and perform its valuable functions. Weeds, invasive vegetation, and even some naturally occurring vegetation also create areas where visibility is obscured, providing places for vandalism and crime to occur unobserved. Weeds with thorns, and spiky seed heads also create hazards, particularly for bicycle tires. Weed control along the corridor is the responsibility of each jurisdiction, except when the City has an inter-local agreement with Salt Lake County. Inter-local agreements typically hold the City responsible for maintaining parks and trailhead facilities along the parkway, while the County is responsible for maintaining the trail. Salt Lake County and state agencies are also assuming a role in the eradication of weeds and invasive species as they revegetate and improve river banks and riparian habitat.



Figure 5.6.1 Invasive Phragmites

- Jurisdictions should cooperate with county and state agencies in their efforts to restore riparian vegetation and eradicate weeds and invasive species.
- Provide 2 feet clear of vegetation adjacent to the paved trail.
- Sweep the trail of stones, gravel, "goat heads", and dirt at least weekly using equipment designed for such maintenance.
- Remove hazard-causing weeds within 5 feet of the trail using

appropriate and safe means.

- Require property owners adjacent to the corridor to mow vegetation that obscures visibility, and to control weeds and invasive species.

Trail Surfacing and Repairs

Over time, asphalt trail surfacing will require repair, and periodic storm and flood events may remove or badly damage large sections of trails. Minor trail repairs are the responsibility of the local jurisdiction; major trail repairs are the responsibility of Salt Lake County.

Crime and Vandalism

Crime and vandalism prevention does occur along the trail corridor.

- Incorporate CPTED (Crime Prevention Through Environmental Design) concepts into trail design, and using those principals, evaluate areas where crime and vandalism can be facilitated.
- Each community along the corridor should initiate a bicycle patrol that rides the corridor one or twice a day at random times. This patrol could include volunteers.

Trash and Debris Removal

Trash and debris is not only unsightly and distracts from the enjoyment of an important natural and recreational resource, it often creates conditions that are unsafe for surface trail users, water users, and even wildlife living-in or migrating through the corridor. Trailhead signing currently states that users should keep the trails "litter free", but more could be done to encourage users to clean up along trails.

Small items of trash and debris along the trail often include metal, glass, wire, items contaminated with human blood or bodily fluids, and other sharp and dangerous items. While each jurisdiction is responsible for removal of trash and debris, available resources for their removal are often stretched or virtually non-existent. In these cases where maintenance budgets are inadequate, opportunities to cooperate with local trails organizations or service groups are a viable and useful way to provide periodic and consistent clean-ups, and at the same time build relationships and promote positive community interaction.



Figure 5.6.2 Section of Trail Washed Out



Figure 5.6.3 Trail Vandalism

- Conduct twice yearly volunteer clean-ups along the entire corridor.
- Coordinate clean-ups among jurisdictions to help boost participation and get information disseminated more readily.
- Jurisdictions should provide adequate trash and recycling receptacles and dog waste stations at trailheads and at stopping points along the trail, and empty them regularly.
- Encourage trail users to pick up trash and debris, and discard it in the next available container or carry it home for disposal.

- water quality improvement projects.
- Read the labels and use herbicides, pesticides, fertilizers, and household chemicals appropriately.
- Use organic products or methods of treatment when within 50 feet of the river or its tributaries and drainages.
- Do not discard chemicals or chemical containers into the river, or its drainages and tributaries.
- Report incidents or suspicious behaviors that could have an adverse effect on water quality to local enforcement agencies.

Large items of debris such as concrete chunks and metal structures, abandoned bridge abutments and foundations, and other industrial and structural elements are a particular hazard to water trail users. These kinds of debris require large equipment and trained personnel to remove properly and safely, and may require coordinated efforts.



Figure 5.6.4 Trash Along Trail

- Remove dangerous and unsightly debris from the river using safe and appropriate means.
- Cooperate with adjacent land owners, municipalities, or other governmental and infrastructure agents and organizations to remove large debris.

Pollution

Several studies and efforts are addressing pollution in the Jordan River, some of which are summarized in Chapter 1. Currently, State and County resources are being applied to upgrade the river's water quality, or at a minimum assure that it does not deteriorate further. These organizations and efforts are addressing large industrial pollution sources, municipal sources, and broader-based watershed and water quality issues. Local jurisdictions should cooperate fully with State and County efforts.

Individual communities and landowners along the corridor and its tributaries and drainages can do their part by becoming educated and aware of urban and agricultural sources of pollutants including fertilizers, pesticides, herbicides, household chemicals, and other sources that are not currently regulated effectively. In spite of label warnings and conditions of use, too many people are still using herbicides and pesticides inappropriately in areas where they can easily run-off into the river or its tributaries. It is important that everyone be aware that individual actions, multiplied hundreds and thousands of times, ultimately result in cumulative effects that are damaging and often irreversible.

- Cooperate with local and state governmental agencies, and

5.0 Jordan River Trail Development Standards

5.1 Purpose of Trail Development Standards

The development standards that follow are intended to direct the detailed design and construction of recreation facilities along the Jordan River Trail as gaps in the trail are completed and facilities are added or upgraded. Development standards help create a unique identity for the Jordan River Trail corridor while maintaining consistency with other Salt Lake County trail facilities. Standards allow users to become familiar with characteristics of the trail, including their location within the greater context of the trail corridor and the surrounding urban setting, what types of facilities to anticipate, and where to locate additional information.

5.2 General Trail Facility Development Design and Development Principles

- Standards are intentionally simple and consistent.
- Salt Lake County design and construction standards serve as the model for Jordan River Trail facilities. Where not specified or where additional clarification is required, the [Salt Lake County Regional Trails Plan \(1994\)](#) should be referenced for additional ideas and guidance.
- Trail facilities should be designed and developed to provide easy access for pedestrians, cyclists, equestrians, and water trail users, according to the designated use of each trail segment.
- Trailheads and paved multi-purpose trail facilities should be ADA compliant where possible, and designated water trail segments and their associated facilities should enable water trail use by groups such as SPLORE that work with clients having special needs.
- All official Jordan River Trail signs should be highly visible, easy to maintain, and consistent throughout the Jordan River Trail corridor. Unnecessary signage should be avoided and removed.

5.3 Jordan River Trail Standards

The Jordan River Trail corridor is composed of three use designations: (1) paved multi-purpose trails, (2) soft-surface equestrian trails (also allows pedestrians and cyclists), and (3) water trails. The trail classifications dictate the surface materials, design, and standards for associated facilities. For example, equestrian trails will never be hard paved surfaces, and water trails will be navigable waterways in association with facilities required for access.

Paved Multi-Purpose Trails

Paved multi-purpose trails serve a wide variety of users, from cyclists and pedestrians, to in-line skaters and people with strollers or dogs. The surface material on these trails must therefore be hard, smooth, and durable.

Paved multi-purpose trails need to be a minimum of 10' wide, with 2-3' of shoulder and 5-6' of clear zone on each side of the trail. The typical

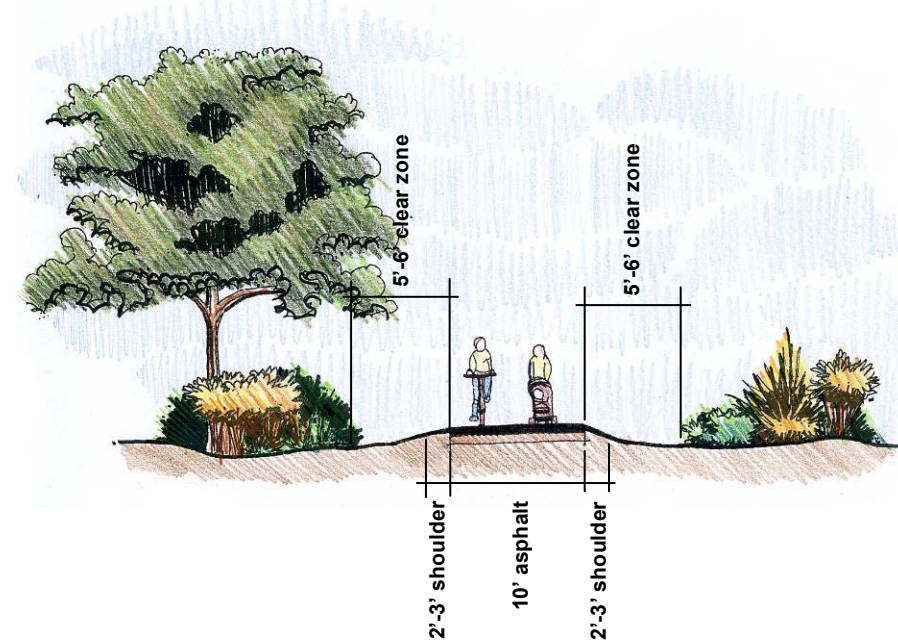


Figure 5.3.1 Paved Surface Trail

paving material is asphalt, but concrete or recycled plastic lumber boardwalks may be used where need or context dictates. Figure 5.3.1 above illustrates the typical Paved Surface Trail.

Equestrian Trails

Equestrian trails are intended to accommodate equestrians and their horses, but also serve as an alternative path for pedestrians and cyclists on wilderness terrain bikes. Equestrian trails are always unpaved, soft surface trails, and therefore restrict the use of in-line skaters and cyclists on road bikes by design.

Equestrian trails may be a maximum of 10' wide, though they are generally 3-5' wide as conditions allow, with 2-3' of cleared shoulder on each side of the trail. The surface material may be finely shredded bark mulch, gravel, or crushed stone. Figure 5.3.2 illustrates a typical Equestrian Trail section.

Special attention needs to be paid to underpasses and bridges that are intended to accommodate horses—clearance under these structures needs to be 12' minimum to allow a rider on horseback to pass through the area without dismounting. It is also desirable to have mounting blocks on both ends of bridges to allow riders to dismount in case they do not feel safe riding under the bridge on their horses. It is recommended that cities and the county maintain as large a separation as possible between the paved multi-purpose trail facilities to prevent conflicts with other trail users and dogs.

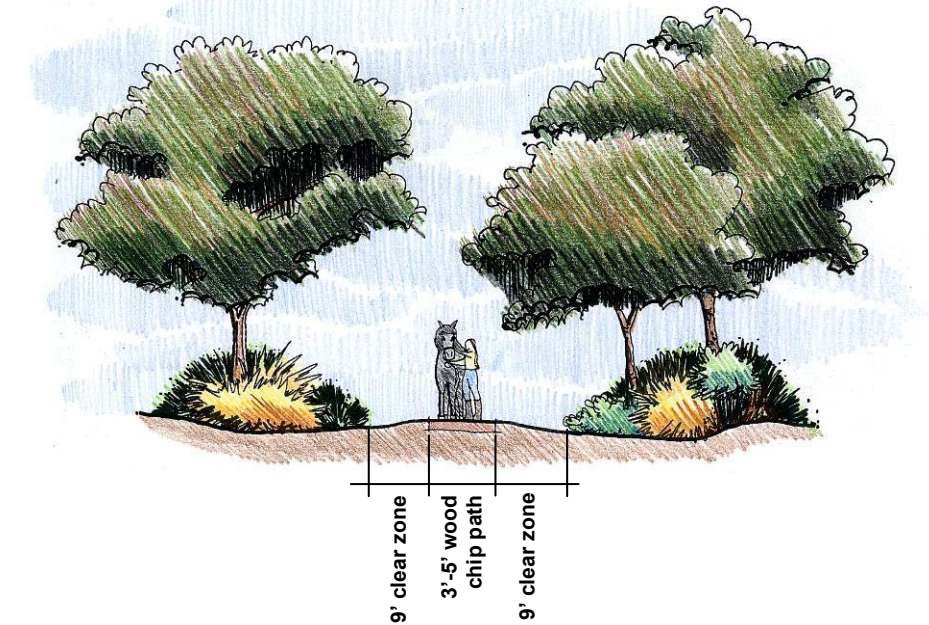


Figure 5.3.2 Equestrian Trail

Water Access Facilities

The National Park Service Rivers, Trails, and Conservation Assistance Program has developed a comprehensive design guide for canoe launches titled *Logical Lasting Launches: Design Guidance for Canoe and Kayak Launches* (see bibliography), which provides overall design considerations and requirements for developing water trail facilities as well as specific designs for a variety of needs and conditions.

As explained in this guide, “A logical, lasting launch provides safe and easy access for paddlers while accommodating the topographic, climatic, and ecological characteristics of its location. Ideally, its construction is cost-efficient and durable and has little impact on the environment and riparian ecology.”

Additionally, “guidelines for newly designed, constructed, and altered recreation facilities issued by the ADA in 2002 require that all public boat launches, which include fixed and floating structures of all sizes, comply with ADA Accessibility Guidelines (ADDAAG) standards.”

Water Trail Put-Ins and Take-Outs

The chart below describes the ideal design for creating an accessible put-in or take-out, as mentioned in *Logical Lasting Launches*. These facilities should be located as close as possible to parking facilities to minimize the distance that users must carry their boats to and from water access points. Water trailhead facilities are described below in the “Trailheads” section.

Put-ins and take-outs should be big enough to facilitate the large groups that are common to and recommended for river trips, and also

JORDAN RIVER TRAIL MASTER PLAN

to provide the space necessary to accommodate water trail users with special needs. However, the size and design will be limited by site constraints at individual locations.

Table 5.3.1 and Figure 5.3.3 explain the preferred designs for put-ins and take-outs.

Table 5.3.1 General Recommendations for Designing an Accessible Launch

Height Above Water	Between 9" and 2' from highest expected water level
Width	At least 5' wide, preferably 6' to 12'
Length	At least 25' to allow paddlers "dry" access to entire length of their boats
Slope	ADA Accessibility Guidelines require that slopes not exceed 8.33% wherever possible; A slope exceeding 15% will make transition from land to water difficult for any paddler
Support	Handrails or other support structures, including step-down designs or ropes, help paddlers balance their weight during put-in and take-out
Location	Ideally in areas without heavy flow, erosion, exposure to elements, heavy boat traffic, or fragile riparian habitats

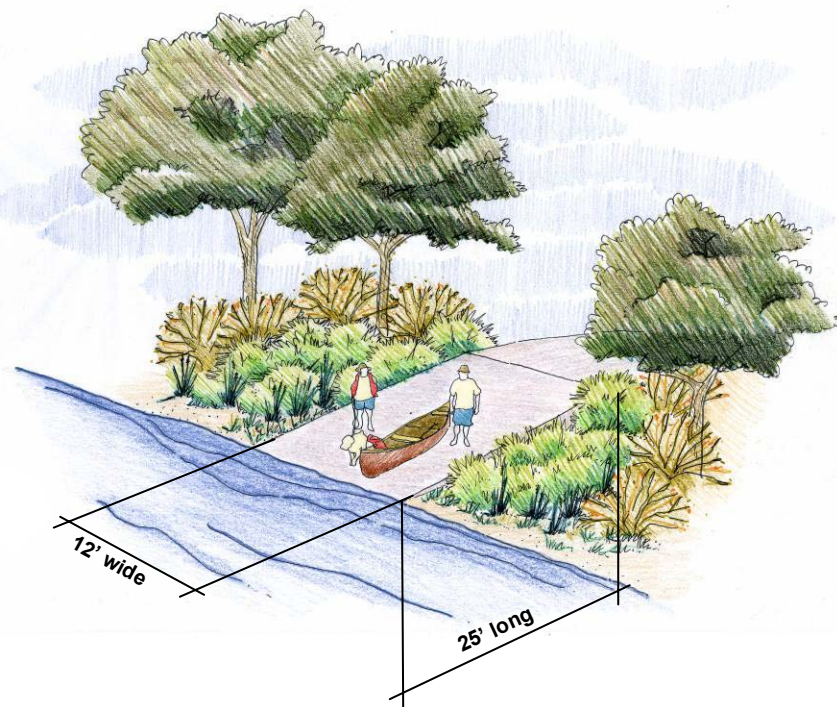


Figure 5.3.3 Preferred Dimensions for Launch/Take-Out Facility

The existing put-in and take-out at 1700 South, waypoint 11-08, near Raging Waters, is the best example of an accessible water trail access point along the Jordan River, and is frequently used by SPLORE and other organizations to launch larger group river trips.

Water Trail Portages

A portage is a combination of a put-in and a take-out facility that allows water trail users to safely avoid dams or other hazards in a waterway by exiting the water upstream of the hazard, and re-launching downstream. As mentioned in *Logical Lasting Launches*,

"An effective portage should include:

- Clear, well-marked signage allowing paddlers sufficient time to reach shore before take-out
- A path at least 2' wide around the [hazard], with a slope no steeper than a 1:3
- An ADA accessible portage cannot have a slope that exceeds 8.33% or 1:12
- At least 8' overhead clearance on the path and 4' to 8' clearance on either side
- A vertical distance of 12" or less between the height of boat and shore
- A route that minimizes the distance that paddlers must carry their boats
- Access points located on inside bends or areas of calm water
- An escape path downstream from portage, when possible, giving paddlers an additional area to egress the water if they miss a take-out or portage."

Proper signage is critical for making portages effective because boaters need sufficient warning and time to navigate safely to the portage take-out. Water trail signs are discussed below under the "Signage" section. Additional safety measures such as the barrels at waypoint 2-08 may help warn boaters of major water trail hazards.

Trailheads

Trailheads serve as access points, staging areas, and information stations for trail users. Requirements vary depending on uses for individual trail segments and can be affected by available land and funding.

Given the hot summer climate, it is recommended that parking be adequately shaded. The implementation of a well-designed shading strategy provides a level of order and structure, transforming parking lot surfaces from undifferentiated asphalt expanses into clearly articulated parking areas



Figure 5.3.4 Multi-Purpose Oxbow Trailhead

that are welcome transitions from sun-exposed physical activity along the trail corridor.

Connections between the trail and trailheads should be clear and obvious. Trailhead signs should be well-sited to clearly identify the trailhead from the parking lot and provide a visual clue of where the trail starts.



Figure 5.3.6 Trail Access Point

Trail use during all seasons has been increasing, and in order to accommodate year-round use, this plan recommends that restroom facilities remain open at key trailheads throughout the year.

Trail Entry Barriers

Motorized vehicles of any kind are not allowed on the trail, with the exception of authorized maintenance vehicles. There have been reports and incidents of unauthorized vehicular access in several areas along the trail. Gates and special barriers may be required at certain trailheads to prevent unauthorized motorized vehicles from entering the trail.



Figure 5.3.5 Trail Entry Barriers

Multi-Purpose Trailhead

Trailheads that serve paved multi-purpose trail segments should provide adequate parking for cars and trucks; restroom facilities; drinking fountains; site furnishings such as benches, picnic tables, trash receptacles, bike racks, and lighting; and trailhead informational

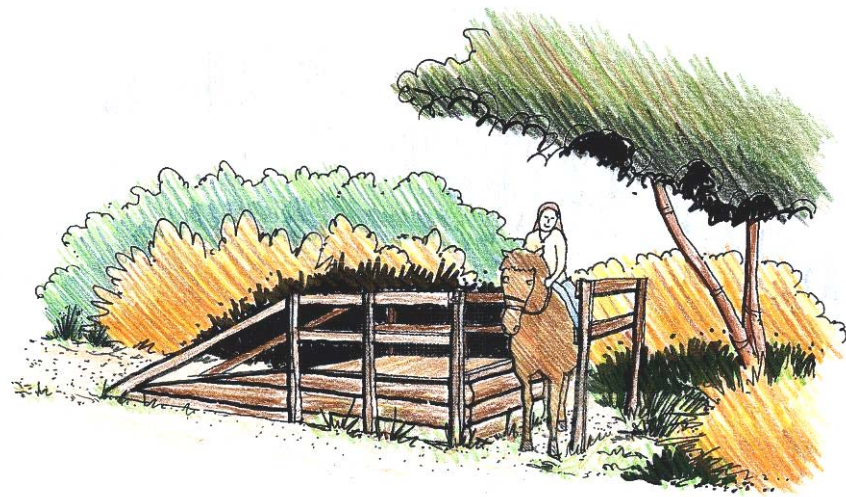


Figure 5.3.6 Equestrian Mounting Platform

signs. Parks located along the trail corridor are often able to accommodate most of these uses, and additional features such as playgrounds or additional recreational facilities while also serving as a trailhead for the Jordan River Trail corridor. Sometimes, however, space is limited, and only minimal trailhead facilities can be accommodated, which include limited parking, trash receptacles, and trailhead informational signs.

Equestrian Trailhead

Equestrian trailheads have similar requirements as the multi-purpose trailheads discussed above, with the additional needs for pull through parking to accommodate horse trailers and the trucks that are pulling them. Other desired facilities are loading/unloading platforms and ramps, tie-up areas, access to fresh water for horses, and small corrals as site constraints allow. Signage should clearly indicate trailheads that accommodate equestrian use.

Water Trailhead

Water trailheads have similar requirements as the multi-purpose trailheads discussed above, with the additional need for parking in close-proximity to well-designed put-ins and take-outs. Parking lots at water trailheads should be big enough to facilitate the large groups that are common to and recommended for river trips, but will be limited by site constraints at individual locations.

Trail Access Points

Trail access points provide small-scale local access to the Jordan River Trail corridor and are typically located within residential neighborhoods, though some are found in more commercial or industrial locations. Facilities are basic and include access via a path, sidewalk, or ramp. Sometimes a trail entry barrier is necessary to prevent unauthorized motorized vehicles from accessing the trail. Trail access points should be signed to raise awareness of the Jordan River Trail corridor.

Lighting

Improperly designed and unnecessary lighting can contribute to light pollution, create undesired impacts to wildlife, and detract from the quality of more natural areas along the river corridor. Therefore, lighting in the trail corridor should be limited to very urban areas and developed parks and recreation facilities where user safety requires lighting. Lighting should minimize light pollution to surrounding areas and be well-designed in coordination with associated site furnishings. Lighting may be more formal, composed of high quality materials, and possibly custom designed to associate the features with a particular city, park, or event. Underpasses need to be lit to a safe level.



Figure 5.3.7 Lighting

Bridges

Bridges limit the natural tendency of the Jordan River to travel within the natural meander corridor, detracts from the quality of water trails users, may create a hazard for water trail users during high water levels, and can add significant cost to trail development projects. Therefore it is recommended that the use of bridges be limited to situations where it is absolutely necessary, and when new bridges are added, they shall be designed to minimize risk and impacts to water trail use. Bridges need to maintain at least 6' clearance at high water level for water trail users to pass safely underneath.



Figure 5.3.8 Bridges Should Provide at Least 6' Clearance During High Water Levels

Vegetation

Appropriate vegetation should be re-established along new trails in accordance with recommendations in the *Jordan River Natural Conservation Corridor Report* and the on-going planning efforts mentioned in Chapter 1 of this plan, improving the quality of wildlife habitat and the experience for trail users.

Park and Recreation Sites

Park and recreation facilities located along the Jordan River Trail will be developed according to the needs and desires of the associated municipalities and organizations. However, the design of these

facilities should adhere to the trailhead and signage standards in this plan where these facilities interface with the Jordan River Trail corridor, serving as either a trailhead, access point, or destination.

Site Furnishings

The Jordan River Trail corridor travels through twelve cities and unincorporated areas of Salt Lake County, traversing a broad spectrum of terrain, from very dense urban developments to more natural, open riparian habitats. Site furnishings located along the trail should fit with the immediate surroundings. In urban locations or more formal parks, site furnishings such as benches, lighting, and trash receptacles may be more formal, composed of high quality materials such as metal, and possibly custom designed to associate the features



Figure 5.3.9 Recommended Park Bench Style



Figure 5.3.10 Recommended Park Trash Receptacle Style



Figure 5.3.11 Recommended Natural Area Bench Style



Figure 5.3.12 Recommended Natural Area Trash Receptacles Style



Figure 5.3.13 Recommended Park Bike Rack Style



Figure 5.3.14 Unique Site Paving



Figure 5.3.15 Artwork in Plaza Along Trail



Figure 5.3.16 Artwork in Park Along Trail

with a particular city, park, or event.

In more natural, open areas furnishings should be more “naturalistic” and simple, consisting of “rustic appearing” materials such as wood or recycled plastic lumber. The frequency and quantity of furnishings in more natural areas should be minimized to preserve the look and feel associated with these areas.

Some areas such as parks or plazas along the trail may feature unique



Figure 5.3.17 Gazebo Style Overlook



Figure 5.3.18 Deck Style Overlook

features such as art or special paving.

Rest Areas and Viewing Sites

Rest areas should be situated in shaded locations, or in areas that offer special beauty or unique viewing opportunities, and should be composed of materials such as wood or recycled plastic lumber that enhance and blend visually with the surroundings.

5.4 Signage Program

This plan recommends the development of specific design standards for signage along the Jordan River Trail. A unified Jordan River Trail

signage program will help increase public awareness about the river, access opportunities, and the recreational possibilities. A coordinated, standard system of signs will also provide safety and use guideline information as well as highlight the natural and cultural elements of the river corridor. The information that follows is primarily an inventory of existing signage types, along with recommendations for future design standard features.

As described in the *Bonneville Shoreline Trail Alignment Plan for Salt Lake County*, all trail signs must adhere to basic principles to fulfill their intended function, which include: fulfilling a need; commanding attention; conveying a clear, simple meaning; commanding respect from trail users; and giving adequate time for proper response.

Sign Standards

The Federal Highway Administration (FHWA) has established color standards for traffic control signage, and these standards should be followed as a comprehensive signage program is implemented in the Jordan River Trail corridor.

- Yellow—General Warning
- Red—Stop or Prohibition
- Blue—Services Guidance or Tourist Information
- Green—Movements Permitted & Directional Guidance
- Brown—Recreation and Scenic Guidance
- Orange—Temporary Traffic Control
- Black—Regulation
- White—Regulation



Figure 5.4.1 Signalized Trail Crossing Directional Sign

The inclusion of the Jordan River Parkway logo in a manner consistent with other trail corridor signage will create a continuity to the entire river corridor, and reinforce the importance of these “official” messages for trail users. In addition, all signage should use consistent fonts for continuity.

Trailhead Direction Signs/Through Trail Signs

Trailhead direction signs should be located along major roads near the Jordan River to easily lead motorists and cyclists to trailheads. These signs will help increase public awareness of the Jordan River Parkway and trailhead locations. This plan recommends two types of directional signs. The first would be incorporated as part of signalized pedestrian crossings, as shown in figure 5.4.1, and should be consistent with the existing road sign style in the vicinity. The second type of sign, intended to be a roadside directional sign, should feature the Jordan River Parkway Trail logo in full color, with the text “Jordan River Parkway Trail,” and have an arrow indicating the direction to the trail access point.

The trail should also be signed at key junctions to notify trail users of the main trail’s direction so that trail users do not end up in neighborhoods or other ‘dead-ends’.

Trailhead Information Signs

Trailhead information signs should be located at formal Jordan River Trailheads to provide important trail information such as:

- Trailhead name;
- Trailhead location;
- A map indicating the trailhead location in relation to the overall

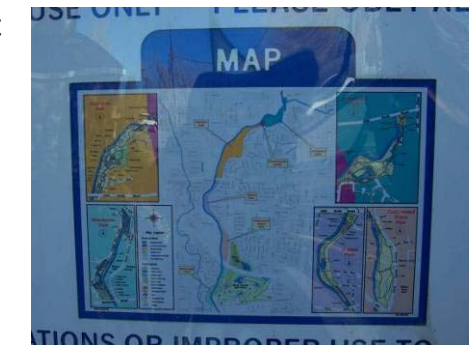


Figure 5.4.2 Good Example of Contextual Trail Map with Insets of Destination Details

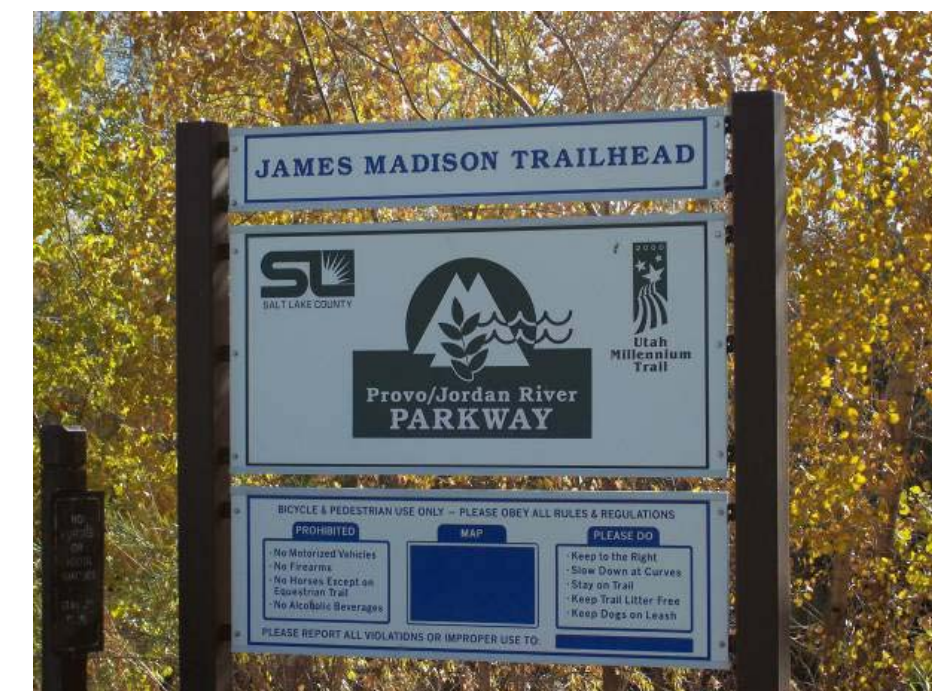


Figure 5.4.3 Typical Trailhead Sign

- trail system, trail junctions, mile marker sign locations, locations of parks and destinations, and unique natural areas;
- A description of permitted uses and hours;
 - Trail etiquette;
 - Other important information such as emergency phone numbers and contact information for comments or complaints; and
 - A box for storing hand-held trail maps.

The Foundation for the Provo Jordan River Parkway, in association with Salt Lake County, recently developed and implemented a new design for trailhead signs, and this new sign standard should be incorporated as new trailhead signs are added or as older signs are

upgraded and improved.

Trail Junction Signs

Trail junction signs should be located where the Jordan River Trail corridor connects to east/west trails, major bikeways, and potential destinations. These signs should be brown with white text, as shown in figure 5.4.4, and should indicate direction and distance to potential destinations.



Figure 5.4.4 Jordan River Trail Junction with River Oaks Golf Course Cart Path

Trail Access Signs

Trail access signs should be located at all trail access points that do not serve as official trailheads with the standard trailhead signage. These trail access signs, as shown in figure 5.4.5, clearly identify the connection to the Jordan River Trail, and provide guidelines and use information for the corridor in general.



Figure 5.4.5 Recommended Trail Access Sign



Figure 5.4.6 Recommended Mile Marker Sign for Salt Lake County

Mile Marker Signs

Milepost marker signs should be contiguous through the corridor, indicating cumulative mileage in both directions from the County boundary. These signs should be located regularly at half-mile intervals. Figure 5.4.6 illustrates the recommended design style, which is similar to the mile markers used by Utah County. Due to the need for consistency and continuity, the mileage marker system should not be installed until all of the gaps are completed.

Location Signs

It is essential that both paved-multi-purpose trail and water trail users are aware of their location in the Jordan River Trail corridor for emergency purposes. A comprehensive system of location signs used in conjunction with mile marker signs not only allows trail users to inform emergency personnel of their location and provides emergency workers with more detailed access information, but also helps users become more familiar with their surroundings, and therefore more comfortable using the trail corridor.



Figure 5.4.7 Recommended Location Sign Design



Figure 5.4.8 Recommended Location/Information Sign Design for Water Trail

Location signs should inform users of their whereabouts as well as the location of adjacent destinations and facilities, such as providing names or where multi-purpose and water trails interface with roads, bridges, parks, or other facilities along the Jordan River Corridor.

This plan recommends the designs shown in figures 5.4.7 and 5.4.8. The first design, simple brown with white lettering, should be located at all major cross streets, parks, trail access points, and special features such as nature centers or points of interest. The second design conveys both location and facilities information at major points along the water trail. These signs should be located near take-outs and portages where appropriate.

Trail Closed Due to Flooding Signs

The level of water flow in the Jordan River varies throughout the year due to many factors, including releases of water from Utah Lake when it reaches “compromise level”, spring runoff from the surrounding watershed, diversions into canals throughout the corridor, and reduced water levels later in the year as the water supply diminishes. Several underpasses along the river are flooded during high water flows, and become impassable to paved multi-purpose and equestrian trail users. In addition, several vehicular and pedestrian bridges that have low clearances become impassable to water trail users during peak water flows.

These trail segments are closed to trail users during high flows and are designated as such with signs indicating the trail closures. These signs should be the same type used for road hazards that fold in half so the warning message is not displayed when the trails are not flooded. This will require dedicated personnel to ensure that these signs are accurately displayed during trail flooding. Signage



Figure 5.4.9 Recommended “Trail Closure” Signage

at trailheads indicating trail closures due to flooding are also recommended.

Water Trail Hazard Signs

There are several hazards located along the Jordan River that are impassable to water trail users, or may be undesirable for less-experienced boaters, requiring portages accompanied by proper warning signs. Chapter 4 discusses existing water trail hazards and provides specific recommendations. As mentioned above in the “Portage” section, proper signage is critical for making portages effective because boaters need sufficient warning and time to navigate safely to the portage take-out.



Figure 5.4.10 Recommended Water Trail Hazard Style

Logical Lasting Launches recommends two resources for detailed information on effective water trail signage: *Publishing Trail Guidance: Maps and Guides, Guidebooks and Signs* (North American Water Trails, 2002); and the Federal Energy Regulatory Commission’s (FERC) website: http://www.ferc.gov/industries/hydropower/safety/guidelines/signage/report/Safety_signage.htm. As stated on the FERC website, “while the specific approach of each agency or group to safety may vary, the basic elements of an effective safety signage program are the same:

- Conduct a thorough needs assessment;
- Develop an effective design;
- Standardize format and message;
- Properly place and locate signs;
- and perform regular maintenance.”

Signs should be highly visible and properly placed, have clear and concise messages indicating type and level of risk and, indicate proper response, and allow plenty of time for user to respond.

Signs for water trail facilities and hazards need to be individually assessed for placement and design to maximize visibility and safety for each element.

Water Trail Put-In and Take-Out Signs

Another component of developing a consistent and effective signage system for the Jordan River Corridor is proper signing of water trail put-ins and take-outs. These signs should be consistent with the other trail corridor signage, following the FERC and FHWA recommendations for safety. Signs indicating take-outs need to be highly visible to watercraft users, and allow plenty of time for boaters to respond accordingly. Put-in signs need to clearly indicate entry access points for people beginning a trip or continuing a trip through a

particular section. These signs may include information similar to that shown in figure 5.4.8 if it is deemed appropriate to the safety needs of water trail users.

Interpretive Signs

Interpretive signs should be distributed at key locations along the paved multi-purpose and water trails to provide educational information related to the trail corridor. Signs may include information about the history of the Jordan River, wildlife and habitat, restoration and protection efforts, unique ecological features, and local history and culture. Due to the unique nature of each interpretive opportunity, a specific design standard is not recommended here. Rather, this plan simply recommends that these signs be well-designed and should incorporate the Jordan River Parkway logo.

General Information Signs

Other information may need to be conveyed that does not fit into the above categories. For these remaining signs, it is recommended that these signs follow the recommendations of FHWA, with the color based on the type of information that needs to be conveyed. Figures 5.4.11 and 5.4.12 show general recreation use information, and are therefore brown. Icons and fonts should be consistent on all signs throughout the corridor, with international symbols that can be



Figure 5.4.11 General Trail Information Sign



Figure 5.4.12 General Trail Information Sign



Figure 5.4.13 Sign warning of Different User Group Interface



Figure 5.4.14 Typical Trailhead Sign

understood by a broad range of trail users. Figure 5.4.13 illustrates a general warning style recommended for intersections of different user groups. In cases of severe danger, bilingual text should be considered in addition to international symbols, as shown in figure 5.4.13.

5.5 Educational Nature Facilities

Educational Nature Facilities are recommended in the Jordan River Corridor in appropriate areas. It is recommended that these facilities have strong connections to Salt Lake County's school districts. Murray School District has an existing Educational Nature Facility along the Jordan River in Murray City, and it would be a valuable addition for the other school districts along the river as well.

5.6 Maintenance

Generally, maintenance of surface trails and the urban water trail are the responsibility of each jurisdiction along the corridor, although there are often opportunities to partner with others to perform the necessary maintenance functions. Maintenance is critical to the success of the trail corridor, and essential in keeping people and animals safe.

Weeds and Invasive Species

Vegetation within the corridor helps control erosion, reduces pollution and sedimentation, and provides habitat and food sources for wildlife. Weeds and invasive plant species need to be controlled to allow naturally occurring vegetation to out-compete and perform its valuable functions. Weeds, invasive vegetation, and even some naturally occurring vegetation also create areas where visibility is obscured, providing places for vandalism and crime to occur unobserved. Weeds with thorns, and spiky seed heads also create hazards, particularly for bicycle tires. Weed control along the corridor is the responsibility of each jurisdiction, except when the City has an inter-local agreement with Salt Lake County. Inter-local agreements typically hold the City responsible for maintaining parks and trailhead facilities along the parkway, while the County is responsible for maintaining the trail. Salt Lake County and state agencies are also assuming a role in the eradication of weeds and invasive species as they revegetate and improve river banks and riparian habitat.



Figure 5.6.1 Invasive Phragmites

- Jurisdictions should cooperate with county and state agencies in their efforts to restore riparian vegetation and eradicate weeds and invasive species.
- Provide 2 feet clear of vegetation adjacent to the paved trail.
- Sweep the trail of stones, gravel, "goat heads", and dirt at least weekly using equipment designed for such maintenance.
- Remove hazard-causing weeds within 5 feet of the trail using

appropriate and safe means.

- Require property owners adjacent to the corridor to mow vegetation that obscures visibility, and to control weeds and invasive species.

Trail Surfacing and Repairs

Over time, asphalt trail surfacing will require repair, and periodic storm and flood events may remove or badly damage large sections of trails. Minor trail repairs are the responsibility of the local jurisdiction; major trail repairs are the responsibility of Salt Lake County.

Crime and Vandalism

Crime and vandalism prevention does occur along the trail corridor.

- Incorporate CPTED (Crime Prevention Through Environmental Design) concepts into trail design, and using those principals, evaluate areas where crime and vandalism can be facilitated.
- Each community along the corridor should initiate a bicycle patrol that rides the corridor one or twice a day at random times. This patrol could include volunteers.

Trash and Debris Removal

Trash and debris is not only unsightly and distracts from the enjoyment of an important natural and recreational resource, it often creates conditions that are unsafe for surface trail users, water users, and even wildlife living-in or migrating through the corridor. Trailhead signing currently states that users should keep the trails "litter free", but more could be done to encourage users to clean up along trails.

Small items of trash and debris along the trail often include metal, glass, wire, items contaminated with human blood or bodily fluids, and other sharp and dangerous items. While each jurisdiction is responsible for removal of trash and debris, available resources for their removal are often stretched or virtually non-existent. In these cases where maintenance budgets are inadequate, opportunities to cooperate with local trails organizations or service groups are a viable and useful way to provide periodic and consistent clean-ups, and at the same time build relationships and promote positive community interaction.



Figure 5.6.2 Section of Trail Washed Out



Figure 5.6.3 Trail Vandalism

- Conduct twice yearly volunteer clean-ups along the entire corridor.
- Coordinate clean-ups among jurisdictions to help boost participation and get information disseminated more readily.
- Jurisdictions should provide adequate trash and recycling receptacles and dog waste stations at trailheads and at stopping points along the trail, and empty them regularly.
- Encourage trail users to pick up trash and debris, and discard it in the next available container or carry it home for disposal.

- water quality improvement projects.
- Read the labels and use herbicides, pesticides, fertilizers, and household chemicals appropriately.
- Use organic products or methods of treatment when within 50 feet of the river or its tributaries and drainages.
- Do not discard chemicals or chemical containers into the river, or its drainages and tributaries.
- Report incidents or suspicious behaviors that could have an adverse effect on water quality to local enforcement agencies.

Large items of debris such as concrete chunks and metal structures, abandoned bridge abutments and foundations, and other industrial and structural elements are a particular hazard to water trail users. These kinds of debris require large equipment and trained personnel to remove properly and safely, and may require coordinated efforts.



Figure 5.6.4 Trash Along Trail

- Remove dangerous and unsightly debris from the river using safe and appropriate means.
- Cooperate with adjacent land owners, municipalities, or other governmental and infrastructure agents and organizations to remove large debris.

Pollution

Several studies and efforts are addressing pollution in the Jordan River, some of which are summarized in Chapter 1. Currently, State and County resources are being applied to upgrade the river's water quality, or at a minimum assure that it does not deteriorate further. These organizations and efforts are addressing large industrial pollution sources, municipal sources, and broader-based watershed and water quality issues. Local jurisdictions should cooperate fully with State and County efforts.

Individual communities and landowners along the corridor and its tributaries and drainages can do their part by becoming educated and aware of urban and agricultural sources of pollutants including fertilizers, pesticides, herbicides, household chemicals, and other sources that are not currently regulated effectively. In spite of label warnings and conditions of use, too many people are still using herbicides and pesticides inappropriately in areas where they can easily run-off into the river or its tributaries. It is important that everyone be aware that individual actions, multiplied hundreds and thousands of times, ultimately result in cumulative effects that are damaging and often irreversible.

- Cooperate with local and state governmental agencies, and

6.0 Implementation

6.1 Environmental Guidelines

This report acknowledges and supports the recommendations made in previous planning efforts. The Jordan River Natural Areas Forum (JRNAF) developed a plan for Salt Lake County titled “Jordan River Parkway Trail Completion Prioritization—Salt Lake County Area,” which was completed in February 3, 2005. Some of the projects recommended in the JRNAF Prioritization Plan have been completed since the document was adopted, and others are currently in the negotiation or design stage. However, the Environmental Guidelines set forth in that document are still relevant, well-thought-out guidelines that should be adhered to as recommendations in this plan are implemented.

“In designing the trail, consideration should be given to environmentally sensitive areas and resources, particularly wetlands, the riparian zone, the Jordan River (including banks and floodplain), steep slopes, viewsheds, mature native trees, and lands that provide unique habitats for wildlife. There has been some discussion as to whether or not the trail should be allowed on both sides of the river. There are multiple trails in some areas already. Each new trail section should be carefully evaluated, including discussion among stakeholders, to determine appropriate trail routing, design, and construction, in compliance with all laws and regulations. (See “A guide to State, Federal, and Local Government Permitting Along the Jordan River Corridor” <http://www.governor.utah.gov/planning/jrnaf.htm>). In addition, we recommend following these guidelines:

1. Wetlands

- a. Avoid wetlands:
 - i. Align trail around them.
 - ii. Create natural buffers between wetlands and the trail.
- b. If the trail must go through wetlands, then use a bridge or boardwalk.
- c. If wetlands are affected, mitigation may be required by the US Army Corps of Engineers.

2. Riparian Zone

- a. Create a buffer between the trail and the river where possible. Consider that the river may meander and change position and that flooding is likely to occur at some future point. The buffer will protect the trail infrastructure as well as the natural river process and vegetation.
- b. Allow views of the river, but discourage off-trail travel through natural buffers such as vegetative screens (e.g. willow, rose). Buffers should be characterized as adequate or sufficient and functional to mitigate the impacts of the trail to wildlife, views, vegetative communities, etc.
- c. Unnecessary bridges should not be encouraged or permitted.
- d. Post signs to encourage people to appreciate and respect the trail and nature.
- e. Provide areas where people can have some access to the river

or views, but the majority of the trail length should be moved away from the river:

- i. Consider opportunities (e.g. kiosks) to educate the public about natural processes, hazards, wildlife, and the benefits of the environment.
- ii. These should be designed to avoid damage to the riparian zone, avoid cut banks, avoid the need to rip-rap, etc.

3. Jordan River, Banks, and Floodplain

- a. Create a buffer between the river and trail where possible. If possible, place the trail away from the river.
- b. Strive to create a diversity of experiences. Basically, this means that the trail does not need to be the same 10 feet from the river for the entire length of the river. Let the trail meander away from the river, perhaps around a wetland, around a parcel of open space, through a woodland (responsibly), or up on a ridgeline.
- c. Avoid the need to rip-rap the banks of the river.
- d. Protect natural vegetation.
- e. Let the river meander.
- f. When the Jordan floods, have alternative routes or have means of closing the trail.

4. Viewsheds

- a. Take advantage of views that allow people to see the Wasatch, the Oquirrh, or other natural areas. This might mean a rise in the trail, a ridgeline, or even protection of a parcel as open space so that a large building (or complex) is not built.
- b. Coordinate with adjacent land owners in development.
- c. It is unacceptable for the trail to diverge in some areas from the narrow confines of the river corridor.

5. Steep Slopes

- a. Steep slopes along the river’s edge should be avoided, because of erosion and for public safety.
- b. If trail must go through or over slopes, allow drainage and passage of water.

6. Mature Trees (Native Species) and Natural Areas

- a. Where stands of native trees (e.g. cottonwoods, willows) exist, align the trail around or design so that minimum number of trees are cut or damaged.
- b. Areas protected or set aside as open space or wildlife habitat are not automatically trail material. Some parcels have conservation easements or other deed restrictions which might prohibit trails, buildings, and other facilities.
- c. It is also important to recognize and respect the use of the land, and to coordinate with the landowner.

7. Additional Considerations

- a. Wherever natural areas and/or habitat will be impacted, mitigation should be included.
- b. Lighting: Recommend not light the trail to avoid disturbance to wildlife. It may be acceptable to light trailhead and parking areas, but have lights on low light poles, directed down rather than up or laterally, and use minimum amount needed.
- c. Trash: Encourage people not to litter and place receptacles at trailheads, not along the trail. This will make it easier to collect,

and will be a lesser enticement for raccoons, skunks, and such residing by the river. Many people like raccoons and skunks, however, they can create health concerns, can be a nuisance, and are predators on birds, bird nests, and other wildlife.

- d. Crime Prevention Through Environmental Design (CPTED) projects need to be coordinated with agencies and other groups interested in the river, especially with regard to vegetation removal and lighting.
- e. Establish and maintain natural landscapes as part of the trail design, funding and construction, including invasive weed control. (See Utah Noxious Weed List http://ag.utah.gov/plantind/nox_utah.html):
 - i. Adopt ways to keep weeds from spreading.
 - ii. Adopt weed control methods.
 - iii. Assure wetlands remain viable, especially where they were established for mitigation.
 - iv. Plant native vegetation.
- f. Dogs:
 - i. Keep dogs on-leash; do not allow them to run free for the safety of people, pets, wildlife, and the river.
 - ii. Encourage people to pick up after their pets".

6.2 Rights-of-Way and Development

The following information and guidelines on rights-of-way and development along the Jordan River Corridor were developed by JRNAF in the “Jordan River Parkway Trail Completion Prioritization—Salt Lake County Area” plan.

“There is not a public right-of-way for a trail along the entire river—only where it has been acquired. There are regulatory controls over development, including trail development, within the river corridor. Controls include:

- County Flood Control—Twenty-five feet along the banks
- State of Utah Parks and Recreation—One hundred and fifty feet from the banks of the river (or as far as the flood plain)
- State of Utah Sovereign Lands—the riverbed
- State of Utah Water Rights—Stream modifications
- State of Utah Department of Transportation—Where roads exist or are developed
- Local Government Zoning—All areas
- US Army Corps of Engineers—Wetlands and riparian areas

Although these controls exist, residential and commercial development occurs within the floodplain—there is no absolute prohibition. The trail group received comments regarding the issue, but considers them outside the scope of this report except as follows:

1. Any new road or bridge construction projects should include underpasses to provide for the trail.
2. When new residential or commercial development is proposed, developers should provide rights-of-way for the trail.
3. New development should always follow the environmental guidelines above.”

6.3 Potential Funding Sources

Funding trails projects is one of the most challenging aspects of the plan. Public funding is increasingly competitive, and many programs are either not being funded or have been substantially reduced. Money from foundations and other philanthropic organizations and groups is also difficult to acquire, in part because available funds are highly sought-after. Nevertheless, all potential sources should be explored to the fullest, and **collaboration between city, county, and state agencies and organizations will help maximize the potential of scarce funds.** Having a good trails master plan is the first step toward securing implementation funding.

City and County Funding Sources

City and County General Funds

Salt Lake County general funds are regularly allocated for the implementation of trails projects, and several Jordan River Parkway trail projects have been implemented using those funds. The same is true with general fund allocations by cities located along the river. However, available funds are limited, and competition is extremely tight between the various on-going county trails projects, as well as increasing demands to use general funds for other projects and purposes. City general fund accounts are also in high demand for a broad variety of projects. Projects which can leverage general fund allocations to secure additional matching funds are able to stretch the dollars farther and accomplish more with limited resources.

Bonding

Cities and Salt Lake County can bond for trail development and spread the cost over many years. Bonding is a common approach, where repayment of the bonds comes from general revenue sources such as property and sales tax, or other earmarked tax revenue. However, unless it is a very large project that benefits many people, bonding is not likely to be a primary source.

ZAP Tax

The **Zoos, Arts, and Parks (ZAP)** Tax has been used to fund many trails throughout Salt Lake County. These funds are administered by the County and can be used to fund county projects within the boundaries of local communities. ZAP Tax funds have been used for trails development in the past, and continue to be a community-supported source of recreation funds. The program was recently extended to 2016 as a result of a countywide vote.

Community Development Block Grants

Community Development Block Grants (CDBG) can be used for trail development in areas of Cities and the County that qualify as low and moderate-income areas. CDBG funds may be used to upgrade trails and improve accessibility to meet requirements of the Americans With Disabilities Act (ADA).

Development Dedications, Exactions, or Contributions

Development contributions are a means for requiring, as a condition of development approval, a builder or developer to give something to the

City or County for the development of public facilities. Contributions can range from impact fees to land dedications for public trail facilities. In the southern part of Salt Lake County, where growth pressures are the highest, development dedications or exactions that are negotiated through the development review process are an excellent means of acquiring rights-of-way, easement, and actual trails development.

State Funding Sources

State funding programs receive allocations from the state legislature, and in some years funds may be very limited or non-existent, even though the program is still in place. It is important to check annually with each agency to determine if funds are currently available.

Centennial Non-Motorized Paths and Trails Crossings

The Utah Department of Natural Resources, Parks and Recreation Division administers this program. The funds are to be allocated towards the provision of safe and continuous pedestrian, bicyclist, and other human-powered and equestrian transportation paths and trails, and to provide access across highways and other impediments. A local match is required.

River Enhancement Programs

The Division of Utah Parks and Recreation administers these funds which provide funds to cities and counties for property acquisition and/or development for recreation (trails), conservation, and wildlife habitat along streams and rivers in highly populated areas. A local match is required by the sponsoring jurisdiction.

Non-motorized Recreation Trails Program

The Division of Utah Parks and Recreation also administers funds for the Non-motorized Trails program, which provides funds for signing, trails, and right-of-way. A local match by the sponsoring jurisdiction is required.

LeRay McAllister Critical Land Conservation Fund

The fund is administered by the Utah Quality Growth Commission and provides funds each year to preserve or restore critical open or agricultural lands in Utah. Money from the fund could be used to purchase land or easements to complete gaps in the existing trail system. Applicants must provide matching funds equal to or greater than the amounts of money received from the fund. Funds must be spent within one year from the date of the grant award. The size of parcels for a purchase is limited to 20 acres or less. Purchases of conservation easements or restoration projects are exempt from this restriction. In recent years, the LeRay McAllister fund provided close to \$2 million each year for conservation projects, however, in recent years fund allocations have varied significantly.

Land and Water Conservation Fund

This Federal fund is available to States, and in Utah is administered by the Utah State Division of Parks and Recreation. Funds are matched with local funds for the acquisition and development of trails, redevelopment of trails and facilities, improvements to accessibility,

and other trail programs and facilities that provide close-to-home recreation opportunities for youth, adults, senior citizens, and persons with physical and mental disabilities.

Utah Reclamation Mitigation and Conservation Commission (URMCC)
URMCC has previously funded Jordan River projects used specifically to improve fish and wildlife habitat, and to protect lands adjacent to the river from development through conservation easements or acquisition. While not specifically a trails-development organization, improving access to the river for recreational purposes and public education does fit into their mission. URMCC is actually an agent of the federal government, established in Utah to design, fund, and implement projects that offset impacts to fish, wildlife, and recreational resources caused by the Central Utah Project.

Federal Funding Sources

Transportation Efficiency Act-21 (TEA- 21) Reauthorization Bill/ SAFETEA-LU

The Transportation Efficiency Act-21 (TEA-21) was recently reauthorized by Congress as the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU). The bill authorizes the Federal surface transportation programs for highways, highway safety, and transit for the 5-year period 2005-2009, providing \$286 billion for a wide variety of transportation and roads projects throughout the nation. These funds are also valid on projects that enhance the ability for children to walk and bike to school.

Although specific project details are still being formulated, these funds are a potential source for funding portions of the Jordan River Trails system. In the past, TEA –21 funded several Salt Lake County trails projects, including portions of the Bonneville Shoreline, Jordan River Parkway and Dimple Dell trail systems. Assuming that funding will follow previous TEA-21 processes, funds will likely be made available to Salt Lake County directly through the Utah Department of Transportation or Wasatch Front Regional Council.

Private and Public Partnerships

Private Fundraising and Corporate Sponsorships

While not addressed as a specific strategy for trails and trail facilities, it is not uncommon that public monies are leveraged with private donations. Examples in the Salt Lake Valley include the Sorenson Recreation Center in Glendale and the Steiner Aquatic Center in Salt Lake City, where private foundations and organizations contributed funds for public recreation facilities. Corporate sponsors, particularly those located adjacent to the river where employees have access to the river for lunch-time walks are also likely partners. The partnerships may either fund trail development, or participate as volunteer organizations in the maintenance of trail sections.

Joint Development

Joint development opportunities can occur between Salt Lake County and other agencies or jurisdictions. In the case of the Jordan River Corridor Trails, trails and related facilities might be developed in

cooperation with the cities along the corridor, State agencies, local wildlife organizations, or other groups and organizations with similar interests. In order to make these kinds of opportunities happen, there must be on-going and constant communication between people, governments, business interests, and others.

Voluntary Efforts, Contributions and Miscellaneous Funding Sources

Service Organizations

Many service organizations and corporations have funds available for trail facilities. Recently, Salt Lake City and local and international Rotary Clubs combined resources to develop a universally accessible playground located in Liberty Park, which was dedicated at the opening of the 2002 Paralympic Winter Games. Other organizations such as Home Depot and Lowes are often willing to partner with local communities in the development of trails, as well as other park and recreation facilities.

In-Kind and Donated Services or Funds

Several options for local initiatives are possible to further the implementation of the trails plan. These kinds of programs would require a proactive recruiting initiative to generate interest and sponsorship, and may include:

- Adopt-a-trail, whereby a service organization or group either raises funds or constructs a given facility with in-kind services;
- Corporate sponsorships, whereby businesses or large corporations provide funding for a particular facility, similar to adopt-a-trail;
- Public trail construction programs, in which local citizens donate their time and effort to trail facility construction and/or maintenance; and
- Organizations such as the International Mountain Bike Association (IMBA), which have provided funds and leadership in similar situations.

6.0 Implementation

6.1 Environmental Guidelines

This report acknowledges and supports the recommendations made in previous planning efforts. The Jordan River Natural Areas Forum (JRNAF) developed a plan for Salt Lake County titled “Jordan River Parkway Trail Completion Prioritization—Salt Lake County Area,” which was completed in February 3, 2005. Some of the projects recommended in the JRNAF Prioritization Plan have been completed since the document was adopted, and others are currently in the negotiation or design stage. However, the Environmental Guidelines set forth in that document are still relevant, well-thought-out guidelines that should be adhered to as recommendations in this plan are implemented.

“In designing the trail, consideration should be given to environmentally sensitive areas and resources, particularly wetlands, the riparian zone, the Jordan River (including banks and floodplain), steep slopes, viewsheds, mature native trees, and lands that provide unique habitats for wildlife. There has been some discussion as to whether or not the trail should be allowed on both sides of the river. There are multiple trails in some areas already. Each new trail section should be carefully evaluated, including discussion among stakeholders, to determine appropriate trail routing, design, and construction, in compliance with all laws and regulations. (See “A guide to State, Federal, and Local Government Permitting Along the Jordan River Corridor” <http://www.governor.utah.gov/planning/jrnaf.htm>). In addition, we recommend following these guidelines:

1. Wetlands

- a. Avoid wetlands:
 - i. Align trail around them.
 - ii. Create natural buffers between wetlands and the trail.
- b. If the trail must go through wetlands, then use a bridge or boardwalk.
- c. If wetlands are affected, mitigation may be required by the US Army Corps of Engineers.

2. Riparian Zone

- a. Create a buffer between the trail and the river where possible. Consider that the river may meander and change position and that flooding is likely to occur at some future point. The buffer will protect the trail infrastructure as well as the natural river process and vegetation.
- b. Allow views of the river, but discourage off-trail travel through natural buffers such as vegetative screens (e.g. willow, rose). Buffers should be characterized as adequate or sufficient and functional to mitigate the impacts of the trail to wildlife, views, vegetative communities, etc.
- c. Unnecessary bridges should not be encouraged or permitted.
- d. Post signs to encourage people to appreciate and respect the trail and nature.
- e. Provide areas where people can have some access to the river

or views, but the majority of the trail length should be moved away from the river:

- i. Consider opportunities (e.g. kiosks) to educate the public about natural processes, hazards, wildlife, and the benefits of the environment.
- ii. These should be designed to avoid damage to the riparian zone, avoid cut banks, avoid the need to rip-rap, etc.

3. Jordan River, Banks, and Floodplain

- a. Create a buffer between the river and trail where possible. If possible, place the trail away from the river.
- b. Strive to create a diversity of experiences. Basically, this means that the trail does not need to be the same 10 feet from the river for the entire length of the river. Let the trail meander away from the river, perhaps around a wetland, around a parcel of open space, through a woodland (responsibly), or up on a ridgeline.
- c. Avoid the need to rip-rap the banks of the river.
- d. Protect natural vegetation.
- e. Let the river meander.
- f. When the Jordan floods, have alternative routes or have means of closing the trail.

4. Viewsheds

- a. Take advantage of views that allow people to see the Wasatch, the Oquirrh, or other natural areas. This might mean a rise in the trail, a ridgeline, or even protection of a parcel as open space so that a large building (or complex) is not built.
- b. Coordinate with adjacent land owners in development.
- c. It is unacceptable for the trail to diverge in some areas from the narrow confines of the river corridor.

5. Steep Slopes

- a. Steep slopes along the river’s edge should be avoided, because of erosion and for public safety.
- b. If trail must go through or over slopes, allow drainage and passage of water.

6. Mature Trees (Native Species) and Natural Areas

- a. Where stands of native trees (e.g. cottonwoods, willows) exist, align the trail around or design so that minimum number of trees are cut or damaged.
- b. Areas protected or set aside as open space or wildlife habitat are not automatically trail material. Some parcels have conservation easements or other deed restrictions which might prohibit trails, buildings, and other facilities.
- c. It is also important to recognize and respect the use of the land, and to coordinate with the landowner.

7. Additional Considerations

- a. Wherever natural areas and/or habitat will be impacted, mitigation should be included.
- b. Lighting: Recommend not light the trail to avoid disturbance to wildlife. It may be acceptable to light trailhead and parking areas, but have lights on low light poles, directed down rather than up or laterally, and use minimum amount needed.
- c. Trash: Encourage people not to litter and place receptacles at trailheads, not along the trail. This will make it easier to collect,

and will be a lesser enticement for raccoons, skunks, and such residing by the river. Many people like raccoons and skunks, however, they can create health concerns, can be a nuisance, and are predators on birds, bird nests, and other wildlife.

- d. Crime Prevention Through Environmental Design (CPTED) projects need to be coordinated with agencies and other groups interested in the river, especially with regard to vegetation removal and lighting.
- e. Establish and maintain natural landscapes as part of the trail design, funding and construction, including invasive weed control. (See Utah Noxious Weed List http://ag.utah.gov/plantind/nox_utah.html):
 - i. Adopt ways to keep weeds from spreading.
 - ii. Adopt weed control methods.
 - iii. Assure wetlands remain viable, especially where they were established for mitigation.
 - iv. Plant native vegetation.
- f. Dogs:
 - i. Keep dogs on-leash; do not allow them to run free for the safety of people, pets, wildlife, and the river.
 - ii. Encourage people to pick up after their pets".

6.2 Rights-of-Way and Development

The following information and guidelines on rights-of-way and development along the Jordan River Corridor were developed by JRNAF in the “Jordan River Parkway Trail Completion Prioritization—Salt Lake County Area” plan.

“There is not a public right-of-way for a trail along the entire river—only where it has been acquired. There are regulatory controls over development, including trail development, within the river corridor. Controls include:

- County Flood Control—Twenty-five feet along the banks
- State of Utah Parks and Recreation—One hundred and fifty feet from the banks of the river (or as far as the flood plain)
- State of Utah Sovereign Lands—the riverbed
- State of Utah Water Rights—Stream modifications
- State of Utah Department of Transportation—Where roads exist or are developed
- Local Government Zoning—All areas
- US Army Corps of Engineers—Wetlands and riparian areas

Although these controls exist, residential and commercial development occurs within the floodplain—there is no absolute prohibition. The trail group received comments regarding the issue, but considers them outside the scope of this report except as follows:

1. Any new road or bridge construction projects should include underpasses to provide for the trail.
2. When new residential or commercial development is proposed, developers should provide rights-of-way for the trail.
3. New development should always follow the environmental guidelines above.”

6.3 Potential Funding Sources

Funding trails projects is one of the most challenging aspects of the plan. Public funding is increasingly competitive, and many programs are either not being funded or have been substantially reduced. Money from foundations and other philanthropic organizations and groups is also difficult to acquire, in part because available funds are highly sought-after. Nevertheless, all potential sources should be explored to the fullest, and **collaboration between city, county, and state agencies and organizations will help maximize the potential of scarce funds.** Having a good trails master plan is the first step toward securing implementation funding.

City and County Funding Sources

City and County General Funds

Salt Lake County general funds are regularly allocated for the implementation of trails projects, and several Jordan River Parkway trail projects have been implemented using those funds. The same is true with general fund allocations by cities located along the river. However, available funds are limited, and competition is extremely tight between the various on-going county trails projects, as well as increasing demands to use general funds for other projects and purposes. City general fund accounts are also in high demand for a broad variety of projects. Projects which can leverage general fund allocations to secure additional matching funds are able to stretch the dollars farther and accomplish more with limited resources.

Bonding

Cities and Salt Lake County can bond for trail development and spread the cost over many years. Bonding is a common approach, where repayment of the bonds comes from general revenue sources such as property and sales tax, or other earmarked tax revenue. However, unless it is a very large project that benefits many people, bonding is not likely to be a primary source.

ZAP Tax

The **Zoos, Arts, and Parks (ZAP)** Tax has been used to fund many trails throughout Salt Lake County. These funds are administered by the County and can be used to fund county projects within the boundaries of local communities. ZAP Tax funds have been used for trails development in the past, and continue to be a community-supported source of recreation funds. The program was recently extended to 2016 as a result of a countywide vote.

Community Development Block Grants

Community Development Block Grants (CDBG) can be used for trail development in areas of Cities and the County that qualify as low and moderate-income areas. CDBG funds may be used to upgrade trails and improve accessibility to meet requirements of the Americans With Disabilities Act (ADA).

Development Dedications, Exactions, or Contributions

Development contributions are a means for requiring, as a condition of development approval, a builder or developer to give something to the

City or County for the development of public facilities. Contributions can range from impact fees to land dedications for public trail facilities. In the southern part of Salt Lake County, where growth pressures are the highest, development dedications or exactions that are negotiated through the development review process are an excellent means of acquiring rights-of-way, easement, and actual trails development.

State Funding Sources

State funding programs receive allocations from the state legislature, and in some years funds may be very limited or non-existent, even though the program is still in place. It is important to check annually with each agency to determine if funds are currently available.

Centennial Non-Motorized Paths and Trails Crossings

The Utah Department of Natural Resources, Parks and Recreation Division administers this program. The funds are to be allocated towards the provision of safe and continuous pedestrian, bicyclist, and other human-powered and equestrian transportation paths and trails, and to provide access across highways and other impediments. A local match is required.

River Enhancement Programs

The Division of Utah Parks and Recreation administers these funds which provide funds to cities and counties for property acquisition and/or development for recreation (trails), conservation, and wildlife habitat along streams and rivers in highly populated areas. A local match is required by the sponsoring jurisdiction.

Non-motorized Recreation Trails Program

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cooperation with the cities along the corridor, State agencies, local wildlife organizations, or other groups and organizations with similar interests. In order to make these kinds of opportunities happen, there must be on-going and constant communication between people, governments, business interests, and others.

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- Public trail construction programs, in which local citizens donate their time and effort to trail facility construction and/or maintenance; and
- Organizations such as the International Mountain Bike Association (IMBA), which have provided funds and leadership in similar situations.

Appendix

A.1 Notes from Stakeholder Group Meetings

Stakeholder Group Meeting #1 Notes **March 6, 2007; 3:00 p.m.; Room S4017 County Complex**

Attended by:

Tim Williams, South Salt Lake City
 Lyle Summers, West Jordan City
 Jeff Salt, Great Salt Lake Keeper
 Dave Hogue, Bluffdale City/Parkway Foundation
 Kent Player, South Valley Parkway
 Gordon Haight, Taylorsville City
 Brad Jensen, Draper City
 Steve Jensen, Salt Lake County
 Shane Ellis, Salt Lake County Engineering
 Scott Baird, Salt Lake County Engineering
 Tim Prestwich, Riverton City
 Vaughn Pickell, Bluffdale City
 Lynn Larson, Salt Lake County Parks and Recreation
 Emery Crook, Salt Lake County Parks and Recreation
 Jan Striefel, Landmark Design
 Lisa Benson, Landmark Design

1. The washout area at the narrows will be repaired this fall.
2. The trail under the Bangerter Highway bridge is now underwater. It probably needs to be relocated; a detour determined that will connect during high water events, or it needs an engineering solution. The same problem occurs at 3300 south. When Redwood Road/Camp Williams Road is upgraded, there will be a bridge and connection provided by UDOT.
3. The 11400 South road extension is in the future; so a temporary bridge at the South Jordan fishing ponds is desired and needed. Need also to keep in mind the water trail and the need to limit bridges which detract from the canoeing experience. Need to take into consideration the feeling of the experience and preserve natural areas.
4. There should be a set of guiding principals for the project. Some thoughts include:
 - Provide adequate public access to the Jordan River Trail, both surface and water..
 - Preserve open spaces and natural areas.
 - Achieve a balance between recreation, wildlife, etc. - a balanced system between the built environment and natural environment.
 - Close gaps and provide a connected system and provide a continuous trail.
 - Prioritize trails to be developed and land to be acquired.

- Link to other trails and take into consideration developments that can provide a dual use or shared use with other facilities, i.e. trailheads, etc.
- Think of the parkway as a whole entity, and the trail is a component.
- Plan for trail maintenance and develop costs for maintenance. (Murray City spends 1.5 million each year on maintenance.
- Provide opportunities for multi-modal use.
- Coordinate with the efforts of other river corridor planning activities.

5. There are other corridor plans in the works. Trails are just one element. There needs to be a strengthening of the ecological aspects.
 - The Mayors Corridor Plan with Envision Utah will focus on land use planning and open space prioritization.
 - Watershed Stewardship plan will identify natural restoration area.
 - Open Space Initiative
 - Jordan River Restoration Program.

Perhaps there is a need for a committee of representation from all the planning efforts so that it is coordinated.

6. If the Midvale project is not too far along, perhaps we can influence it.
7. West Valley City has plans for the trail on the west; but perhaps is it's not needed.
8. Getting public involved – maybe set up tables along the trail and ask trail users; also get press releases out and notes to sports and outdoors writers to get information about upcoming public meetings and open houses.
9. We may want to do another Stakeholder Meeting in early April to discuss in more detail the river trail. Jeff volunteered to do his power point presentation. Jan and Lynn will look at the schedule.
10. The County has an RFP out right now to design a trail in Bluffdale from 14600 South to approximately 15800 South, and to design the Jordan Narrows Trailhead.
11. Rose Creek drainage in Bluffdale is a possible trail corridor.
12. Avoid putting too many trails and facilities into an area, which can detract from reason people go to the corridor to begin with
13. Areas need to be prioritized as sensitive – the upper 1/3 of the corridor are the most sensitive areas.
14. Consider presenting to City Council in key segments to keep them informed and encourage support of the plan. Also consider press

conferences, press releases, and handing out info about the public meetings to people on the trail, including a press release about the launching of the website.

Stakeholder Group Meeting #2 Notes **April 2, 2007; 3:00 p.m.; Room S4017 County Complex**

Attended by:

Dan Fazzini, SLCBAC
 Susan Schroer, SPLORE
 Dell Cook, Salt Lake City
 Craig Thaxton, South Jordan City
 Suellen Riffkin, Resident
 Tyler Barlow, Bluffdale City Planning
 Gregg Cudworth, West Valley City
 Lyle Summers, West Jordan City
 Jeff Salt, Great Salt Lake Keeper
 Kent Player, South Valley Parkway
 Tim Prestwich, Riverton City
 Vaughn Pickell, Bluffdale City
 Lynn Larsen, Salt Lake County Parks and Recreation
 Jan Striefel, Landmark Design
 Lisa Benson, Landmark Design

1. Kent Player of Draper City shared photos of the trail near 12300South. The water is up several feet over the trail, and the bank is eroding rapidly.
2. The underpass at 7800 South is underwater. Dan Fazzini had difficulty finding a safe route across 7800 South due to the locked gate on the west side of the river and an improperly placed sign in the middle of the trail access pathway.
3. There is concern over the release of water from Utah Lake at compromise level. Many expressed concern that the release should be more gradual than currently is.
4. The project schedule was reviewed and will be adjusted to allow more time for development of a preliminary draft plan. Landmark Design will revisit the schedule, and will be sending out a revised schedule as soon as possible. For now, everything on the schedule will be shifted down a slot so that the preliminary draft will be submitted to staff on May 7.
5. Some issues from last meeting were discussed and clarified. Regarding item 2 from Meeting 1 Notes about trails being under water – There may be a possibility to tie in with the sewer district's plans to connect directly into Bangerter Highway (if this is the direction they decide to go in ultimately).
6. Regarding item 6 from Meeting 1 Notes about the Midvale Project – This was a discussion about the Bingham Junction/Midvale Slag area, and how it would be nice to take the opportunity at the time of development/redevelopment in areas along the trail to have appropriate trail facilities and trail access accommodated as part of that growth. A related issue is trail access along both sides of roadways passing through the trail corridor, such as along 7000 South. Access along this roadway is provided only on the south

side, making it dangerous for cyclists or others wanting to access the trail safely if they are traveling in the westbound direction. The master plan should advocate trail access as part of all new developments within the river corridor. The new gated development along the north side of 7000 South excludes trail access for the general public.

7. Regarding item 11 from Meeting 1 Notes about the Rose Creek Drainage – It was brought up in discussion that Rose Creek would be a good east/west trail connection opportunity. The master plan should investigate other possible east/west trail connections and access points.
8. Regarding item 14 from Meeting 1 Notes about presentations to the City Councils – This was a discussion about how to get support for the Jordan River Parkway Trail Master Plan by the cities in the river corridor. It was suggested that presentations should be made to each of the City Councils as the process unfolds, to help improve communication and garner support. Another possible option is to present to the County Council of Governments (COG). This may be a plan that the Wasatch Front Regional Council (WFRC) will want to integrate into their planning documents. As an example, the meander corridor was established years ago by the County, but the Cities really were not involved in the process and as a result, have not always followed the recommendations by the County regarding development within the river corridor. This plan can be a wonderful planning tool if it is done right.
9. The project web site will be set up as soon as possible, and the existing conditions mapping will be posted. Links to page will be placed on SL County Parks and Recreation web site, as well as the Salt Lake City Bicycle Advisory Committee web site.
10. Jeff Salt of the Jordan River Parkway Foundation and Great Salt Lakekeeper presented findings of a study and data collection effort conducted in 2001 with a grant from the National Park Service in partnership with Salt Lake County Parks and Recreation Division; SWCA, Inc.; and Monson Engineering, Inc.

Project supported by Utah Division of Forestry, Fire, and State Lands; Utah Division of Parks and Recreation; Salt Lake County Engineering Department; Foundation for the Provo-Jordan River Parkway; Utah Whitewater Association; and SPLORE.

Presentation covered:

- Jordan River facts
- Legal Support of access for the protection of navigation, fish and wildlife habitat, public recreation, and water quality
- Need for Study: River is largely unsafe for recreational boating; hazards create liability for responsible parties; general public is not aware of hazard locations; demand for recreational boating is high; public access is severely limited; existing boating facilities are underdeveloped; and no coordinated water trail plan exists.
- Project Goals: to promote “navigability” of the Jordan River; enhance public access; increase public boating opportunities; improve boating safety; support development of a coordinated water trail; and build a river conservation constituency.

- Desired Outcomes: maps of hazards and recreation points; marketing tools (CD ROM and multi-media shows); navigational hazards removal plan; recreational boating plan; boating guide book; and web site applications.
 - Study process
 - Hazards – Weir at 4600 South set as highest hazard for comparison of hazards along river, no warning signs, no barricades, unsafe portage, flood debris, re-circulating current, and 5 foot drop-off. Hazards discussed were fallen trees, irrigation dams and weirs, utility pipelines, flood debris, concrete debris, and fences.
 - Discussed success of concrete debris removal at 7800 South.
 - Study also performed “Recreational Boating Assessment” which looked at location and functionality of launches, portages, and potential launches and portages. Also discussed dangerous boating access facilities and ideal facility at 1700 South.
11. The Jordan River Foundation (JRF) and SPLORE are currently providing public recreation opportunities on the Jordan River. Currently it is unsafe, there is little access, and no signage or information available to the public.
 12. The Utah Division of Forestry, Fire, and State Lands is responsible for managing the bed and surface of the river, including the removal of debris.
 13. The hazards at the Jordan Narrows were discussed. Currently boaters are having to pull out their boats at the barrels before the dam and are walking along the high-speed rail line tracks to a point where they can put in again. Is there any way we can negotiate an easement around the dam?
 14. There are a whole range of hazards - some vary with the flow of the river. Bluffdale, Draper, and Utah County have seasonal boating use because of low flow later in the year. The use can vary by section. Further north, there are year-round boating opportunities.
 15. Need to think about signage along the river to let users know where they are. This is an important safety issue as well for emergency response. The information acquired in the river study was incorporated by VEC (911 emergency responders).
 16. 1700 South has one of the best access ramps for boating. SPLORE is an organization which takes clients with special needs out on the Jordan River in canoes. This ramp allows plenty of room for staging the large groups (typically 20 to 30 people and 10 -12 canoes for JRF and SPLORE trips), as well as a calm, wide area in the river for users to practice newly-learned skills before setting off down the river. The ramp has a gentle slope, is durable (concrete) and wide. There is also a large parking area, which is also necessary to accommodate large boating groups, which is always safer than going out alone on the river. The same design is desirable for launch points and portages. Need to do some research and find out if there are national standards for boat access ramps. They should be ADA accessible, especially being on “sovereign lands” guaranteed by the Constitution.
 17. Future launch sites should take advantage of natural slow-moving

- lagoons in the river where possible, rather than making alterations.
18. Boating north of 2100 South is usually much more consistent and safe because the surplus water has been taken off at the Surplus canal in West Valley City.
 19. Need to remove redundant or non-functional boating access facilities, or change their uses to something more appropriate.
 20. Have two types of users in the Narrows area – those that have started their trip at Utah Lake and want to continue through the Narrows, and those that will be starting their trip in the Narrows. There are a lot of issues in this area with The JVVCD pump station, the railroad, and the canal companies.
 21. Maintenance is one of the most important issues that needs to be addressed. If good facilities are installed, whether water trail or multi-purpose trail, they must be maintained or the functionality is eliminated.
 22. Used to be a whitewater kayaking competition held at Winchester Park area each year, but the re-design has eliminated that use at that location. The whitewater community would like to see the possibility of this event returning, but it would be an expensive change to implement.
 23. Low bridges are also a hazard for equestrian users – need at least 12’ of clearance to get a horse under a bridge.
 24. Greg Cudworth stated that there is some demand for equestrian use in the Chesterfield area.
 25. Where possible, a separation of at least 5’ between equestrian and hard-surface trails is desirable. Visual and sound separation is the ideal situation to avoid horses being spooked by users of the hard-surface trails.
 26. Equestrian users are typically doing out and back trips, rather than using two vehicles/trailers to do a long one-way route. Maybe certain areas of the Parkway should be designated for equestrian use with loop trail systems rather than trying to make a continuous north-south equestrian route. Also need to balance trails, and avoid putting too many trails in sensitive areas. Some areas do not have the right-of-way width to allow unpaved equestrian trails and hard-surface trails. However, it is not safe or desirable to have equestrians using hard-surface trails.
 27. Balance ecology and nature with recreational needs.
 28. Need to do research and find an example of a desirable trailhead for equestrian use to help establish standards.
 29. Nature centers are also an important element that the plan should explore and address.
 30. Goat-heads cause flat tires for cyclists on the trail. One of the most problematic areas has been north of 3300 South, where vegetation has been disturbed through dredging and vegetation removal. Dell Cook mentioned that the trails are supposed to have 2’ wide shoulders. It may help reduce the goat-head encroachment on the trail if this shoulder is maintained.
 31. Separating equestrian and hard-surface trails with bars is undesirable from a maintenance and safety standpoint.
 32. Craig Thaxton of South Jordan mentioned Mary Draper as a good contact for equestrian issues, and will contact her about the project.

A.2 Verbatim Comments from General Public and Stakeholder Group Members

Comments Received Throughout the Planning Process

- Hello, I am sending along my comments for the masterplan for the parkway. I have been going down on several different sections for many years and it is truly a gem in this overpopulated city that we now live in. I would like to see more places along the parkway that are designed off leash. As you are aware the county is developing plans for more off leash areas and I think that there are several place along the river that could be designated off leash. The new park on 123 and the land to the south of that green grass (that I have never seen people on could work). The parkway is the perfect spot to get away form cars. Biking with my dogs is great but there is not legal area in which to ride. Other areas are closer to Thanksgiving point, Does this plan include Bluffdale. I know this is sensitive subject but we are users that also want a piece of the pie for our beloved companions. Thank you for taking the time to read this and consider these requests. Please feel free to contact me if there is anything that I can do to help. Are there any "dog people" on the stakeholder list?

- I am advocating construction of a dirt trail on land adjacent to the Jordan River Parkway just south of the 7100 South trail head in West Jordan. I presented my proposal to the West Jordan Open Lands & Parks Committee last night. The proposal was well received and was made an agenda item for their next meeting on June 14. In the interim a West Jordan staff planner is to analyze the proposal and make his recommendation to the committee on the 14th. This committee is authorized to make recommendations to the West Jordan City Council concerning trails.

Since this dirt trail would leverage facilities of the Jordan River Parkway i.e. the 7100 South parking lot, the trail head, the JRP trail itself for access to the trail head of the dirt trail as well as being an enhancement to the desirability of the JRP, it seems the dirt trail should be included in the JRP Master Plan which Landmark Design is currently working on. Since the trail has not yet been approved perhaps it should be mentioned as "a possible JRP enhancement". To not mention the trail in the Master Plan would seem to be contrary to the purpose of the Plan. I am attaching my draft proposal for the dirt trail as well as my PowerPoint presentation package for the trail.

- I've been working with Mont Evans who has been talking with Draper City. We'd like to plan for a Nature Center along the Parkway. Riverton has been talking about this for years, but with the sewer plant construction the possible locations have been eaten up. If it's the right time to talk about future Parkway Master Plan features, I'd like to propose that you add a Nature Center in the attached location.
- Sorry to be so tardy, but I have not had time to really ponder the JRP. I do know that people like loop trails (paved as well as dirt)

and they should be incorporated wherever possible. Equestrian/soft jogging walking trails should be built at least from Murray to Utah Lake. Remember that the trail head on the Arrow property was given State funding with the belief that equestrian use would be able to go from there in EITHER direction on the JRP.

- I think the County should be getting as many relevant parties involved as possible, this would include the duck/gun club. At least offering them a seat/say at this stage may save problems down the road, or at the very least we could say that we gave them an "opportunity" if they choose to not attend and would not be able to make the argument that they were left out of the loop and not given consideration.

Other groups which I could think of would be the 3 school districts (I constantly see busses by the outdoor nature amphitheatre in Murray), riparian/bird watching groups, running & rollerblade groups and possibly some of the rec. and senior centers which may border the pathway.

My big things are: getting it "completed" & enforce/protect right-of-ways and tied into Davis and Utah county, consistent signage, mileage markers which don't start over each city, and better on-trail informational signs at intersections and diverge points (rather than a single soon stolen map, which you have to memorize). And lastly, better design/location of curb cuts for cyclists when the trail deadens into a parking area (such as at the park just south of 5400 S).

- I have the following concerns: Location/Status/City:
 - Shield's Lane (100th S.)/90th South West of Jordan River/Needs equine trail/West Jordan
 - 114th South to Shield's Lane West of Jordan River/Needs equine trail/West Jordan
 - 114th South to 118th South (Riverton City boundary) West of Jordan River/Need access, equine trails/West Jordan
 - Tithing Hill Bridge to 118th South West of Jordan River/Equine trail needs to be honored, identified from bridge to 118th South (currently pump house encroaches on trail)/Riverton City
 - Equine Parking Lot at 123rd South trailhead East of Jordan River/Needs to be identified, fenced Equine trail from the parking lot along 123rd south fence line to Jordan River bridge needs to be identified, marked, mowed/ Draper
 - Equine trail from Equine Parking Lot at 123rd South trailhead North to 118th South East of Jordan River/Equine trail needs to be separated 20 feet from the asphalt trail, identified with signage, mowed and/or barked to 118th South/Draper
 - Temporary parking lot South of 123rd South on East side of Jordan River/Needs to be closed/Riverton
 - Equine trail from 123rd South to Galena Canal on East side

- of Jordan River/Needs signage and maintenance/Draper
- Galena Canal equine trail/Needs to be preserved with new subdivision plan/Draper
- Bangerter Hwy to 146th South West of Jordan River/Equine trail needs to be identified with signage/Bluffdale
- Arrowhead County Parking Lot West of Jordan River/Needs completion and access to existing trails/Bluffdale
- 146th South to Narrows canal washout/equine trails needed/Bluffdale
- Canal washout to Narrows (180th S.)/Access, asphalt and equine trails needed/Bluffdale

I guess we need to find out what the "real" status is suppose to be around the golf course. I was operating under the assumption that the golf course was to be providing or allowing access to the parkway trail. As it currently stands, I see the following issues with this section:

- The golf course has either left up or placed no-trespassing signs in areas blocking about 200' of access to the paved section of the trail which currently dead ends on the north to the golf course and 2 undefined & maintained easements.
- There are new Sandy trailhead signs at the north end (200' from 9000s past no trespassing signs, why is this here if you can't legally get there?), and another at the "trail end" sign just before the "trail" crosses the bridge to terminate into the subdivision? This looks to be where some construction is being started for the trail to the next TH? And another at a new TH? Parking lot inside the clubhouse parking lot.
- There is a UDOT no trespass sign on the East of the river, do they own this land? On the West of the river where the trail is indicated on the map, there is a setback of a chain fence, but another no trespass sign and no clear TH. Sandy City shows this as a TH to the parkway on their trail maps.
- If the golf course was to provide access for this section, then they need to remove the offending no trespassing signs, and place signage directing users to the proper TH's.
- The "no parking for sale" signs are placed on the east side of the river at 9000s, 154-3 is not a format for any Ordinances in the county and may be an illegally placed sign. The correct syntax is: Sandy City Traffic Code Chapter 8 §154-3.
- The bridges seem to be of the same design as others placed by the County elsewhere on the parkway. Were these put up with taxpayer dollars or under a P&Z or easement agreement by the golf course, if so why are there no trespassing signs blocking access?
- The TH in the golf course parking lot has no public access (you have to traverse the private parking lot to get there, there is no signage identifying it other than a Sandy TH sign at the end of the parking lot), no signage on the roadway giving permission to access it, and no ADA access

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directly from the parking lot (you have to wheel out the parking lot down the driveway to access the trail.)

- The sign at the S end of the trail makes no logical sense either. The "end of trail" sign seems premature, it s/b at some point after the bridge on the West, also the TH sign is placed right next to it, it should be facing the other way if this is the "beginning" from that end.
- When I purchased a home at the Villas in Hidden Creek in western Sandy in April of 2004 I was told that the Jordan River trail extended all the way north to downtown Salt Lake City and south to 115th South, except for a very short section near the golf course at 90th. I was told that this trail would be a good reason to purchase especially since I enjoy walking and biking. The short unfinished section would be completed within a year they told me. Also the trail would continue beyond 115th South in the near future. It now goes all the way to 126th South except for the sections I mention below.

We have now been in our home two years and eight months. The unfinished portions of the trail have still not been finish and most of it has not even been started. That leaves us at a gap in the trail such that we must negotiate over a mile of roadway, some without even sidewalk, in order to use either the north or south sections of the trail.

I took an approximate measurement of the entire trail from the Temple to 126th South and found it to be 17 miles as the crow flies. (probably 20 miles via the trail) I also measured the unfinished portions of that trail to be less than 2.2 miles. (about 13%) See Attached map. Green in completed trail. Red, Blue, and yellow are unfinished portions of the trail.

I have discovered that those unfinished trails are controlled by four jurisdictions. (Salt Lake County, City of Sandy, City of West Jordan, and City of South Jordan.) They control approximately the following amounts of trail. None of the unfinished trails involve developed property so can easily be built at this time. Salt Lake County 1.0 miles. City of Sandy 0.4 miles. City of West Jordan 0.4 miles. City of South Jordan 0.4 miles.

I am suggesting that it is NOW time to get these trails done so that it can be done at a minimal cost. What can you do to make it happen?

- From open question email to MBAC/UBC lists:
Priorities/Comments:
 - Trail Conditions x3: 2100 S to 3300 S, broken up; 3900 S, vegetation cleanup/bridge, trail very dirty still; 7800 S Bridge, very dangerous for rollerblades, and issue for strollers; 2100 S Railroad Gate, who is responsible, it seems closed most of the time now; Lights between 2100 and I-80, some on, some off, loose wires; Roots, the 3300

S has been this way for years with no apparent attempt at fixing.

- Legacy Parkway/Jordan Trail Connections
- Goatheads x4: Mainly between 3900 S and 2100 S; Either eradication, maintenance or ?.
- Signage x2: Trail Markers; Trax signs (on parkway)
- TH directional signs on majors roads; continuation/Intersection Signs for trail users (so they don't dead end into a subdivision).
- Existing "map" x3
- Missing Sections x3: North County areas; I-80 area; Golf Course at 90th S; Sandy/S Jordan ½ mile section; South County.
- My wife and I ride the Jordan River Trail at least every other day and have been doing so for the past two months. Our typical ride is 700 South to 5400 South, occasionally longer.

In the past two weeks alone, I have phoned the police for : people lighting fires, people smoking crack in a park, people shooting toward the middle area of the loop in West Valley, people riding ATV's very fast on the trail, kids spray-painting at the SR201 underpass and the 3300 South underpass, and people spinning 'donuts' in gravel along the trail. This does not count the numerous possible prostitutes along the trail from 2100 South to 3300 South. I am glad to do so, but each time, the operator wants to know what south I am at and I have no idea being on a trail.

As part of the new Master Plan, may I suggest the following as a regular rider?

- Tighten access to motorized vehicles. We have seen many vehicle tracks and ATV's on the trail. At 1700 South there is a long area where cars can strike a rider and gravel all over the trail from vehicle usage.
- Marker posts. These should give a point that can be relayed in an emergency or when reporting crime. As part of this system, trail pointers or directions would really help. There are often trails that are equally large leading off to nowhere and no way of knowing where the through trail is. It ends with no notice of how to proceed at 8200 South or so. Maybe mark watering points?
- Random 2-3 times per week minimum patrol by municipality with jurisdiction. Better, regular police presence could really help keep it safer and cleaner.
- Weed control. The Salt Lake City stretch is in every bike guide for it's terrible thorns and it takes Kevlar tires with self-healing tubes to not be left with a flat on every ride...without leaving the path!

I would suggest funding for these items be priority as they would greatly improve the safety of every user of the trail, as well as help preserve the conservation areas by removing homeless camps

and curbing graffiti. I have spoken with Salt Lake City and South Salt Lake police departments, and both agree these ideas will help protect the public and are worth the effort to consider them.

I appreciate your time.

- I'm not sure who's jurisdiction these fall into, but here goes:

Just South of the parkway bridge near 3100 S the pathway seems to be taking on damage by erosion again. This section use to have a "trail closed" sign hanging there, but that is gone now. There was a bypass placed at some point, but if we put signage up again, it would be nice to either remove this section close to the river or put the signs in a more visible manner so they can be seen at dawn/dusk and at the start/end of the bypass.

Also, at 7800 S the trail is currently under water. The natural path to take as you are already on the W side of the river traveling N is the path to the street which has a 8' locked fence which lines up closely to where the path on the other side comes out in the farm. So backtracking back across the bridge to the parking lot you are faced with not only crossing the busy street, but now traveling across the bridge with a very narrow and dirty shoulder only to have to basically dismount in traffic to get through the opening left by the sign in the photo. Our road bikes were barely able to make this by lifting the bike on the back wheel and pivoting though the narrow opening. The sign could be better placed. I can't imagine much of anything getting through that hole even without the sign there, and there are other options for placement of the sign.

- I have just been again reviewing the on-line info on the JRT. I have just been again reviewing the on-line info on the JRT. You indicate there will be 2 open house meetings in the late summer. Have you scheduled them yet?

I used to live in S. Jordan in the Parkway Palisades subdivision and was briefly on the committee and then left for a mission. I was hoping that when I returned 2 years later the project would be completed. I now live in W. Jordan just above R. Oaks Golf Course and feeling quite disappointed in the lack of access and trail here. My wife and I walk regularly and I run and bike. I have been home for a year and still no change. I will be leaving town 10 Sep for a month and will probably miss the meetings. I wanted to give my input and get an update. Could you please follow up with an e-mail. The following are my areas of concern/interest.

- When will the full length going south be completed?
- SL County owns the land by the power station on 90th across from R. Oaks according to the map. We need a connector in there. The road between the corner of 90th and the trail head to the north is dangerous.
- The map shows the trail going part way through the R. Oaks Golf Course. I have not been able to find signage indicating that. They chase me off there when I go running

- or biking in there where the map indicates.
- The map indicates the trail ends on the R. Oaks GC at the bridge and the corner of a new sub-division going in. That sub-division has put a fence up cutting all access off from the homes above there. There are 2 more new subdivisions now going in above the new one adjacent to the course. Is there a way to have access to the trail (future completion of the trail)? I notice on the map West Jordan City does not have ANY section completed!!!
- There has been work going on right where the trail ends in the Golf Course and then across the river with a large pipe going in. Are you looking to put a bridge across there and continue to follow the work to connect with the trail just under the bridge at Shields Lane? If not, you could extend the trail on the same side of the canal there, put a bridge in or just open the chain link fence allowing crossing there and then continuing along the edge of the golf course, past the tees, one fairway, one more set of tees and then through the tamarisk and Russian Olives going out of the end of the course and then put in a bridge just before Shields lane. What is the plan and when? We need a trail and we need access in that stretch. West Jordan is not doing a good job of widening 1300 W just south of 90th to the S. Jordan boundary it is dangerous riding, running, biking, or walking to get out of the neighborhood in Trimble Creek area and that is where the 3 new sub-divisions are going in. The neighborhoods to the West of 1300 W need access also. This is my highest concern.
- The area from the park/trailhead on Shields lane going South needs bags and a few more trash bins for dog droppings. There are ample places in West Valley and elsewhere north. going South from Shields there are not many trailheads with bags and signage and trash bins.
- When is S. Jordan going to declare right of way to Peter Coates property and finish the trail through that section? Or is there another plan?
- It was a pleasant surprise to come home and run and bike the section south of Bangerter as far as it goes. What is the plan for finishing the section down to the Jordan Narrows and when?
- When I run or bike north (I usually go to about 23rd S and sometimes further) and there are LOTS of people using the trail. There is a definite tapering off of use from Gardener Village going south. There are a lot of us who are really anxious to get it finished so we can go from anyplace north to the airport or south to Utah Lake. I look forward to the day I can start out at Willow Creek Park in Lehi and run home without having to take back routes, jump fences, and bushwhack or get up on the roads with traffic. When will that be?
- What plans are there for connector trails from the east or west to the JRT? It would be easy to make a connector with Dimple Dell Canyon via Dry Creek and Shields

Lane. But, that is another project. Thanks for the patience on the long e-mail and I look forward to your answers. I am retired and have plenty of time if you need someone on your board/committee.

- Thank you for the opportunity to see the plans you have for the Jordan River and trail in the Midvale/West Jordan corridor. You may want to add meander columns on both sets of maps.
- I noticed in Section 12 that the trail system doesn't mention the east west developed trail going from near B32 to near I-215; perhaps this is the equestrian trail mentioned in "Issues and Opportunities". This has been a corridor for reaching the 2200 West bike route via the 2200 North I-215 overpass. Hopefully, this section will not be "abandoned" as this draft ignores its ample existence and intimates that the impending sports complex may do so.

This raised section of trail serves to protect residential areas from the threat of floodwaters and potential raising of the Great Salt Lake. Paving it would enhance its structure though not accommodate equestrians; perhaps a dual trail as I was told will pursue the east side of the river would serve here also.

Mostly I am concerned the sports complex will disregard our safety in adjacent subdivisions in their zeal to over construct and perhaps remove this levee.

- Having attended the meeting last Thurs evening, I have the following comments;
 - The completed graphic maps would be a useful resource for the public who wish to use the trail / waterway system. The completed information should be made available in a downloadable PDF in 8 ½ x 11 format.
 - It is important that not one more acre be given up to built or hard-surface development along the river course. Every effort should be made to acquire any remaining open land for public ownership.
 - The provision of recycle and garbage receptacles needs to be extended along all sections of the trail that currently are not serviced
- PLEASE continue to develop the Jordan River Trail. It is a huge asset to the community and needs to be expanded to cover the entire length of the Salt Lake Valley. Thanks for listening to my feedback.
- Thank you for the opportunity to review the preliminary draft of the Jordan River Corridor Trail. I think it is a wonderful document, very thorough and informative. I appreciate the way the plan is

organized, with research, assessment and recommendations for each type of proposed trail use presented in a standard format. I also appreciate the information that I found in the Implementation Plan.

As a member of the Parley's Trail Coalition, I was particularly interested in references to the intersections between Parley's Trail and the Jordan River Parkway Trail. I found those on the map on page 3-42. Most importantly, the Plan includes, as "Vision Statement Three," a recommendation to "CONNECT TO OTHER TRAIL FACILITIES AND MAXIMIZE SHARED USE WITH OTHER FACILITIES." Like the Jordan River Plan, the Parley's Trail Plan calls for trailheads at Workman Park and at 1700 South, and I agree that trail planners should design those access points so they perform efficiently for users of both trail corridors.

I also want to mention that I really like the recommendations for trail location signs. In that regard, I hope that the Jordan River Trail signage will provide a model for all trails in Salt Lake City.

- Need regular removal of branches/debris/garbage from river, regular removal of graffiti, marker posts to indicate actual trail path vs. side trails/nature loops, marker posts to indicate location along trail to help emergency personnel identify where a person is if calling for help, 'Goathead' thorn management, and signage indicating nature and length of closures and alternate routes
- Possible off-leash dog parks
 - Bordered by Hayes Ave. 1000 W. and Jordan River
 - North of Glendale Park east of Jordan River, west of 1100 W, south of 1400 S.
- Look at
 - Kayak Park and play spots
2100 So. – Surplus Diversion
Winchester – 6400 So.
Work with SLC and Rocky Mountain Power to remove dam at Gatsby Power Plant.
- At the sewer pipe 4.5 C just north of 9000 South it will be an easy interim resolution to put up a warning sign and direct boats to a portage on the right side (river right).

At 7000 South I believe there is a pipe causing a pour over. When we went through there this summer there was about a 1 foot drop, under the bridge on the upstream side.

At Winchester Street there is easy access from the river bank downstream. Of higher priority is an easier takeout upstream. The walking trail provides good portage.

Near 4500 S there needs to be a launch access. One alternative would be from Big Cottonwood Creek from the Millcreek Township property. Another alternative would be from Murray's property just

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north of the diversion dam which makes a nice run to the 2320 Park.

The canoe chute at the Power Company is frequently clogged with logs and trash. Should either be redesigned or better maintained. When clogged it is dangerous and backs the water up into city storm water drains.

Maintenance should include keeping low hanging branches out of the water so they don't become trash traps.

I applaud the design of concrete ramps for launch and takeout sites.

Water quality issues need to be addressed and setbacks for development must be enforced.

- Our police department [South Salt Lake] has informed me that there is an ongoing problem with prostitution and drug dealing at the Oxbow Trailhead. They have planned several stings and other operations to address the problem. Are there design elements that can be done to open up the trail area to discourage these activities? There is a lot of vegetation in the area that creates cover which encourages these activities. Around the Oxbow Jail the trail is more open and these problems are not as frequent.

South Salt Lake would like to develop a trail along Mill Creek through our city (from about 700 East to the Jordan River Trail). Is this something you would like to mention in your master plan or is this outside the scope? Right now the project is only barely in the discussion stage.

- Thanks so much for the hard work!

Please look at areas for off leash dog parks along the corridor. We need places to go with our companions.

The water trail is wonderful. It will be great to have more boating in the area.

Preserving open space should be a main goal, leaving as much natural habitat as possible. Walking along a trail in open space is much different than walking in a subdivision.

Signage is important.

Waste station (doggie bags) along the trail is important.

Maybe impose a user fee to maintain the trails.

- Unfortunately, I cannot attend your Open House. As well as congratulating you on an impressive draft report, may I please offer some general comments?

1. The public is disturbed by the media reports of crime and aberrant sexual activity along the existing parkway. No doubt there will be pressures to develop the trail as a "vegetation-free" zone which is easier to manage and supervise. But many studies (well summarized in "Last Child in the Woods" show that children need wild areas. The best solution is probably fairly intensive police patrolling (on bicycles) - which implies a robust, multi-jurisdictional maintenance budget (as a possible model, or at least a precedent, think of SLC Public Utilities funding Sheriffs to help protect the Cottonwood Canyons watersheds).

2. Trash control is central to an enjoyable trail system. In summer I volunteer at Silver Lake at the head of Big Cottonwood Canyon. Three years ago, when I started, I could more or less guarantee to fill a large trash bag several times a day. Since then, we have tried to ensure that all through the weekends one of us (Forest Ranger, Watershed Officer, or volunteer) is always on patrol around the lake, picking up trash and talking to people. Result: trash has almost ceased to be a problem. This replicates findings in other places across the country. So you will need a robust (well-funded, well-staffed) program to deal with this issue.

3. Signs are of course very important to bring in the public and to help encourage proper behavior. I have three suggestions:

* Use graphics wherever possible. Decades ago Europe, faced with multi-lingual drivers, phased out almost all writing on road signs in favor of simple, easily-interpreted graphics. For example, if I am driving around looking for a place to launch a boat, I want to be able to look out for just two self-explanatory and consistent graphics: one indicating the direction to the Parkway, and the other signifying a launching place. And these need to be before the road junction, not at it (far too late in traffic)!

* Be positive. The list of restrictions on SLC's watersheds is long, and entirely justified - but the prohibition notices are far more effective if they explain that this water will be in your faucet within 24 hours, and so polluting the source is a bad idea. Purely negative signs tend, I think, to be more frequent victims of tagging.

* Provide good maps. At intervals along the Parkway, an overall map plus a larger-scale one of the vicinity. On a website, easily-downloadable (PDF?) copies of all maps (plus, of course, other educational materials such as bird lists; references to other resources such as Bend in the River; etc.)

4. Preserve or even create wetlands. There is a tendency for all construction, however careful, to damage wetlands, and then "restore" them with a few pushes of a bulldozer. Where the trail crosses wetlands, it needs to be on a boardwalk (again, see Silver Lake: phenomenal renewal since the boardwalk was installed). Where damage is inevitable, it should be minimized. Where restoration has to be done, make sure that appropriate plants are used and that a follow-up mechanism (with a committed budget) is available to make sure that invasives don't take over.

5. And finally, a high priority has to be to get the Parkway completed from 200S to North Temple. If this were done, the number of visitors would undoubtedly increase significantly. Hopefully, this could be linked to the eventual "day lighting" of City Creek, to restore more of our original system.

Please excuse me writing at such length, but I hope that these comments are useful to you.

- Your firm appears to be the main consultants for the Jordan River Urban Water Trail. It is with this in mind that I would like to send you an extract of an email I recently sent to Salt Lake County.
 - Having been to competitions in Austin, TX and Oklahoma, Ok recently our dreams have been ignited and so we are looking to see what we can do on our river.
 - We would like to present our case or hopes for what our organizations would like to see somewhere on the Jordan River to the most appropriate person or persons. If this is not your area of concern could you please either let me know who you think might be the best person to send this email to or forward it to them if you can.
 - Some of the sections mentioned above do not come under this plan as they are part of the Surplus Canal but the 2320 S section does.
 - As you may have surmised our main interest is rowing but the main focus of the plan is for recreational canoeing which can tolerate a greater number of hazards than a Rowing eight. As you have studied the river perhaps not to with a view to rowing, I would like your thoughts about a stretch of the river between sections 4 & 5 from waypoint 6.16 through 7.4 or section 6 between the Midvale City Boundary north and south where the river is straight enough to be able to row on. I realize that to make either of these stretches suitable for rowing might be beyond the scope and cost of this project and may require more environmental study as the river would need to be widened, deepened and a number of these hazards removed completely, but we live in hope.
 - Racing Shells would require I guess a minimum of one meter of water depth and ideally 20 plus meters of width to enable crews to row side by side and turn, with a length of 2000 meters and more to train on. Also some sort of boat storage or preferably space for a boat house would also be needed. I am hoping that we will be able to bring a number of our members to the last open house on the 6th December where we may be able to discuss what we would love to see on the river.

- I read the article on the trail system in today's SL Tribune. Unfortunately I will not be able to attend the last open house due to a work conflict, but I do want to provide some feedback on the pathway.

First, some "qualifications." My wife and I are what most people would consider as "serious runners." We've run several marathons, 1/2 marathons, etc. We participate in triathlons. We also sponsor an annual 5K run (utahhumanrace.org) to benefit the Utah Food Bank and LDS Humanitarian Services and had 1500 participants this year.

As folks who run, we can tell you that all the running gurus advocate that runners seek non-paved routes so as to minimize the impact to their bodies. Dirt type (decomposed granite?) trails usually are the optimal surface. We recently went for a run along the American River in Sacramento and thoroughly enjoyed the beauty of the area and the trail itself. As you can see in this link with rolling photos (<http://www.arpf.org/>) the trail itself has decomposed granite for approx. 24" on each side of the asphalt. All walkers/runners are advised to use the non-asphalt trail so bikers can ride the main strip (max speed 15 mph). This is good for both bikers and runners!

I can tell you that we run on the Draper-based Porter Rockwell trail all the time. We love it. But I would never be able to ride my bike on it due to the numberless puncture weeds that actually creep onto the asphalt from the trail edges. I've seen folks trying to cut the puncture weeds out, but it's a monumental task and gets worse every year. A decomposed granite type of trail, especially with runners using it, would not tolerate weeds (especially if the trail were sprayed) and would keep the path bike friendly.

- Thanks for talking with me just now regarding the Jordan River Master Trail plan. I am part of a group of coaches and rowers trying to bring rowing into Salt Lake City, and am hoping to exchange information with those who are working on developing the community-based recreational uses of the waterways within Salt Lake City. I look forward to seeing you on Thursday, and would love to answer any questions your team might have about the needs of rowers and the sport of rowing in general. I've enclosed a brief collection of slides (Powerpoint) to act as an introduction to the sport, and would love to speak to the possibilities of rowing on the Jordan River and associated waterways at the meeting on Thursday. In addition, I will offer a free rowing instructional session to those members of your firm and the stakeholders of the project when the next season emerges.
- I was wondering if anyone had considered creating off leash sections of the JRP. Have you had many requests for off-leash sections?

I would like to encourage this because every time I am on the parkway (which is virtually every day) I see people with dogs. I have a dog and I also like to get exercise. I think we need long sections of the parkway designed so that dogs can run alongside their owners as they walk, jog, bike or roller blade. The fenced in circle at Midas Park is nice, but it is the only off leash area on the parkway. And Folks and dogs need long stretches as well as round circles.

There are several sections of the parkway that could be fenced off where people could take their dogs off leash and the people who don't want to be in an off leash area would not be bothered.

I have taken my dog on the parkway literally 1,000's of times in the winter. More than half of the people who use the parkway in the winter have dogs with them. This has been the case nearly every time I have taken my dog for a walk in the winter. I am not stretching the truth here. And I have walked every section of the trail in the many years I have been using it.

I understand the safety issues. Being a woman, I am very concerned with safety. There are off-leash parks elsewhere in the valley, and the problems are few.

You might say that it would be cost prohibitive, however a long stretch would probably require less upkeep than an enclosed circle or a developed section. It would not require grass to water. Just a fenced off path, some shade trees, benches and drinking fountains for the humans and their dogs would be nice. Some inexpensive exercise equipment along the way to do sit-ups, pull-ups, etc would be nice also. And of course doggie bags and garbage cans.

But any kind of off-leash consideration would be appreciated.

- For some this trail is peaceful weekend walk or bike ride, and all I know who use it for such love it. For others it is the only safe route for a bicycle to go north and south. This trail is very important, and I look forward to its completion. We have people in our office waiting and as soon as it is done they will ride it to work. The south end where it stops around 8600 South is needed, a way to get around that golf course.

Also I have been asked by friends to comment about those dangerous weeds in the Salt Lake City section, they are razor sharp, some call them goat heads, I don't know what they are.

- I really enjoy the Parkway Trail, both for walking and biking. I'm delighted to see the progress north of 12600 south, looks like it only lacks a bridge to complete the connection.

However, the ongoing problem of goatheads needs to be addressed. It's great to have the trail, but not if it's unusable by

bike, which is the situation at present, especially north of 2100 south. The goatheads were finally sprayed, but only after they had overgrown the edges of the pavement, and then they were just left there to dry out and harden up. Later the mower came through and scattered the ones on the side of the trail all across the trail. A friend and I had 5 flats between 2100 south and 200 south; we won't be riding that section again soon. What would happen to a toddler who fell onto one of those vines?

A commitment should be made to spray the puncture vines early enough in the year that they haven't formed the thorns yet, with follow up spraying as needed. If they are not sprayed before the burrs form, they should be physically picked up and placed in plastic bags and taken to the landfill. A few years of conscientious control of this evil weed would all but eliminate the problem.

I'm eagerly looking forward to seeing the trail connected from Utah Lake clear to Salt Lake. It's a great asset to the quality of life in this valley.

- I was hoping to make the meeting tonight. But I won't be able to make it, so I wanted to provide just a few brief comments and ask some questions.

I took less than twenty minutes to review the draft that you have on the website. So no doubt you may have already addressed the questions I have, but I did not see them easily and wanted to ask. Comments:

1. I am impressed with the amount of detail that has gone into this plan. Thanks for all the good work, including a river trail and the pathway.
2. I just scanned it, so maybe there are some things that could stand correction, but I did not quickly see any.

Questions: I am wondering a few things that you may or may not have addressed:

1. Does the document discuss the role of the Jordan River as a sovereign land and show where the Division of Forestry, Fire and State Lands considers to have ownership of the River?
2. Does the document fully discuss the trail in Utah County that is completed and the soon to be completed trail in Davis County? These two components make the completion of the trail in Salt Lake County ever more important and viable?
3. One of the main reasons I wanted to go tonight is to talk about the possible trail going along the Surplus Canal and connecting to the trail by the Golf Course at the Salt Lake Airport. It seems to me that this would be a very viable and interesting extension. Has this been considered?

I think it would be wise to promote the completed success of the Big Idea in Utah County and to highlight how it is coming to fruition in Davis County. To me it is a way of saying that it is almost done, we just need Salt Lake County to finally to finish its part. (Note: It would be good to verify the Davis County part, my thinking on this part as that the trail will essentially be finished in Davis County when the Legacy Parkway is completed. But I have not looked at the details on this for a long time.)

I have not talked to others about this for a long time. I do know if it is feasible. But I do know the trail on the Jordan River by Surplus is essentially completed. And there is the trail that goes through the golf course at the Airport, which I rode on this Fall. It is a neat trail. I would love to see these a connection between these two areas fully connected. Also, there is a story here. The Surplus Canal is in my mind more like the River and at what time the area by where the Surplus Canal goes probably was the River. It would be a neat story to highlight along with a trail.

Thanks for considering these comments.

- I have been a user of the Jordan River Trail for many years as both a pedestrian and cyclist. It is a natural gem in this rapidly developing valley. Its main assets in my opinion are its multi-use nature, its proximity to many valley residents, the natural corridor that it preserves for wildlife and humans to enjoy, the fact that it is a continuous path for many miles, and street crossings that minimize interaction with automobile traffic by passing either over or under the street.

Many cities to the north of Sandy have done a wonderful job of developing the trail. The Sandy/West Jordan section has always been a disappointment to me, and I'm glad to see that a plan is finally in place to complete the sections of trail necessary to continue the trail south from ~8600 South where it currently dead ends at 700 West. Ideally, the trail would run along the river as it heads south from ~8600 South. I understand from attending one of the open houses that some fur farmers with private land in this area are preventing such an alignment. However, land ownership does change, and I would like to see such a trail remain on the map as part of the long term master plan. As such, I would like the master plan to include both the recommended and alternative alignments from ~8600 South to 9000 South with safe crossings of 9000 South (either underpasses or bridges) for both alignments. The recommended alignment can be built sooner and provide continuous access through area, while the alternative alignment will connect sections of the trail that have already been built on the east side of the river south of 9000 South to the trail from the north.

I am concerned that the alignment proposed in the documents I downloaded from your website for the 8600 South to 9000 South

section do not coincide with a notice on the trail alignment that was sent to me by Sandy City. This map shows the trail alignment along 700 West just north of 9000 South. The trail then crosses 9000 South to join up with the currently developed trail through Sandy on the east side of the river. For the record, I think a 700 West alignment for the "trail" is a bad idea. Not only is a surface street a bad choice from the standpoint of families using the trail (I specifically take my kids to the Jordan River Trail to keep them out of traffic while bike riding), but it ruins the concept of the trail as a buffer between development and the river. I don't want to breathe car exhaust and look at development as I ride through this section of trail. I have those things everywhere else I ride in the valley. I hope that you can bring Sandy City on board so with your master plan. I would be happy to fax you the alignment information sent to me by Sandy City; just let me know.

Another issue of concern for me is the encroaching development along the river corridor. Development has encouraged the channeling of the river, which in turn creates a deeper river channel and drops the water table. I hope this master plan provides communities with the information they need to see the Jordan River corridor as an asset to their communities rather than something to be moved into the smallest possible space. I hope some of the natural meanders of the river can be restored both to keep the water table high and to serve as flood control.

I support the concept of canoe routes. We like to canoe in my family, but there is a dearth of navigable water in this area. The Jordan River would be a wonderful option if it were a little more accessible.

One final issue I want to raise is that of non-native vegetation and weed control. I think budgets for trail building should include weed-control/invasive plant funding as well. There are sections of the trail with fields of goatheads on either side of the trail. Non-native trash trees such as Russian olive and tamarisk dominate the riverbanks in many sections. As a trail user, I would like to see these weeds replaced by willows and cottonwoods, grasses and shrubs. Weed control will create a more productive environment for wildlife and humans.

Thank you for considering my comments.

- I am writing to comment on the Jordan River Trail Master Plan. I have biked and walked on the trail for most of its existing length. I have seen that the trail is very popular and believe that use will only increase with time. I would like to make several comments.

Because families use the trail, having the trail run along a street is not a good idea. The trail should be physically separated from motorized vehicles. The route crossing 9000 South in Sandy should not be placed along the road as one of the options proposes.

Street crossings need to be safe if the trail is really going to be used. Sandy built a trail along the TRAX line several years ago. The trail is not of much value because there are no lights or other means of stopping traffic for trail users at the 9000 and 9400 South crossings. As a result, the short sections of trail are of little value and I have not seen many users on them.

In general the river corridor should be left undeveloped so the river can reclaim its flood zone and meander as it did before being straightened. Natural habitat for wildlife is becoming rare in the Salt Lake Valley and the long natural area along the Jordan River will be valuable for birds and other wildlife. As much land as possible should be set aside for open space to prevent further development along the river.

- I just completed a brief review of the "draft" of the master plan and noticed the reference to division rules governing state sovereign lands. We would like an additional portion of R652-70-100 quote in the master plan so that it reads as follows:

Administrative Rule R652-70-100: Sovereign Lands

"This rule provides for the management and classification of the surface of sovereign lands in Utah, which include but are not limited to the beds of Bear Lake, the Great Salt Lake, Utah Lake, the Jordan River... It also provides for the issuance of special use leases, general permits and easements on sovereign lands and the procedures and fees necessary to obtain these rights of use. This rule implements Article XX of the Utah Constitution, and Section 65A-10-1."

and that R652-2-200 be completely quoted as follows

Administrative Rule R652-2-200:

The state of Utah recognized and declares that the bed of navigable waters within the state are owned by the state and are among the basic resources of the state, and that there exists, and has existed since statehood, a public trust over and upon the beds of these waters. It is also recognized that the public health, interest, safety, and welfare require that all uses on, beneath or above the beds of navigable lakes and streams of the state be regulated, so that the protection of navigation, fish and wildlife habitat, aquatic beauty, public recreation, and water quality will be given due consideration and balanced against the navigational or economic necessity or justification for, or benefit to be derived from, any proposed use.

- I will not be able to attend next week's meeting. However, I have reviewed the master plan and have the following comments:

Have trail connections been identified from the parkway to the

potential commuter rail stations along the corridor? I know many of these stops will be outside of Draper, but I feel it would be something that we would want to address in this plan.

The plan currently states that we will continue to close all under-crossings when they flood in the spring. It seems that they get flooded every spring, even those years with low snow pack. I wonder whether the plan should recommend that an engineering solution should be explored in the future which may allow access during typical spring runoffs.

The plan states that the main Jordan River Parkway Trail should not be realigned in the area south of the silo. Some of the thinking behind the Galena Study, which showed some new trail alignments through the state land, was that the main trail could be realigned from the silo area south to the top of the hill (along an historic railroad bed), avoiding the steep portion of trail which is about a 10% slope for several hundred feet (refer to attached scan). Just something you may want to consider in the plan.

It would be good to show the Galena Community Park on the map (refer to attached scan). There has also been some discussion about providing paved access along the canal, adjacent to the equestrian trail, to the park and onto the sidewalk on 12300 South

Page 3-13 shows amenities at the Rotary Park. They can now add a playground to the list.

Because of the continued high use of the parkway in the winter months, a unisex stall will be heated and remain open year round in our new restrooms at Rotary Park. I wonder whether the plan should encourage other popular trail heads to have similar facilities for winter use.

Plan shows parking at the end of Alan Point Drive at 11800 South and should be deleted. The parking is not an official trail head parking, but rather an emergency turn around which may go away once the road continues north.

Note that the Jordan River Parkway Trail, between 11400 South and 11800 South has been paved. Also note that this has been a joint effort project between Draper City and Salt Lake County.

The typical trail cross sections are showing 9' clear zones beyond the trails (which seem quite excessive) and do not match the clear zones identified in the text.

The plan states that weed control, minor trail repair, and trash removal is the city's responsibility. As we understand, the County would be responsible for those items for the trail segments they maintain per the inter-local agreement, and the city would be responsible for the parks and trail head facilities along the parkway (such as the Rotary Park).

- Will the trail system provide equestrian uses? And If so, will it be limited to any specific areas? What if any are the restrictions for equine use?

Can I assume then that as long as the horses do not use the paved trail they can still venture further north or is there a formal restriction? That is what I meant to ask when I asked about restrictions.

I did look at the provisions for equestrian use and am very grateful for those. It seems that lately there are less and less places to ride and so I applaud your sensitivity and dedication ensuring some equine use.

If you need any assistance or recommendations in regards to that part of the design, let me know. I work for Planning and Development services and have some very good connections with the Utah Horse Council and so on.

- I'd like to underline the importance of finding a solution to the goathead thorn/puncture vine problem which Dan Fazzini addressed at yesterday's Jordan River Parkway Master Plan meeting. I place more importance on solving this problem than on 'filling in the gaps' in the Parkway. Ten rideable miles are worth a lot more than 20 unrideable miles.

Here is a link to the possible biological solution to the problem: www.goatheads.com.

- Give plan to all cities and make it available to the County Open Space Committee.
- Winds on the Great Salt Lake are problematic for rowing. Surplus Canal is better. Need 50' width of water to turn a 4 man boat around. Fifteen miles per hour is the minimum desirable rowing speed, with at least 3 KM of length.
- Concrete ramps are good for launches.
- Yes, providing more canoe accesses would be appreciated.
- Could a walkway be provided along the river in the Point of the Mountain area?
- Kayak play areas at 2100 South and 6400 South.
- I'm so glad you are doing this. I hope others will fall into line. Working toward native vegetation from exotics will take money and attention. Houston area has a biodiversity manual for the entire county. Could we also do this for Salt Lake County. What about crossing permits that promote trails and water trails?

- Consider using volunteers to remove Russian olives.
- Need a way to know when it is safe to kayak and canoe, ie measurement in the water or hotline where people can get info.
- Off-leash dog parks are suitable in select areas along the river.
- Put benches in the shade—they are wasted in the sun.
- Two foot grass/soft area adjacent to trails—not clear of vegetation.
- My only comment is actually a response to a comment in the Safety/Signage section that reads, "Vegetation removal is not the solutions for inappropriate/criminal behavior along the trail * regular patrol are more effective. Kids need wild areas." My response to the committee would be that, at least in West Valley City, we regularly patrol the trail both on bike and on foot and also regularly, with the help of the health department, remove the homeless camps. Unfortunately this has not been an effective means of keeping criminal behavior or homeless camps from our section of the trail. We have noticed, however, that sections of the trail that are better groomed, such as Murray, do not suffer from these problems. The concealment provided by overgrown vegetation affords the perfect atmosphere for these unwanted activities.
- In the Salt Lake City Bicycle & Pedestrian Master Plan a trail is proposed along the east side of the surplus canal from where it diverges from the Jordan River Trail near 2100 South to 500 South. Please show this as a "City Proposed Trail" on the map for Segment 9.

On page 3-43 the text states that: *"The Jordan River Trail continues across Fremont Drive on the east side of the river with a signalized Crosswalk..."* There is no signalized crosswalk at this location. However, we are in the process of installing pedestrian warning flashers at this location which should be completed in March 2008.

On your map for Segment 10 and on page 3-43 you are missing the main segment of the Jordan River Trail. Your text states: *"The Jordan River Trail continues across Fremont Drive on the east side of the river with a signalized crosswalk, and travels north along the west edge of Jordan Park and the International Peace Gardens. Neighbors can access the trail near Brooklyn Avenue using Bridge B21. The trail traces the west edge of the park and curves along its northern end. The trail then ends near Montague Avenue where a utility building and raised railroad tracks present an unsafe crossing for trail users. The trail picks up again north of the railroad tracks on 900 South."*

The Brooklyn Avenue bridge, besides being a trail access for the neighbors, is part of the main trail heading north (see attached jpg)

file). Heading north, the trail crosses the bridge B21 to the west and then continues north along the west side of the river and then along the east side 1100 West (as a separate path). Just to the north of the railroad tracks it heads east across the river on bridge B22. This section of trail was installed due to the unsafe rail crossing on the east side of the river just north of Montague Avenue.

This is also mentioned on page 3-46 under Issues and Opportunities:

"There is a gap in the paved trail near 900 South. The trail stops at the north end of Jordan Park/International Peace Gardens near a utility building and the railroad tracks make this portion of the trail inaccessible. There is no pedestrian rail barrier at this location."

There is not gap in the paved trail. The problem is lack of signage for properly directing trail users to stay on the main path.

On page 3-46:

3.10 B Paved trail Gap: 900 South Railroad Tracks

"There is a short gap in the trail as it crosses 900 South Street and the railroad. The railroad track along 900 South is being abandoned and will be removed in 2008. The abandoned right-of-way will become the property of Salt Lake City. The trail should be connected across 900 South to connect Jordan River trails in Jordan Park to Ninth South Park. A trailhead should be developed in this location, which would also serve as a trail corridor connection to a future 900 South trail heading east to Liberty Park developed by Salt Lake City. For the safety and enjoyment of watercraft users on the river, there should be no additional bridges developed in this section".

As mentioned above, there is no gap in the main trail. I agree with the rest of the text here.

We are in the process of installing pedestrian warning flashers at the 500 North crossing which should be completed in March 2008.

We are in the process of installing a signalized crossing at 1000 North which should be completed spring 2008.

For the PDF file that I downloaded from the County's website, on the map for Segment 11, the bridge locations are shown but they are not labeled (such as B29, etc.).

On page 3-16 Alan Point Drive is incorrectly spelled as Alan Pointe Drive.

Thanks for the opportunity to comment!

- Thanks for making sure that we were all able to see the various comments.

As an ecological restoration coordinator and environmental educator on the Jordan River in urban Salt Lake City, I noticed a few conflicting comments that will require addressing:

1. Puncture vine/Goats Head: This seems to be what everyone is talking about! This is a concern that requires addressing as one stakeholder pointed out that "ten ridable miles are worth a lot more than 20 unridable miles." Spraying and biocontrols (weevils) were suggested, but no one seemed address helping nature with taking the proper course. Spraying is a good, quick fix, but it can cause water quality and health issues. Perhaps it's fine for the short run. With the proper rehabilitation of the land, however, non-puncturing, native, drought-tolerant, low-growing plants can out compete the puncture vine. This is an additional, and logical, method to consider. It will take time, so it is something to add to the arsenal of solutions.

2. Off-leash areas: Many folks who want off-leash areas also want natural areas. Unfortunately, these do not go hand-in-hand. Dogs are not wild and not native, and they do not fit into our ecosystem as it has evolved. Encouraging greater dog use will increase the number of off-leash dogs along this important wildlife corridor. This causes stress to the native wildlife collecting along one of the only riparian corridors remaining above ground in the Salt Lake Valley. Since lowland riparian habitat is the single most important habitat type for birds in our region, this habitat must be preserved in as natural a state as is possible (for more information on this topic, see the Jordan River Natural Conservation Corridor Report, 2000).

In addition to concerns about habitat, off-leash areas on a bikeway also raise safety concerns. Mill Creek Canyon enforces a code in which dogs are allowed off-leash only on odd days when specific bike trails are closed for this very reason. Since, JRT is used as a commuter-way with bikers traveling at fast speeds, off-leash areas along the trail itself may not work here.

3. Safety: I hope that concerns raised about safety do not result in the cutting down of native cottonwoods and willows. These plants are important to the health of the entire riparian system and for streambank stability. Unlawful activity occurs in open areas along the Parkway as well. Increased patrols, perhaps some bicycle patrols, that cover the Parkway itself and not only the city streets that can be accessed by car, would do more than changes in vegetation to combat the many crime issues.

Thank you for taking the time to consider these comments,

- I am a resident of West Jordan and curious about the Jordan River Trail. I have heard complaints from several neighbors about the absence of the trail closest to our area (9000 South - it is beautiful north and south of us). So I have started looking into it and have been led to you. What can residents do? We all want it completed, but feel useless or helpless in doing anything. Even your website said "The project began in late August 2006 and is expected to be completed by the end of 2007." Well, it is not completed.

I look forward to hearing from you and hope you can offer some advice. I would like to know what we (residents) can do /now/, rather than hope that someday the trails will all be connected.

A.3 Detailed Paved Trail Recommendations

Feature Category	Map Code	Segment	Jurisdiction	Recommendation	Description	CIP Budget Implication	Responsibility	Other Notes
Paved Trail	3.1 A	1	Bluffdale	Repair mud-slide damage to paved trail segment.	0.5 mile paved trail	Funded	Salt Lake County/Bluffdale	Funded, to be completed 2008.
Trailhead	3.1 B	1	Bluffdale	Construct Phase II of the Jordan Narrows Trailhead to accommodate paved and equestrian trail users.	2 acre trailhead park with restroom, and paved and unpaved parking. Other desirable future elements equestrian users are hitching rails, access to fresh water, loading/unloading platforms, and mounting platforms.	Funded	Salt Lake County/Bluffdale	Phase II and III construction in 2008/2009; UDOT and ZAP funded. Phase I was completed by Salt Lake County.
Paved Trail	3.1 C	1	Bluffdale	Construct paved trail from future Jordan Narrows Trailhead to South end of Parry Farms Development	.75 miles paved trail	\$534,600	Salt Lake County/Bluffdale	
Paved Trail	3.1 D	1	Bluffdale	Construct paved trail from South end of Parry Farms Development to Future Bridge Crossing	Approximately 1.75 miles paved trail	\$415,800	Salt Lake County/Bluffdale	
Paved Trail	3.1 E	1	Bluffdale	If needed, develop an alternative paved trail alignment that parallels the Utah Lake Distribution Canal from the Sunrise Stables riding area to the Wood Hollow natural drainage.	Approximately 1.3 miles paved trail	\$308,880	Salt Lake County/Bluffdale	
Eq. Trail	3.1 F	1	Bluffdale	Construct equestrian trail from Jordan Narrows trailhead to South Jordan Canal and Jordan River crossings near Parry Farms Development.	Approximately 2.3 miles equestrian trail	\$121,440	Salt Lake County/Bluffdale	Negotiate easements with Utah and Salt Lake Canal Company, Telluride Power and Light, and Utah Power and Light.
Eq. Trail/Paved Trail	3.1 G	1	Bluffdale	If needed, develop an alternative paved trail alignment that parallels the Utah and Salt Lake Canal east of the Sunrise Riding Stables near the Jordan River north to the north end of the Parry Farms subdivision.	Approximately 1.7 miles equestrian trail/0.1 miles paved trail	\$113,520	Salt Lake County/Bluffdale	
Gate	3.1 H	1	Utah and Salt Lake Canal Company	Install a gate across the canal road to prevent trail users from traveling south on the canal road.	Gate	\$2,000	Salt Lake County/Bluffdale	
Bridge	3.1 I	1	Utah and Salt Lake Canal Company	Install an equestrian bridge over the Utah and Salt Lake Canal.	Bridge	\$150,000	Salt Lake County/Bluffdale	
Bridge	3.1 J	1	Utah and Salt Lake Canal Company	Install a bridge over the Utah Lake Distribution canal.	Bridge	\$150,000	Salt Lake County/Bluffdale	

JORDAN RIVER PARKWAY TRAIL MASTER PLAN

Feature Category	Map Code	Segment	Jurisdiction	Recommendation	Description	CIP Budget Implication	Responsibility	Other Notes
Bridge	3.1 K	1	Bluffdale	Install a bridge over the Jordan River.	Bridge	\$250,000	Salt Lake County/Bluffdale	
Paved Trail	3.2 A	2	Bluffdale	Construct multi-purpose trail from Parry Farms Development to Spring View Farms Trailhead.	Approximately 1.25 miles paved trail	Funded	Salt Lake County/Bluffdale	In design; construction 2008/2009; ZAP funded.
Signage	3.2 B	2	Bluffdale	Sign underpass as closed during flooding	Signage	\$1,000	Salt Lake County/Bluffdale	See Chapter 5 - Signage Program.
Trailhead and Paved Trail	3.2 C	2	Bluffdale	Complete Arrow Trailhead, and connect to existing paved trail. Add signing.	Expand existing equestrian facilities to include parking for paved trail users, park, and 0.5 miles of paved trail for pedestrians.	\$418,800	Salt Lake County/Bluffdale	
Eq. Trail	3.2 D	2	Bluffdale	Construct equestrian trail from Parry Farms to Spring View Farms.	Approximately 2.8 miles equestrian trail	\$147,840	Salt Lake County/Bluffdale	
Eq. Trail	3.2 E	2	Bluffdale	Construct equestrian trail from Arrow Trailhead to Bridge B2	Approximately 0.5 miles of equestrian trail	\$26,400	Salt Lake County/Bluffdale	
Eq. Trail	3.2 F	2	Bluffdale	If needed, develop an alternative equestrian trail alignment that follows the west side of 1300 West, beginning where 1300 West diverges from the Jordan River, continuing north to 14600 South, turning east to Future 14600 South Crossing, and north to proposed new bridge.	Approximately 1.4 miles of equestrian trail	\$73,920	Salt Lake County/Bluffdale	Negotiate easements with private land owners.
Eq. Trail	3.2 G	2	Bluffdale	If needed, develop an alternative equestrian trail alignment that follows closer to the eastern bank of the Jordan River, turns west, crosses 14600 South near the river, and follows the west bank.	Approximately 0.6 miles of equestrian trail from where trail diverges from 3.2 D alignment	\$31,680	Salt Lake County/Bluffdale	
Road Crossing	3.2 H	2	Bluffdale	An at-grade crossing will be installed to provide a safe crossing point for pedestrians.	Painted crosswalk and signage	\$5,000	Salt Lake County/Bluffdale	
Road Crossing	3.2 I	2	Bluffdale	An at-grade crossing will be installed to provide a safe crossing point for equestrians.	Painted crosswalk and signage	\$5,000	Salt Lake County/Bluffdale	
Eq. Bridge	3.2 J	2	Bluffdale	An equestrian bridge will be installed across the Jordan River to accommodate equestrian users.	Bridge	\$250,000	Salt Lake County/Bluffdale	

JORDAN RIVER PARKWAY TRAIL MASTER PLAN

Feature Category	Map Code	Segment	Jurisdiction	Recommendation	Description	CIP Budget Implication	Responsibility	Other Notes
Road Crossing	3.2 K	2	Bluffdale	An at-grade crossing will be installed to provide a safe crossing point for equestrians.	Painted crosswalk and signage	\$5,000	Salt Lake County/Bluffdale	
Eq. Bridge	3.2 L	2	Bluffdale	If needed, an equestrian bridge will be installed across the Jordan River to accommodate equestrian users.	Bridge	\$250,000	Salt Lake County/Bluffdale	
Paved Trail	3.3 A	3	Draper	Realign portion of paved trail near the silo. Any other new paved trails in this area will not be the "official" Jordan River Parkway Trail.	Approximately 0.5 miles of paved trail. New trails have been proposed for the Utah State Department of Natural Resources Land east of the Jordan River.	\$118,800	Draper	Any new trails should be minimal and should connect to the Jordan River Parkway Trail System.
Access	3.3 B	3	Draper	Restrict unauthorized access to trail and utility access area with lockable gate, and revegetate gravel area.	Unauthorized vehicles have been accessing this area, driving on the trail, and parking.	\$30,000	Salt Lake County/Draper/UDOT	
Signage	3.3 C	3	Draper	Sign underpass as closed during flooding.	Signage	\$1,000	Salt Lake County/Draper	See Chapter 5 - Signage Program.
Policy	3.3 D	3	Draper	Any new equestrian trails in this area will not be the "official" Jordan River Parkway Trail.	New trails have been proposed for the Utah State Department of Natural Resources Land east of the Jordan River.	N/A	Draper	Any new trails should be minimal and should connect to the Jordan River Parkway Trail System.
Eq. Trail	3.3 E	3	Draper	Construct an equestrian trail along 12300 South St. to form a loop trail system around Riverbend Nature Area.	Approximately 0.4 miles of equestrian trail along northern boundary of Riverbend Nature Area	\$21,120	Salt Lake County/Draper	Coordinate with Riverbend Nature Area land owners.
Eq. Trail	3.3 F	3	Riverton	Re-align equestrian trail in Dr. O. Roi Hardy Park at pump house.	Approximately 0.1 miles of equestrian trail needs to be re-aligned where pump house has encroached on trail.	\$5,280	Riverton	
Signage	3.3 G	3	Draper	Sign equestrian trail between Bangerter Highway and Alan Pointe Drive.	Signage	\$3,000	Salt Lake County/Draper	See Chapter 5 - Signage Program.
Paved Trail	3.4 A	4	South Jordan City	Construct a paved trail from Reeves Lane to 11400 South.	Approximately 0.5 miles of paved trails along the west side of the Jordan River	\$118,800	Salt Lake County/South Jordan City	South Jordan City has negotiated with landowner for an easement.
Eq. Trail & Signage	3.4 B	4	Draper	Re-align equestrian trail , where possible, to separate it from the multi-purpose trail between 12300 So. and Alan Pointe Drive. Sign equestrian trail.	Approximately 0.6 miles of equestrian trail along the east side of the river and signage	\$31,680	Draper	See Chapter 5 - Signage Program.

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Feature Category	Map Code	Segment	Jurisdiction	Recommendation	Description	CIP Budget Implication	Responsibility	Other Notes
Eq. Trail	3.4 C	4	South Jordan	Construct an equestrian trail from Reeves Lane to 11400 South.	Approximately 0.6 miles of equestrian trail along the west side of the river	\$31,680	Salt Lake County/South Jordan City	South Jordan City has negotiated with landowner for an easement.
Bridge	3.4 D	4	South Jordan/Draper	Install a bridge at 11400 South across the Jordan River	Bridge	\$250,000	UDOT/South Jordan/Draper/Salt Lake County	May be done in conjunction with the vehicular bridge crossing at 11400 South.
Signage	3.5 A	5	South Jordan	Sign underpass as closed during flooding.	Signage	\$1,000	Salt Lake County/South Jordan	See Chapter 5 - Signage Program.
Paved Trail	3.5 B	5	South Jordan, Salt Lake County, and West Jordan	Construct a paved trail between Shields Lane (10000 South) and 8400 South	Approximately 0.8 miles of paved trail on the west side of the river to Bridge B5 in River Oaks Golf Course and approximately 1.2 miles of paved trail from Bridge B5 to 8400 South	\$190,080	South Jordan/Sandy City	In design; construction 2007 funded. Easements have already been negotiated by South Jordan City from Shields Lane to Bridge B5 in River Oaks Golf Course. Need to negotiate with various agencies for northern segment. Shields Lane to Bridge B5 funded.
Paved Trail	3.5 C	5	West Jordan	Construct a paved trail between the existing paved trail segment in River Oaks Golf Course to the recommended paved trail segment which follows the North Jordan Canal Road along the western edge of the golf course.	Approximately 0.3 miles of paved trail along the northern edge of River Oaks Golf Course, north of the driving range	\$71,280	Salt Lake County/Sandy City	Need to negotiate with River Oaks Golf Course.
Paved Trail	3.5 D	5	West Jordan/Salt Lake County	If needed, develop an alternative route from the recommended underpass at the North Jordan Canal Road to connect to the paved trail near 8400 South.	Approximately 1 mile of paved trail along the southern edge of the Rocky Mountain Power property, crossing the Jordan River along 9000 South, turning north and following the east bank of the river until it connect to the existing paved trail.	\$237,600	Salt Lake County/West Jordan	Need to negotiate with private land owners and various agencies.
Underpass	3.5 E	5	West Jordan/Salt Lake County	Construct an underpass along the North Jordan Canal Road to get trail users past 9000 South safely.	Underpass	\$1,000,000	Salt Lake County/West Jordan/UDOT	
Trailhead	3.5 F	5	Sandy City/Salt Lake County	Provide curb cut, paved trail connection, and official trailhead signage.	One curb cut in conjunction with paved trail connection to existing paved trail (approximately 30 feet) and signage for trailhead	\$5,000	Salt Lake County/Sandy City	
Bridge	3.5 G	5	West Jordan/Midvale City	Install a bridge near 8450 S. across the Jordan River.	Bridge	\$250,000	Salt Lake County, West Jordan, and Midvale City	
Policy	3.5 H	5	South Jordan, Salt Lake County, and West Jordan	No new equestrian trails are recommended in this section.		N/A	South Jordan, Salt Lake County, and West Jordan	

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Feature Category	Map Code	Segment	Jurisdiction	Recommendation	Description	CIP Budget Implication	Responsibility	Other Notes
Policy	3.6 A	6	Midvale	Any new paved trails in this area will not be the "official" Jordan River Parkway Trail.	New trails have been proposed for the Midvale Slag and Sharon Steel sites east of the Jordan River.	N/A	Midvale	Any new trails should be minimal and should connect to the Jordan River Parkway Trail System.
Signage	3.6 B	6	West Jordan	Sign underpass as closed during flooding.	Signage	\$1,000	Salt Lake County/West Jordan	See Chapter 5 - Signage Program.
Underpass	3.6 C	6	West Jordan	Redesign the 7800 South underpass to be more gradual with safer grades and visibility.	Underpass	\$100,000	Salt Lake County, West Jordan, and UDOT	When road upgrades are made, include the pedestrian crossing.
Railroad Underpass	3.6 D	6	UTA, West Jordan, and Midvale	Construct a new pedestrian underpass when TRAX line is built.	Underpass	N/A	UTA, West Jordan, and Midvale	Underpass should be constructed to remain functional at high water levels, and should have enough clearance for all paved trail users. See project 4.6 under water trail projects for further recommendations.
Trail Abandonment	3.6 E	6	West Jordan	Eliminate the eastern part of the paved trail loops just north of Gardner Village.	Abandon approximately 0.1 miles of paved trail and revegetate	\$5,000	Salt Lake County/West Jordan	Portions of these trail segments have already been lost to streambank erosion.
Policy	3.6 F	6	West Jordan/Midvale	No new equestrian trails are recommended in this section.		N/A	West Jordan/Midvale	
Paved Trail	3.7 A	7	Taylorsville	Pave trail from trailhead at Hidden Cove Dr. to river trail.	Less than 0.1 miles of paved trail	\$23,760	Taylorsville	
Policy	3.7 B	7	Taylorsville/Murray	No new equestrian trails are recommended in this section.		N/A	Taylorsville/Murray	
Policy	3.8 A	8	West Valley	Any new paved trails in this area will not be the "official" Jordan River Parkway Trail.	New trails have been proposed north of Meadow Brook Expressway to 3300 South	N/A	West Valley City	Any new trails should be minimal and should connect to the Jordan River Parkway Trail System.
Policy	3.8 B	8	Murray/Taylorsville/West Valley/South Salt Lake	No new equestrian trails are recommended in this section.		N/A	Murray/Taylorsville/West Valley/South Salt Lake	
Signage	3.9 A	9	West Valley/South Salt Lake	Sign underpass as closed during flooding.	Signage	\$1,000	West Valley/South Salt Lake	See Chapter 5 - Signage Program.
Policy	3.9 B	9	West Valley City	Any new paved trails in this area will not be the "official" Jordan River Parkway Trail.	New trails have been proposed between 3300 South Bridge B15 at approximately 3100 South	N/A	West Valley City	Any new trails should be minimal and should connect to the Jordan River Parkway Trail System.
Maintenance	3.9 C	9	South Salt Lake	Control vegetation selectively to help discourage undesirable behavior, clean up weeds and debris along trail, and repair paved trail.	Approximately 2.3 miles of trail that need to be cleaned up and repaired.	\$15,000	South Salt Lake and Salt Lake County	Trail corridor needs to be maintained according to recommendations established in Chapter 5 at a minimum.

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Feature Category	Map Code	Segment	Jurisdiction	Recommendation	Description	CIP Budget Implication	Responsibility	Other Notes
Policy	3.9 D	9	West Valley/South Salt Lake/Salt Lake City	No new equestrian trails are recommended in this section.		N/A	West Valley/South Salt Lake/Salt Lake City	
Paved Trail	3.10 A	10	Salt Lake	Pave trail at the Peace Labyrinth to the river trail.	Less than 0.2 miles of paved trail	\$47,520	Salt Lake	
Paved Trail/Trailhead	3.10 B	10	Salt Lake	Construct trail crossing at 900 South and rail road tracks. Provide trail access at 900 South.	Less than 0.1 miles of paved trail, and new access signage. Signage for directing users to west side of river.	\$25,760	Salt Lake	When rail right-of-way is abandoned in 2008, this connection is possible.
Paved Trail	3.10 C	10	Salt Lake	Pave trail just south of I-80.	Less than 0.1 miles of paved trail	\$23,760	Salt Lake	
Underpass	3.10 D	10	Salt Lake	Construct a pedestrian underpass that will continue trail to north.	Pedestrian underpass to take pedestrians safely under I-80	Funded	Salt Lake	The funding has already been secured to design and construct this underpass.
Policy	3.10 E	10	Salt Lake	No new equestrian trails are recommended in this section.		N/A	Salt Lake	
Paved Trail	3.11 A	11	Salt Lake	Pave trail from I-80 to North Temple.	Less than 0.1 miles of paved trail	\$23,760	Salt Lake	
Underpass	3.11 B	11	Salt Lake	Extend the trail underneath Interstate 80 by providing an underpass.	Underpass with trail.	\$300,000	Salt Lake	
Trail & Railroad Crossing	3.11 C	11	Salt Lake	Continue the trail from Interstate 80 north to North Temple.	Approximately 0.5 miles of paved trail and a pedestrian-operated railroad crossing.	\$10,000,000	Salt Lake	City is completing negotiations for rights-of-way through the properties north of I-80, and when rail right-of-way is consolidated the trail can be constructed. Rail company has agreed to an at grade, pedestrian-operated crossing.
Trailhead	3.11 D	11	Salt Lake	Construct trailhead south of 1000 North, east of the river.	Negotiate and formalize a trailhead with Utah Division of Facilities and Construction Management using existing parking lot	\$700,000	Salt Lake	
Trailhead	3.11 E	11	Salt Lake	Develop a new trailhead at 1000 North to serve both water and paved trail users.	New trailhead south of 1000 North on east side of river	\$185,000	Salt Lake	
Signage	3.11 F	11	Salt Lake	Replace signage at 1000 North, and install signage at the north end of the Jordan River Golf Course for Maintenance Access Only	Signage	\$2,000	Salt Lake	
Policy	3.11 G	11	Salt Lake	No new equestrian trails are recommended in this section.		N/A	Salt Lake	

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Feature Category	Map Code	Segment	Jurisdiction	Recommendation	Description	CIP Budget Implication	Responsibility	Other Notes
Paved Trail	3.12 A	12	Salt Lake	Construct multi-purpose trail between Bridge 31 and Riverview Avenue.	Less than 0.5 miles of paved trail from 1000 north to Bridge B31, approximately 0.6 miles of paved trail from Bridge B31 to Redwood Road, and approximately 1.3 miles of paved trail from Redwood Road to 2700 North	Funded	Salt Lake	In design, construction 2007 funded.
Street Crossing	3.12 B	12	Salt Lake	Construct pedestrian crossing at Redwood Road and 1800 North.	Signalized street crossing at Redwood Road near Bridge B32	Funded	Salt Lake	In design and funded.
Maintenance	3.12 C	12	Salt Lake/Salt Lake County	Clean up garbage and debris from this section of trail.		\$10,000	Salt Lake/Salt Lake County	Trail corridor needs to be maintained according to recommendations established in Chapter 5 at a minimum.
Bridge	3.12 D	12	Salt Lake	Replace Bridge B32	Unsafe, outdated bridge needs to be replaced	\$250,000	Salt Lake	Bridge needs to provide ample clearance for water trail users during high water levels, and should meet all current safety and design standards required by Salt Lake City and Salt Lake County.

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A.4 Detailed Water Trail Recommendations

Feature Category	Map Code	Segment	Jurisdiction	Recommendation/Description	CIP Budget Implication	Annual Operations and Maintenance Cost	Responsibility	Other Notes
Portage/Signage	4.1 A	1	Canal Company, JWCD and Salt Lake County	Construct safe portage around dam and install signing to warn boaters of barrels stretching across river.	\$12,000		JVWCD/Salt Lake County	Negotiate with Canal Companies and Jordan Valley Water Conservancy District to install signs.
Bridge	4.1 B	1	Salt Lake County	Remove and replace bridge.	\$140,000		Salt Lake County	Negotiate with responsible party for removal and replacement. Obtain required permits.
Hazard Removal	4.1 C	1	Salt Lake County	Remove hazardous concrete pilasters.	-		Salt Lake County	Negotiate with responsible party for removal and replacement. Obtain required permits.
Launch/Portage Upgrade	4.1 D	1	Salt Lake County	Redesign and reconstruction portage/launch. Dismantle existing.	\$20,000		Salt Lake County	This is a good location. Combine two existing areas into one location.
Signage	4.1 E	1	Salt Lake County	Interpretive opportunity at old power station.	\$2,500		Salt Lake County	One 24" x 36" interpretive sign.
Hazard Removal	4.2 A	2	Bluffdale	Remove barbed wire fencing.	-		Bluffdale	Negotiate with responsible party for removal. Obtain required permits.
Hazard Removal	4.2 B	2	Bluffdale	Remove earthen weir.	-		Bluffdale	Negotiate with responsible party for removal. Obtain required permits.
Launch/Portage Upgrade	4.2 C	2	Bluffdale	Redesign and reconstruction portage/launch. Dismantle existing.	\$15,000		Salt Lake County	Include some designated parking.
Hazard Removal/Signage	4.2 D	2	Bluffdale	Remove view-obstructing vegetation, and sign abrupt change in direction of river.	-		Salt Lake County	Obtain required permits.
Bridge	4.3 A	3	Riverton and Bluffdale	Replace or reconstruct footbridge that is too low.	\$180,000		Salt Lake County	Obtain required permits.
Launch Upgrade/Trailhead	4.3 B	3	Bluffdale	Redesign launch and provide parking.	\$110,000		Salt Lake County	This is a valued site. Assumes no restroom, because users can walk up to Jordan River Rotary Park Trailhead or Dr. O. Roi Hardy Park.
Hazard Removal	4.3 C	3	Riverton	Clear vegetation and debris from channels.	-		Salt Lake County	Obtain required permits.
Launch	4.4 A	4	South Jordan	No launch recommended	N/A		Salt Lake County	
Signage	4.4 B	4	South Jordan	provide sign warning boaters of potential water flow change.	\$500		Salt Lake County	
Launch/Trailhead	4.5 A	5	South Jordan	Provide launch and parking.	\$185,000		Salt Lake County	Includes restroom.
Portage	4.5 B	5	South Jordan	Redesign and reconstruct portage. Dismantle existing.	\$5,000		Salt Lake County	Coordinate with golf course and canal company.
Hazard Removal	4.5 C	5	Salt Lake County	Lower or relocate sewer pipe causing hazardous rapids.	-		Salt Lake County	Negotiate with responsible party.
Signage	4.6 A	6	Midvale and West Jordan	Provide sign warning boaters of potential water flow change.	\$500		Salt Lake County	Negotiate with responsible party.

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Feature Category	Map Code	Segment	Jurisdiction	Recommendation/Description	CIP Budget Implication	Annual Operations and Maintenance Cost	Responsibility	Other Notes
Signage	4.6 B	6	Midvale	Provide sign warning boaters of potential water pollution.	\$500		Salt Lake County	Negotiate with responsible party.
Hazard Removal	4.6 C	6	West Jordan and Midvale	Remove any obstructions left from TRAX construction.	-		Salt Lake County	Negotiate with TRAX to remove any debris and remaining structures after the bridge is reconstructed.
Signage	4.6 D	6	West Jordan and Midvale	Providing signs warning boaters of hazardous river conditions.	\$1,000		Salt Lake County	
Launch	4.6 E	6	Murray and West Jordan	A launch is not needed here.	N/A		Salt Lake County	
Safety Device, Launch/Portage Consolidation	4.6 F	6	Murray and West Jordan	Install barrels prohibiting boaters from going under the bridge. Install new launch at waypoint 9-05. Redesign old launch, waypoint 9-06, for use on the multi-purpose trail as an overlook or fishing dock.	\$15,000		Salt Lake County	Use barrels to force boaters to use this portage with a relocated/upgraded launch.
Launch Removal	4.7 A	7	Murray	Remove this launch.	\$5,000		Murray	Unsafe and in disrepair. Not needed.
Signage	4.7 B	7	Murray and Taylorsville	Provide sign warning boats of changing river conditions.	\$500		Salt Lake County	Unpredictable conditions caused by underwater pipelines.
Launch Upgrade	4.7 C	7	Taylorsville	Redesign and reconstruct this launch.	\$10,000		Salt Lake County	
Launch Removal	4.8 A	8	Taylorsville	Remove existing boat launch.	\$5,000		Salt Lake County	
Safety Device	4.8 B	8	Taylorsville	Install barrels forcing boaters to use portage.	\$5,000		Salt Lake County	This is a highly dangerous area.
Portage	4.8 C	8	Taylorsville	Develop portage.	\$10,000		Salt Lake County	
Launch Upgrade	4.9 A	9	West Valley	Redesign and reconstruct this launch.	\$10,000		Salt Lake County	
Launch Upgrade	4.9 B	9	West Valley	Redesign and reconstruct this launch.	\$10,000		Salt Lake County	
Portage	4.9 C	9	Salt Lake	Redesign and reconstruct this portage avoiding the diversion dam for Surplus Canal.	\$10,000		Salt Lake County	
Signage	4.9 D	9	Salt Lake	Provide sign warning boaters of low bridge and how to portage around.	\$500		Salt Lake County	
Standards	4.10 A	10	Salt Lake	Good example for ideal launch facility	N/A		Salt Lake County	
Signage	4.10 B	10	Salt Lake	Provide sign indicating danger caused by changing water conditions.	\$500		Salt Lake County	
Signage	4.10 C	10	Salt Lake	Provide signs indicating danger caused by changing water conditions.	\$500		Salt Lake County	
Signage	4.10 D	10	Salt Lake	Provide signs indicating danger caused by changing water conditions.	\$500		Salt Lake County	
Hazard Removal	4.10 E	10	Salt Lake	When track is abandoned, remove all bridge elements in the river and on its banks.	-		Salt Lake County	Work with Salt Lake City and railroads to assure everything is removed.
Access	4.10 F	10	Salt Lake	Design and construct small ramp allowing access to restroom facilities for longer boat trips.	\$5,000		Salt Lake County	

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Feature Category	Map Code	Segment	Jurisdiction	Recommendation/Description	CIP Budget Implication	Annual Operations and Maintenance Cost	Responsibility	Other Notes
Bridge	4.10 G	10	Salt Lake	Replace and construct low footbridge.	\$100,000		Salt Lake	
Signage	4.10.H	10	Salt Lake	Redesign, relocate, and separate signs warning boaters of Canoe Chute and where to exit the river.	\$1,500		Salt Lake County	Frequent and appropriately spaced signs are needed.
Launch Upgrade	4.10 I	10	Salt Lake	Repair and upgrade this boat launch.	\$10,000		Salt Lake	
Bridge	4.11 A	11	Salt Lake	As rail tracks are consolidated, design new bridge to provide adequate clearance and not trap debris.	\$100,000		Salt Lake County	Work with Salt Lake City and railroads to assure bridge is designed adequately and abandoned structures are completely removed.
Trailhead	4.11 B/4.11 F	11	Salt Lake	Utilize developed parking at State Fair Park.	\$140,000		Salt Lake	Negotiate with Utah Division of Facilities Construction and Management for shared-use of State Fair Park parking lot. Would include restrooms.
Signage	4.11 C	11	Salt Lake	Provide sign warning boaters of low clearance during high water and how to portage around the hazard.	\$500		Salt Lake County	
Signage	4.11 D	11	Salt Lake	Provide sign warning boaters of low clearance and potentially changing water conditions.	\$500		Salt Lake County	
Signage	4.11 E	11	Salt Lake	Provide sign warning boats of changing river conditions.	\$500		Salt Lake County	
Launch Removal	4.11 G	11	Salt Lake	Remove ramp.	\$5,000		Salt Lake	
Launch Upgrade	4.11 H	11	Salt Lake	Upgrade and improve existing ramp, and design and construct trailhead.	\$115,000		Salt Lake County	
Launch Upgrade	4.12 A	12	Salt Lake	Redesign and reconstruct boat launch	\$10,000		Salt Lake	Include boater access, parking, restrooms, and emergency river access.
Bridge	4.12 B	12	Salt Lake	Replace this old, deteriorated bridge	\$100,000		Salt Lake	
Signage	4.12 C	12	Salt Lake	Provide sign warning boaters of low clearance.	\$500		Salt Lake	
Connection	4.12 D	12	Salt Lake County/Davis County	Coordinate water trail with Davis County, and consider permit program to help address environmental concerns	-		Salt Lake County/Davis County	

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